

Desk Copy

CITY OF HERCULES
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NEW PACIFIC PROPERTIES SPECIFIC PLAN

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April 11, 2000

RESOLUTION NO. 00-43

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF HERCULES
APPROVING THE NEW PACIFIC PROPERTIES SPECIFIC PLAN (SP# 01-99)
FOR 206 ACRES ON THE FORMER PACIFIC REFINERY SITE**

WHEREAS, the City Council of the City of Hercules has considered an application for a Specific Plan (SP# 01-99) filed by Phillips Brandt & Reddick/Kenneth Ryan (on behalf of Hercules LLC, d.b.a New Pacific Properties) in accordance with Sections 10-1.1301 through Section 10-1.1310 of the Hercules Municipal Code;

WHEREAS, in accordance with the California Environmental Quality Act, the City Council has certified a Final Environmental Impact Report (Final EIR) for this project, after due study and deliberation and public hearings, and directed that a Notice of Determination be circulated to the public; and

WHEREAS, the City Council finds, after due study and deliberation and public hearings, finds that the proposed Specific Plan further implements the City of Hercules' overall redevelopment and economic development goals and efforts to attract quality development; and

WHEREAS, the City Council finds, after due study and deliberation and public hearings, finds that the proposed location of the Specific Plan will eliminate a vacant industrial site and provide for the redevelopment of this site for a new residential community; and

WHEREAS, the City Council finds, after due study and deliberation and public hearings, that the proposed change in land use policy through the Specific Plan will help foster continued economic development and continued implementation of the Hercules Redevelopment Agency's redevelopment efforts.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Hercules hereby adopts the Specific Plan and all of the text, maps and illustrations contained therein.

PASSED AND ADOPTED by the City Council of the City of Hercules on this
11th day of April, 2000, by the following vote:

AYES: Segerberg, Taraya, Watson and Blackmon

NOES: Paras

ABSENT:


Herman Blackmon, Mayor

ATTEST


Doreen Mathews, City Clerk

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I. EXECUTIVE SUMMARY

A. INTRODUCTION

1. Intent and Purpose of Specific Plan

The New Pacific Properties Specific Plan document is intended to provide for the orderly and efficient development of the Specific Plan area in accordance with the provisions of the City of Hercules General Plan. It contains development standards and design guidelines which apply only to properties within the Specific Plan area which are intended to implement the goals, objectives and policies of the City's General Plan.

To address changing market conditions over the life span of a project of this scope and size, it is the intent of the Specific Plan to be flexible in terms of the product type, actual location, and acreage of the residential products within the site, so that products can be developed and rearranged within the plan as long as the overall 880-unit project density is not exceeded. However, major community framework elements such as parks, open space, trails, a school site and the retail/residential flex site are anticipated to remain in the general locations presented in the Land Use Concept plan.

The purpose of the New Pacific Properties Specific Plan is two-fold:

- a. to implement the General Plan policies by presenting more detailed direction for future development; and
- b. to establish zoning standards and implementation mechanisms applicable solely to the New Pacific Properties project.

Implementation of the New Pacific Properties Specific Plan provides the parameters for establishment of a cohesive planned development. This will be achieved by coordinating the land use, intensity, scale and aesthetic characteristics of development with the goals and policies of the Hercules General Plan. The New Pacific Properties Specific Plan allows up to 880 residential units.

2. Authority and Scope

The New Pacific Properties Specific Plan is being prepared and established under the authority granted to the City of Hercules in accordance with the requirements of the California Government Code, Title 7, Division 3, Articles 8 and 9, Sections 65450 and 65507.

The State of California encourages cities to adopt Specific Plans either by resolution to establish a policy document, or by ordinance to establish a regulatory document. The New Pacific Properties Specific Plan document is intended to be adopted by the Hercules City Council through two legislative actions. Chapters I, II, III, V and VI are adopted by resolution. Development standards and regulations (Chapter IV) for the Specific Plan is adopted by ordinance. When adopted by City legislative action, this Specific Plan document will serve both planning and regulatory functions. The New Pacific Properties Specific Plan contains the standards, procedures, and guidelines necessary to accomplish this purpose.

All future development plans, tentative parcel and/or tract map(s), and/or other similar entitlements for properties located within the boundaries of this Specific Plan area shall be consistent with the regulations set forth herein and with all other applicable City of Hercules regulations in affect at the time of adoption of the Specific Plan. Furthermore, all regulations, conditions, and programs contained herein shall be deemed separate, distinct, and independent provisions of the New Pacific Properties Specific Plan. In the event that any such provisions are held invalid or unconstitutional by a State of Federal court of competent jurisdiction, the validity of all remaining provisions of this plan shall not be affected.

3. Background

The City of Hercules owes its roots to the American Industrial Revolution when a California Powder Works plant was established on the shores of San Pablo Bay. The name Hercules comes from a potent and explosive black powder first made in the California Powder Work's Santa Cruz plant, which was eventually relocated to Hercules. The company named their product after Hercules, the Greek mythological hero known for his strength, in order to signify how powerful their black powder was. In the business of making black powder, dynamite and eventually TNT, the California Powder Works company prospered. The success of the Powder Works plant grew into the company town of Hercules which was incorporated in 1900. By 1917, Hercules was the largest producing explosives plant in the U.S. However, over time, the declining market for dynamite and the danger to employees, coupled with the booming population of the area, lead the Company to change its focus, and eventually the lands including the New Pacific Properties site were sold.

Today, the City covers approximately seven square miles. Its 1998 population of 19,050 reflects a comparatively younger community than neighboring communities and is ethnically diverse. The median household income in Hercules is \$60,000, one of the highest in west Contra Costa County.

A portion of the New Pacific Properties site itself is the location of the former Pacific Refinery Company petroleum refinery. The refinery imported crude oil and other petroleum feedstocks to produce a mix of fuel products such as liquified petroleum gases, gasoline, diesel fuel, jet fuel, fuel oil, and asphalt flux. In 1994, the company proposed to modify and add to the existing processing equipment in order to produce the cleaner burning reformulated gasoline and diesel fuel mandated by the Federal Clean Air Act Amendment of 1990 and the State of California rules and regulations. An Environmental Impact Report was prepared by the City of Hercules and certified by the City Council for that proposal in March of 1994. From 1994 through 1997 the facility was primarily used for the production of asphalt and for petroleum storage operations. Economic factors lead to the shut down of the refinery. In 1997, New Pacific Properties acquired the properties within the Specific Plan area. New Pacific Properties has received approval for, and is in the process of, decommissioning and dismantling of the refinery and conducting the environmental remediation of the site. In March 9, 1999, the City adopted a Redevelopment Plan which includes the New Pacific Properties site.

4. Location

a. Regional Location

The project is located within the City of Hercules in west Contra Costa County, in northern California. Contra Costa County is one of nine counties comprising the San Francisco Bay area, and falls on the east side of smaller San Pablo Bay approximately 25 miles northeast of San Francisco, and 30 miles south of Napa. *Exhibit 1, Regional Context Map*, shows the project's location in relation to the Bay area and surrounding urban centers.

b. Specific Plan Area

The Specific Plan area includes the New Pacific Properties site located at 4901 San Pablo Avenue, at the northeast end of the City of Hercules' limits, immediately adjacent to the unincorporated community of Rodeo. It contains 206.3 acres of land and is flanked by Union Pacific Railroad to the west, and beyond that by San Pablo Bay; the community of Rodeo to the north; the Interstate 80 (East Shore Freeway) to the east; and North Shore Business Park and the PG&E hydrocarbon temporary storage facility to the south. The PG&E facility is located on the east side of San Pablo Avenue and the North Shore Business Park development on the west side. See *Exhibit 2, Vicinity Map*.

San Pablo Avenue divides the project area into two subareas. The area east of the road (approximately 62 acres) is undeveloped and has been used for cattle grazing in the past. The area to the west of San Pablo Avenue (approximately 144 acres in total) is occupied by the former Pacific Refinery. Storage tanks, wastewater treatment ponds, detention basins, process units and piping, and administrative space covered roughly 77 acres of this refinery area. The balance of the site west of San Pablo Avenue is largely undeveloped.

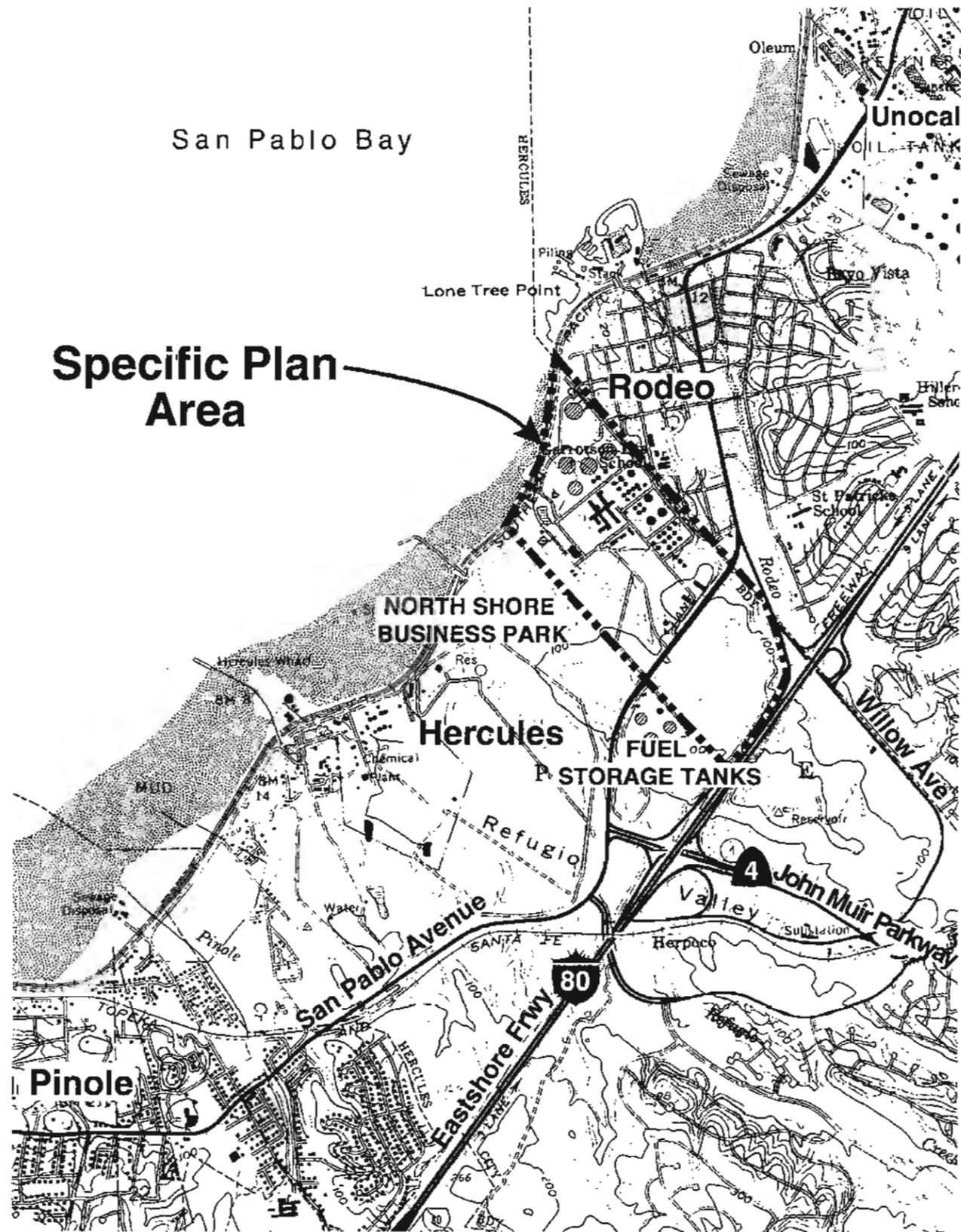
The Specific Plan area slopes from southeast to northwest, ranging in elevation from 212' mean sea level (MSL) near the I-80 (East Shore Freeway), to sea level at San Pablo Bay. The I-80 freeway lies to the east of the Specific Plan area. Freeway access is provided from the I-80/SR-4 and I-80/Willow Avenue interchanges, with San Pablo Avenue being the local connection to the Specific Plan area. *Exhibit 3, Aerial Photograph* shows the Specific Plan area as viewed from above San Pablo Bay.

B. OVERALL PROJECT INTENT

In developing the New Pacific Properties Specific Plan, the land development concept has been designed with the following goals in mind, as are reflected through this Specific Plan document:

- To reclaim the former Pacific Refinery site and transform it into an aesthetic living environment taking advantage of its prime bay shore location and viewsheds.
- To create a community that embraces all aspects of good urban design including





VICINITY MAP

NEW PACIFIC PROPERTIES SPECIFIC PLAN



11-10-09

EXHIBIT 2



Specific Plan Area

I-80 Freeway

Willow Ave.

San Pablo Ave.

PG&E Fuel Storage Tanks

Linus Pauling Ave.

North Shore Business Park

Rodeo

San Pablo Bay

considerations for functionality, social needs, economic viability, respect for the environment and aesthetic qualities.

- To establish consistency and implement the City of Hercules General Plan goals and policies.
- To provide a desirable community where people will want to live and recreate.
- To Provide for a range of housing opportunities responsive to local needs.
- To Provide for on-site retail commercial opportunities to offer convenience to the Specific Plan area and nearby residents.
- To create a cohesive identity for the Specific Plan area.
- To provide a planning framework that responds to the physical and market driven aspects of future development opportunities.
- To unify the project area through implementation of a strong landscape, architectural and signage program.
- To Establish a comprehensive pedestrian and bikeway system which assures public access to the bay shore area and to provide other on-site recreational amenities.

C. PROJECT SUMMARY

The New Pacific Properties Specific Plan and land development concept would transform the former refinery site into a planned residential community with parks, open space, trails, a public school and potential retail uses. Under this scenario, the 206.3-acre Specific Plan area will be developed into the following land uses: approximately 128.4 acres of land would be designated for up to 763 single-family detached residential units; 7.8 acres would be slated for 117± multi-family attached units or up to 45 single-family detached units; 6.0 acres would be reserved for a retail/residential flex site accommodating approximately 65,000 sq. ft. of floor area or 50 additional single-family detached residential units; 7.5 acres would accommodate a School Facility consisting of an elementary school and a park and play field; 26.9 acres of land would be set aside for additional neighborhood, linear, community trail and shoreline parks, parkway and natural open space lands; with the remaining 29.7 acres occupied by roads and associated open space. The proposed Land Use plan (*Exhibit 9, Land Use Concept*, found in Chapter III, Section A) illustrates the approximate layout of the various proposed land uses. This concept is presented for illustrative purposes only. It is intended that the plan be flexible in its implementation to address changing market conditions over the life span of a project of this scope and size. An amendment process is described in Section VI, Implementation Program, of this document should significant changes to the land development concept be proposed in the future. The New Pacific Properties Specific Plan allows for redistribution of various product types, a range of land uses, and a process to shift the location and refine the acreage of the various products within the Specific Plan area as market needs and demands dictate, provided that the overall density and general mix of densities is not exceeded and that circulation patterns are not significantly affected. For example, should the market indicate that there is need for one housing type over another, that product type can still be built within

the overall density in the planning areas as defined by the Land Use plan. However, major community framework elements such as parks, open space, trails, a school site, as well as the retail/residential flex site are anticipated to remain at their locations as depicted in *Exhibit 9, Land Use Concept*.

D. SPECIFIC PLAN DOCUMENT ORGANIZATION

The New Pacific Properties Specific Plan defines a vision and establishes guidelines for the short and long term development of the site. The basic approach for preparing this Specific Plan has been to recognize the interrelationship between land use, design, regulation, and sound economic, market, and financial considerations. The Specific Plan is arranged into seven chapters as follows:

- **Chapter I. Executive Summary** This chapter includes a discussion of the intent and purpose of the Specific Plan, the authority and scope, project background, a description of location of the site, the goals of the project, and an overview/summary of the project.
- **Chapter II. Planning Context** This chapter provides a summary of the existing conditions affecting the Specific Plan area, including existing City land use designations, existing and surrounding land use discussions, a summary of the existing infrastructure within the project area, and a brief discussion of market trends.
- **Chapter III. Specific Plan Elements** Forming the core of the Specific Plan document, this chapter presents the Land Use plan for the project, including proposed land use designations, and discusses the proposed circulation, grading, infrastructure and communities services elements of the Land Use plan.
- **Chapter IV. Development Standards** This chapter specifies the permitted and conditionally permitted uses in each Land Use designation proposed, and specifies standards for development within those districts, such as lot coverage, building height, setbacks, etc.
- **Chapter V. Design Guidelines** A creative yet flexible set of guidelines and design criteria for siting, architecture, landscaping and signage is specified providing direction for streetscape improvements, project identification and theming, hardscape elements and architectural guidelines.
- **Chapter VI. Implementation Program** This chapter contains a phasing program, a process for development/design review, and procedures for amending this Specific Plan.
- **Chapter VII. Relationship to Hercules General Plan** Chapter VII discusses how the Specific Plan conforms with the City of Hercules General Plan objectives and policies.
- **Technical Reports** This Specific Plan is based on several technical reports which were used in preparing both the Specific Plan and the environmental analysis.

II. PLANNING CONTEXT

A. JURISDICTIONAL CONSIDERATIONS

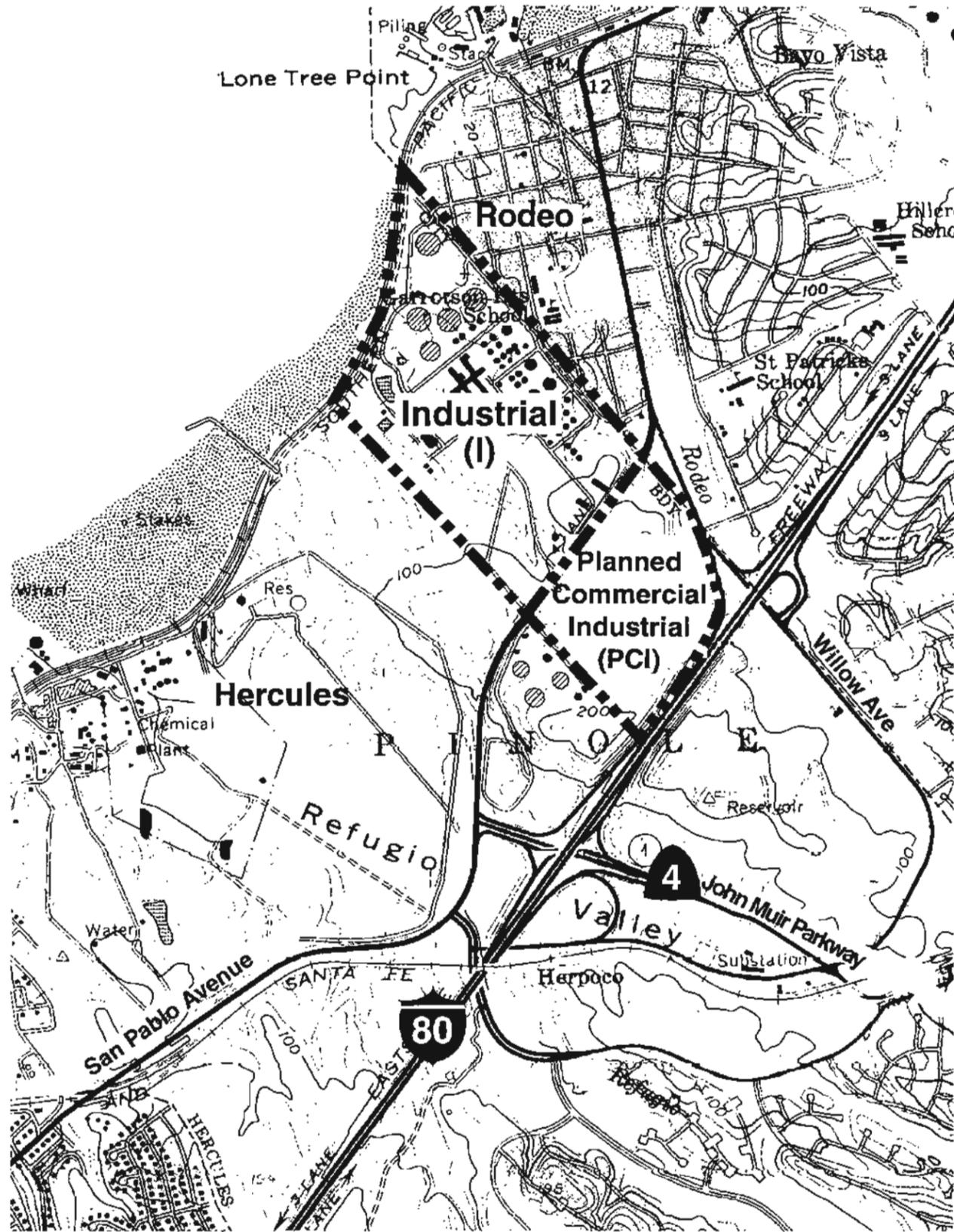
1. City of Hercules General Plan

The City of Hercules General Plan identifies distinct land use areas which include Residential, Parks-Open Space, Public, Commercial, Industrial and Other (Historic District). The General Plan document itself dedicates a chapter to each of the City elements it addresses. These consist of Land Use, Circulation/Scenic Highway, Housing, Open Space/Conservation, Safety/Seismic Safety, Noise, Hazardous Waste Management Plan, Economic Development, and Growth Management. Almost all chapters contain sections discussing Authority, Research and Analysis, and Implementation, although sections on Objectives/Goals, Policies, and Proposals or Programs are also incorporated in each element. The General Plan was approved by City Council on September 22, 1998.

The General Plan Land Use Element includes a Land Use Diagram which identifies nineteen (19) land use categories. The categories specify the allowed intensity and/or density of that particular land use. The population density is derived by multiplying 3.12 persons per household times the units per acre. The General Plan also measures the intensity, bulk and scale of commercial development in terms of allowable Floor Area Ratio (FAR). The FAR is the ratio of allowable building floor area to size of the lot. Gross floor area of a building is divided by the lot area to produce the FAR. The western portion of the New Pacific Properties site currently falls within the City's General Plan "Industrial" land use designation, while the eastern portion is designated "Planned Commercial Industrial." A General Plan Amendment from "Industrial" and "Planned Commercial Industrial" to "Specific Plan", will be needed to have the General Plan best reflect the proposed project and products. *Exhibit 4, Existing General Plan Land Use Designations*, shows the existing General Plan designation of the Specific Plan and surrounding areas.

There are several recommendations found in the General Plan which directly relate to the New Pacific Properties site. These recommendations or proposals include the following:

- Minimize the number of access points on and off of San Pablo Avenue.
- Avoid permanent damage to existing tree masses.
- Include private open space and recreation areas within residential neighborhoods.
- Locate regional hiking trail along the bay shoreline.
- Connect school yard/parks with trail systems leading to public open spaces.
- Integrate rights-of-ways into the open space system as trails and/or visual open space.
- Provide education system improvements.
- Maintain a traffic level of service (LOS) of "Low" D for Basic Routes (ie. all roadways and intersections within the Specific Plan area aside from San Pablo Avenue).
- Maintain a traffic LOS of E for Routes of Regional Significance (San Pablo Avenue).



EXISTING GENERAL PLAN
 LAND USE DESIGNATIONS
 NEW PACIFIC PROPERTIES SPECIFIC PLAN

The City has also adopted a Growth Management Element of the General Plan to comply with Contra Costa County Measure C (1988). The measure calls for "Routes of Regional Significance" to have a separate "traffic service objective" set cooperatively by all the jurisdictions of western Contra Costa County. San Pablo Avenue is identified as a route of regional significance. The Circulation Draft of the West County (1994) recommends a traffic service objective of LOS E at signalized intersections on San Pablo Avenue. The City has also adopted Levels of Service (LOS) for "Basic Routes", which include all remaining roadways within the City. An LOS "Low" D is required for residential streets and intersections during peak hours, as well as for commercial/industrial developments within the New Pacific Properties Specific Plan area. The particulars of Measure C are discussed in more depth in Section B. 4. of this chapter.

It should be noted that the Land Use Element of the General Plan recognizes that the City of Hercules "has sought to preserve its industrial based past while changing into a residential community of diverse character." The proposed primarily residential project will be one more step towards effectuation of that change. An amendment to the City's General Plan and General Plan Land Use Map will be required. Consistency with the General Plan is discussed in detail in Chapter VII.

2. City of Hercules Sphere of Influence/Annexation

The New Pacific Properties site falls within the City of Hercules City limits, and thus no annexation to the City is required.

3. City of Hercules Zoning Ordinance

The New Pacific Properties Specific Plan area is zoned "Industrial (M)." *Exhibit 5, Existing Zoning Designations*, shows the Industrial (M) zoning. In addition to the General Plan Amendment request, a rezoning from Industrial (M) to various Specific Plan zoning districts will be required in order to implement the project.

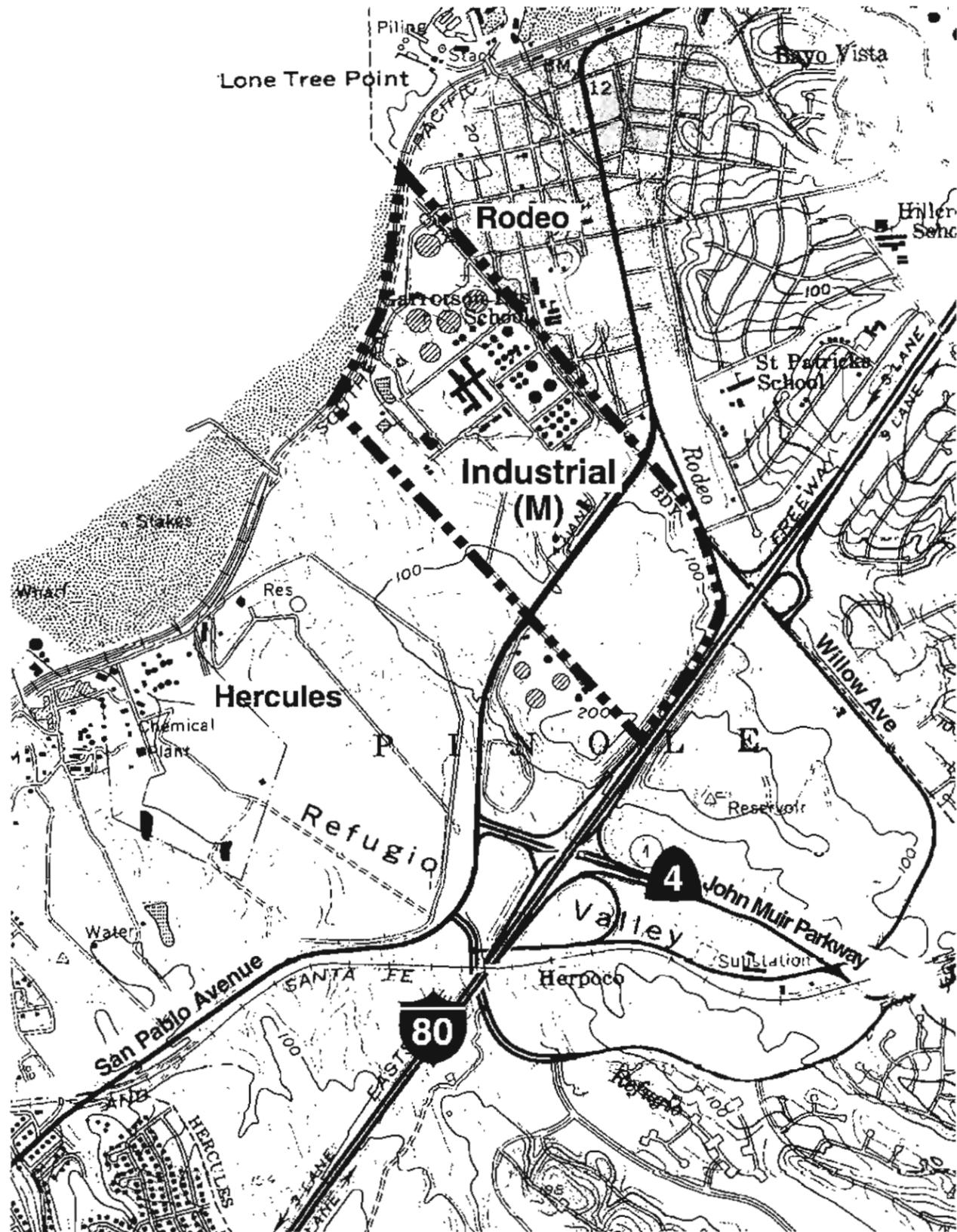
4. City of Hercules Redevelopment Plan

The City of Hercules recently completed an expansion of its Redevelopment Project Area which included the Specific Plan and other properties. A comprehensive Environmental Impact Report (EIR) was certified on March 9, 1999 by the City of Hercules for the Redevelopment Plan which includes the New Pacific Properties Specific Plan area.

B. EXISTING CONDITIONS

1. Historical Land Use within the Specific Plan Area

A portion of the western section of the New Pacific Properties site is the location of the former Pacific Refinery Company petroleum refinery. Economic factors lead to the closure of the refinery, and the New Pacific Properties has received approval for, and is in the process of, decommissioning and dismantling of the refinery and conducting the environmental remediation of the site. The remainder of the property east of San Pablo Avenue has historically been vacant and has been used for cattle grazing.



2. Existing Land Use

The site was occupied by the Pacific Refinery Company petroleum refinery which imported crude oil and other petroleum feedstocks to produce a mix of fuel products such as liquified petroleum gases, gasoline, diesel fuel, jet fuel, fuel oil, and asphalt flux. Site improvements included fuel storage tanks, a wastewater treatment facility and treatment ponds, detention basins and office space. These improvements are presently being dismantled in accordance with a remediation plan. The facility is expected to be fully decommissioned by the end of 1999. *Exhibit 6, Topography and Land Use Map of the Former Pacific Refinery*, shows the approximate location of the various refinery plant facilities prior to their removal.

3. Surrounding Land Uses and Relationship

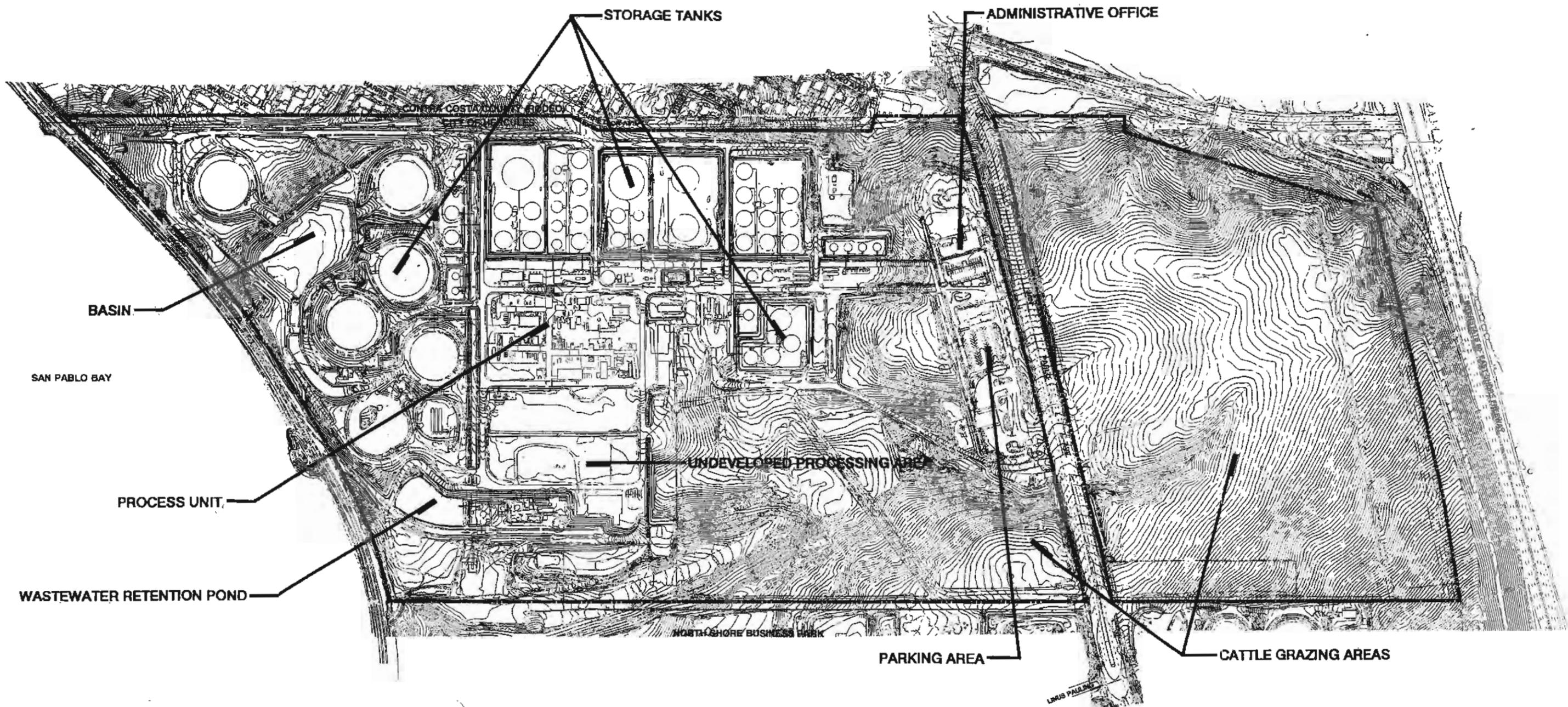
The New Pacific Properties Specific Plan area is surrounded by the following land uses, as depicted on *Exhibit 7, Surrounding Land Use*:

Northwest - The Union Pacific Railroad abuts the Specific Plan area to the immediate northwest, with San Pablo Bay lying beyond.

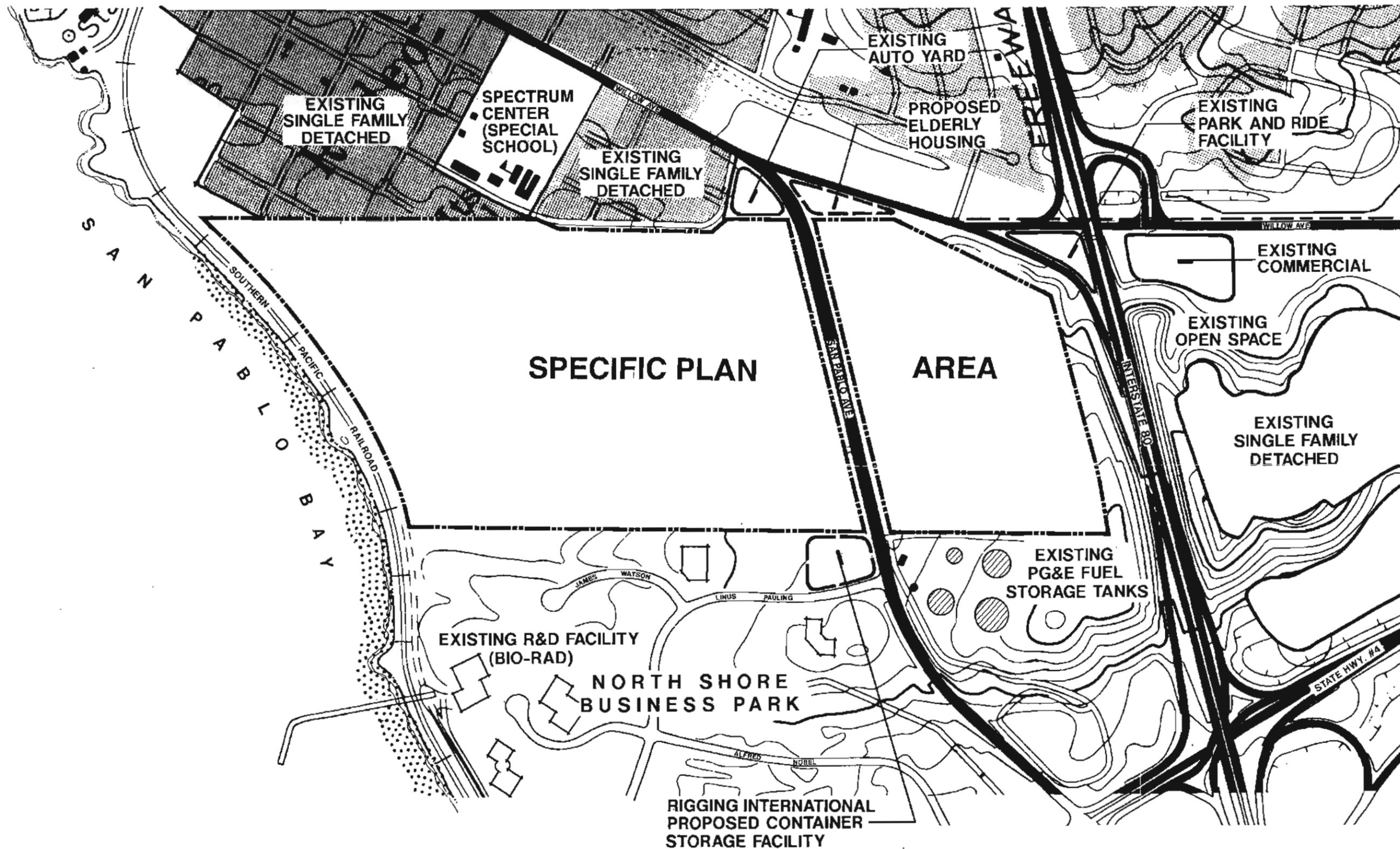
Southeast - The East Shore Freeway (Interstate 80) abuts the Specific Plan area to the southeast. The hilly lands across the freeway beyond to the southeast have been developed into low density single-family residential development in accordance with the City's Zoning.

Northeast - The New Pacific Properties Specific Plan area abuts the unincorporated community of Rodeo to the northeast. A fully developed, single-family residential community which also includes the former Garretson Heights School site, now leased to the Spectrum Center school for the handicapped, is located along the northwestern boundary. Northeast of San Pablo Avenue, the Specific Plan area abuts a Park-&-Ride Caltrans facility and a vacant, triangular shaped property, which has been approved for a senior housing project situated between the Specific Plan area and Willow Avenue in the community of Rodeo.

Southwest - The property to the southwest of the New Pacific Properties Specific Plan area and west of San Pablo Avenue has been approved for development into a master planned research and development industrial park known as the North Shore Business Park. Much of this property contains graded pads with tenants including Bio Rad Research Development complex, A&B Diecasting, Contra Costa County Social Services offices with a few remaining vacant pads. To the southeast of San Pablo Avenue, the adjacent property contains a fuel temporary storage facility owned by PG&E. Improvements include administrative buildings, pumping facilities and fuel storage tanks.



TOPOGRAPHY AND LAND USE MAP OF THE FORMER PACIFIC REFINERY
NEW PACIFIC PROPERTIES' SPECIFIC PLAN



4. Circulation, Traffic and Transit

Highways - Hercules is served by two major transportation routes: Interstate 80 (East Shore Freeway) and State Route 4 (John Muir Parkway). Access to the Specific Plan area is provided from two freeway exits. One is from the south at the I-80/SR-4 interchange, west to San Pablo Avenue and north to the site. The second access is from the I-80/Willow Avenue exit, west to San Pablo Avenue and south to the property.

Traffic - San Pablo Avenue is designated as a "Route of Regional Significance" by the Contra Costa Transportation Authority, with a recommended traffic service objective of LOS E at signalized intersections. LOS "Low" D has been established as the traffic service standards for Basic Routes (all other roadways) affecting the Specific Plan area. The proposed project has been designed to be consistent with these objectives. A preliminary traffic analysis concluded that the proposed land use plan results in significantly less average daily trips (ADT) than would be generated with build out under the current General Plan and Zoning industrial/commercial use designations.

Bus - Public transportation is offered throughout the community by several bus companies: Western Contra Costa County Transit Authority (WestCAT) and WestCAT Dial-A-Ride Service, Alameda-Contra Costa Transit (AC Transit), WestCAT Express, East Bay Airporter and Greyhound Bus Lines Service. The AC Transit line runs along San Pablo Avenue through the New Pacific Properties site. The BART Express line runs to Linus Pauling Avenue just a few hundred feet to the south of the site.

Rail - Bus service connects to the Bay Area Regional Transit (BART) rail system in Richmond at the El Cerrito Del Norte station located 9 miles southwest of Hercules. The BART system connects the City with Fremont to the south, Pleasanton/Livermore to the southeast, Concord to the east and San Francisco and Daly City across the Bay and to the south. The AMTRAK terminal is also located in Richmond and Martinez (15 miles). The Union Pacific and Santa Fe Burlington North Railroads provide rail freight service to the area. The Union Pacific railroad line runs along the west side of the New Pacific Properties holdings, forming the western boundary of the Specific Plan project site.

Air - Buchanan Field Airport is located in Concord, 16± miles to the east. Oakland International Airport is located 26± miles to the south, while the San Francisco International Airport is situated approximately 29± miles to the southwest.

5. Topography

The project is characterized by topography gently sloping towards San Pablo Bay. It slopes from southeast to northwest, ranging in elevation from 212' mean sea level (MSL) near the East Shore Freeway (I-80), to sea level at San Pablo Bay. Much of the project area has been graded to form terraces stepping down to the bay and accommodated the former refinery improvements. *Exhibit 6, Topography and Land Use Map of the Former Pacific Refinery*, shows the existing topography of the site and former Pacific Refinery improvements.

6. Geology, Soils and Seismic Safety

Geologically, the General Plan identifies alluvial deposits towards the northeast portion of the site; San Francisco Bay Estuary and Delta lowlands modern sediments (water saturated muds, peat and loose sands) toward the bay; and bedrock throughout the balance of the Specific Plan area. The sediment areas have a high-moderate damage susceptibility during earthquakes. Geological reconnaissance of the site noted that the entire site is dominated by sandstone of Miocene age with manmade earth fill deposits at several locations. Alluvial deposits consisting of sand, silt and clay were also noted. Soft clayey materials were noted in the north-northeast portion of the site.

A Preliminary Soils analysis of the Specific Plan area indicates that the site can provide safe conditions for residential development provided property mitigation measures for seismic hazards are taken.

The site is not in an Alquist-Priolo Earthquake Fault Zone (previously Alquist-Priolo Special Studies Zone) and no known active faults traverse the site. However, the site is in close proximity to several active faults, and a major seismic event on any of the nearby Bay Area faults could cause strong to severe ground shaking and possible liquefaction at the site.

According to the 1995 California Building Code, the site lies within Seismic Zone 4. A detailed geologic field reconnaissance was conducted in 1971 as part of the City's General Plan, which determined that although all of San Francisco Bay area is considered seismically active, the City is relatively safe from seismic dangers. However, because the site falls within 15 kilometers of a type A fault (as identified in the Uniform Building Code), near fault issues will be addressed in seismic design of structures. Although the General Plan identifies some locations within the site as being subject to possible liquefaction, field investigation by the Soils Engineer indicates mostly cohesive materials at the site.

7. Slopes/Erosion

The site has an average slope of 4.78% over the entire property. The majority of the Specific Plan area southwest of San Pablo Road has been terraced to accommodate the former Pacific Refinery improvements. The northeastern portion of the Specific Plan area northeast of San Pablo Avenue is slightly steeper, with an average slope of 6.59%.

8. Wind/Climatic Influences

Prevailing wind direction is from the south/south west, from San Pablo Bay onto the site. The mean hourly wind speed is 7 miles per hour. Temperatures average from 49.3°F in January, to 61.6°F in July, with an average yearly mean temperature of 57.1°F.

9. Biological Resources

The New Pacific Properties Specific Plan area does not support a diverse biological environment since the majority of the site has been historically in heavy industrial and pasture use. Wetlands have been identified and delineated in a small portion of the Specific Plan area, as discussed below. A large stand of eucalyptus trees run along the northern boundary of the Specific Plan area.

Focused Biological surveys have been conducted, with sensitive vegetation found in the western portion of the site near the bay shoreline, in an area slated for open space on the Land Use Concept plan. Additional surveys of rare plants have been conducted in conjunction with the project environmental review.

10. Wetlands

There have been 0.97 acres of wetlands and 0.03 acres of jurisdictional waters (as defined by the U.S. Army Corps of Engineers) identified and delineated on the project site. The wetlands are fed by irrigation water, and drainage from the local catchment and are located in a small depression along the southwestern property boundary and along an ephemeral drainage course nearby. These areas are shown on *Exhibit 8, Opportunities and Constraints*, provided later in this chapter.

11. Flooding

The New Pacific Properties site is not subject to flooding. It falls within Zone C, defined as areas of minimal flooding, as depicted on the Federal Flood Insurance Rate Map.

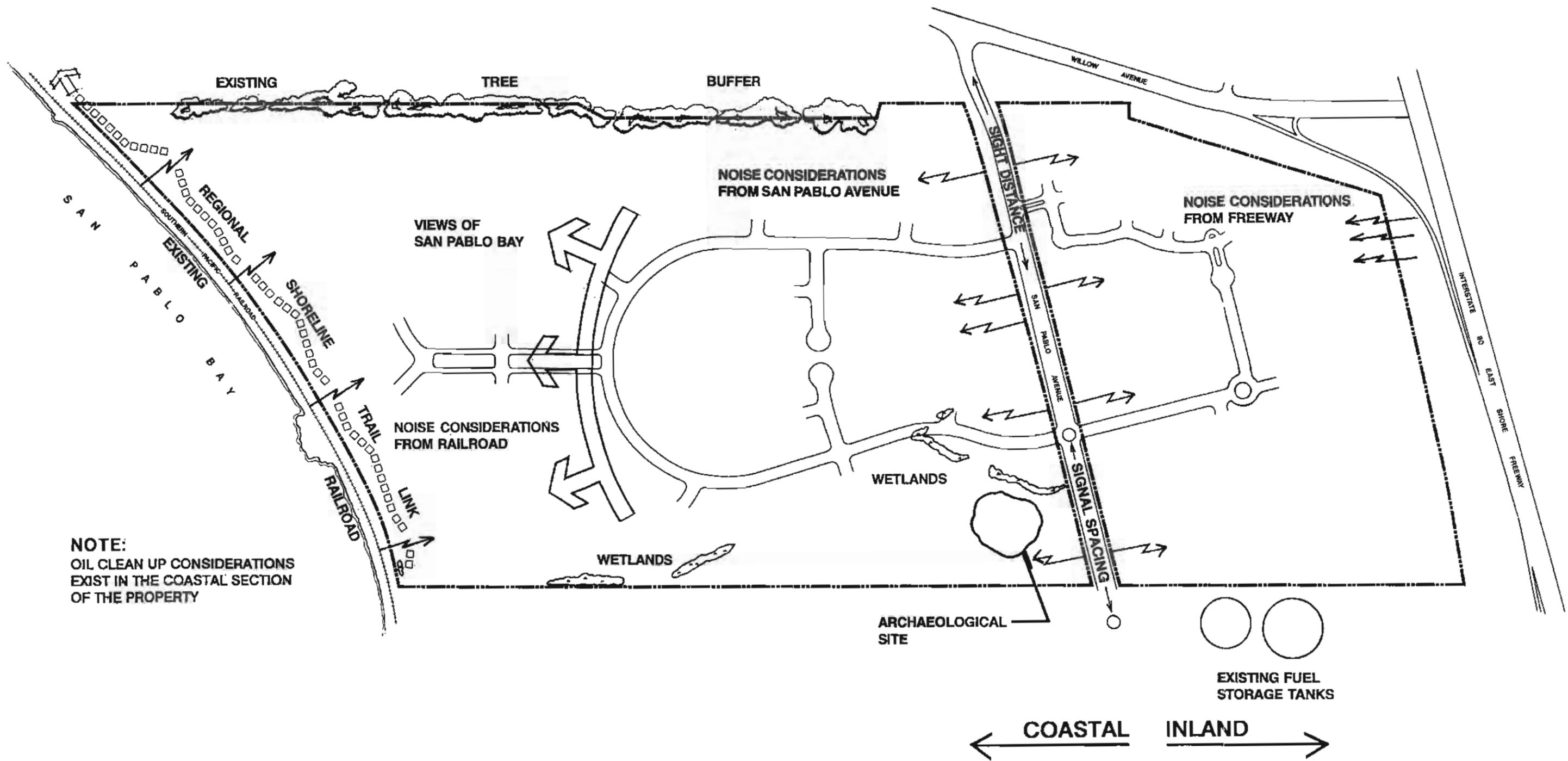
12. Cultural Resources

Archaeological Resources - A preliminary archaeological investigation of the site determined that development of the proposed project could result in impacts to one potentially significant archaeological site located on the property. This site has the potential to yield important information regarding early bay/estuary adaptations by Native Americans. Project implementation for this area will proceed pursuant to CEQA's archaeological mitigation requirements. The archaeologically sensitive area is noted on *Exhibit 8, Opportunities and Constraints*.

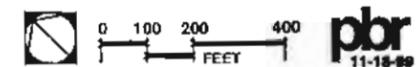
Paleontological Resources - Excavation could potentially impact fossils contained in the San Pablo and Montezuma formations. However, recovery of fossil specimens and associated geologic data would mitigate this impact. Project implementation will proceed pursuant to CEQA's paleontological mitigation requirements.

13. Noise

A preliminary noise analysis has been conducted in conjunction with preparation of the Specific Plan. Both rail and freeway noise were evaluated. The Specific Plan area falls within existing and projected Day-Night Noise Levels (Ldn) of 60 to 70 dBA. Those areas over 65 dBA represent narrow bands in three areas of the Specific Plan: along the East Shore Freeway (Interstate 80) freeway at the east end of the Specific Plan area; along the Union Pacific Railroad which runs along the western boundary of the New Pacific Properties site; and along San Pablo Avenue, which cuts through the Specific Plan area. The majority of the proposed project falls under the 60-65 dBA range, which is deemed normally or conditionally acceptable for the uses proposed, with appropriate mitigation.



OPPORTUNITIES AND CONSTRAINTS
NEW PACIFIC PROPERTIES SPECIFIC PLAN



Again, it is the intent of the Specific Plan to be flexible in terms of the actual product type, location of product and specific acreage as long as the overall project density is not exceeded, so that the acreage numbers provided above are not fixed or absolute, but rather conceptual and adjustable.

1. P1 Residential

This designation consists of 31.9 acres occupying the northwestern portion of the site. This Land Use designation is intended to provide single-family detached residences on large sized lots. The P1 residential neighborhood has been designed to maximize view opportunities, create inviting streetscapes, and provide excellent residential access to project amenities, including the Shoreline Trail/Park, Neighborhood Park and formal Linear Park areas. A density of 2.0-8.0 units per acre is envisioned, with a minimum lot size of 6,000 square feet.

2. P2 Residential

The P2 area makes up 14.6 acres of the overall project site, and is located on the west side of San Pablo Avenue, in the heart of the project area. It is located in the center of the Coastal side of the property, in a semicircular configuration with excellent pedestrian access opportunities to the Community Trail Park and Enhanced Parkway. Much of the P2 neighborhood is designed to offer views of the bay. The Community Trail Park serves as the focal point and major amenity for the P2 development area. A density of 2.0-9.0 units per acre is envisioned, with a minimum lot size of 5,500 square feet.

3. P3 Residential

The P3 Residential designation allows single-family dwellings on 5,000 square foot lots along the southwest boarder of the project area. The P3 neighborhood also takes advantage of the bay area view opportunities as well as utilizing the adjacent wetland area as an attractive passive open space element. Excellent pedestrian access to the Shoreline Trail/Park, Neighborhood Park, formal Linear Park and project trail system is also provided. A density of 2.0-9.0 units per acre is envisioned over the 25.2 acres forming the P3 area. An area has been designated within the P3 area to allow for the possible development of an on-site wastewater treatment plant.

4. P4 Residential

The P4 designation consists of 22.3 acres. The P4 neighborhood is bordered by the P1 and P2 residential areas to the west, San Pablo Avenue to the east, the community of Rodeo to the north, and P2 and P8 residential neighborhoods to the south. The Enhanced Parkway bisects the P4 area in an east/west direction. Excellent pedestrian access to the Enhanced Parkway and overall trail system is provided, and to the extent possible, views of San Pablo bay are provided along the western edge of the neighborhood. In addition to maintenance of the existing tree buffer long the northeast boundary, short cul-de-sacs have been incorporated into this neighborhood, which create architectural relief to the project when viewed from the community of Rodeo. A density of up to 13.0 units per acre and a minimum lot size of 4,000 square feet are proposed.

5. P5 Residential

The P5 designation allows for single-family residential dwelling units on lots of 3,375 square feet. Located on the eastern side of San Pablo Avenue, and comprised of 17.3 acres, the P5 neighborhood provides excellent pedestrian access to the project school via the project's Enhanced Parkway. Attractive landscape entry treatment at San Pablo Avenue tied to an aesthetically pleasing and functional roundabout design element also reinforce the high quality cohesive community design program for the project. A density of up to 13.0 units per acre is proposed.

6. P6 Residential

This designation is made up of 7.3 acres located in the central eastern region of the inland side of the Specific Plan area, bordering the Interstate 80 East Shore Freeway. The large areas of open space to the northeast provide an appropriate separation from areas to the north, while at the same time offer view opportunities of the school, park and areas to the north and east. Conveniently located within walking distance of the school and the retail site, this neighborhood will provide excellent opportunities for community interaction via the Enhanced Parkway. The P6 area includes a unique "Two-Pack" residential product which locates two dwellings in close proximity to each other in a manner reflective of traditional neighborhood design. This concept reduces the previously unusable space in one side yard while redistributing it to the adjacent property, and maximizes square footage and usability along the expanded side yard. This allows for a higher density, yet maintains setbacks, maximizes yard space, and minimizes garage scape. Varying setbacks from the curb create a diverse streetscape and maintain the detached status of each home. The P6 area will have a density of up to 15.0 units per acre with a minimum lot size of 3,037square feet.

7. P7 Residential

The P7 Residential designation is located in the northern region of the inland section of the site, bordered to the west by San Pablo Avenue, to the north by sloped open space and the Community of Rodeo beyond, the project school to the south, and P6 residential area to the southeast. The P7 neighborhood provides excellent access to the school and park, with many lots located adjacent to the School Facility area. It is comprised of 9.8 acres, with a density of up to 17.0 units per acre and a minimum lot size of approximately 2,668 square feet.

8. P8 Residential

The P8 designation is the only multi-family residential component of the project, and may offer townhouses, condominiums or apartments at a density of 12.0-30.0 units per acre. However, should the market indicate that the need for another product type is stronger than the P8 multi-family residential product, the Specific Plan allows for other product types at a lower density to be designed in the P8 area, provided that the overall density for the entire New Pacific Properties Specific Plan area is not exceeded. The 7.8-acre site is located in the center of the project, along the southern boundary, immediately west of San Pablo Avenue. Its elevated location provides excellent view opportunities of San Pablo bay along the

western boundary of the area. With the Enhanced Parkway forming the northern boundary of the P8 area, this location also offers convenient pedestrian access to all amenities of the Specific Plan area via the overall project trail system.

9. Retail/Residential Flex

The Retail/Residential Flex land use designation covers a 6.0-acre portion of the site and is located to the east of San Pablo Avenue. It is bounded by the Wickland fuel temporary storage facility to the south, San Pablo Avenue to the northwest, the project school to the northeast, and the P5 neighborhood to the west. It will accommodate either a mix of commercial uses or single-family residential uses, depending on the market demands and timing of implementation. If the residential option is pursued, site design and product types shall be consistent with the surrounding development and shall be complementary to the overall community design.

The commercial uses in this planning area are to be arranged as a unified development, taking the form of a community shopping center. Uses in this area could include retail businesses or other customer-oriented operations with an FAR of 0.25. Potential retail venues include a convenience store, a food outlet, neighborhood restaurant, and small service businesses such as a dry cleaners, hardware/garden store, photo shop, barber shop, etc.

10. School Facility

A 7.5 acre site within the project area will accommodate an elementary school facility. The school site will include a variety of active park elements including, but not limited to, multi-use grass fields, a soccer field, athletic courts, and baseball diamonds. These park elements will serve as a field/playground for the students of the proposed school during school hours, but will also be accessible to residents of the New Pacific Properties Specific Plan area and other residents of the City at large. *Exhibit 37, Conceptual School Facility Landscape Plan* depicts this concept.

11. Open Space, Conservation and Parks

The Conceptual Open Space and Recreation Plan delineates a cohesive system of open space, conservation and park elements for the Specific Plan project area. The active and passive recreational facilities, pedestrian trail and bikeway network, landscaped slopes, and enhanced parkways, open space buffers and conservation areas are integral components of this master planned community. These amenities serve to create a comprehensive landscape design theme that will provide identity for the new community, as well as a variety of recreational opportunities for all City residents.

Additionally, the proposed parks will serve to satisfy the City's park requirement. The residential component of this project requires 13.8 acres of parkland. This plan provides 14.3 acres of parkland, which exceeds the City's park requirement for this project area.

The following is a description of each of the proposed open space, conservation and park components of the Specific Plan project area. Note that the various park areas are named

solely for the purpose of being able to distinguish between the different open space and park areas. Actual names for each of the parks will be determined as the project nears implementation.

a. Shoreline Trail/Park

A 9.5-acre area of shoreline along San Pablo Bay east of the Union Pacific Railroad right-of-way is planned as a Shoreline Trail/Park. The park will contain a meandering pedestrian/bicycle trail running north/south through the park, which will serve as a critical link in the regional bayshore trail system. The trail will allow for a connection to the existing Lone Tree Park in the community of Rodeo to the north and will extend to the southern border of the property, with connection anticipated to a future shoreline trail to the south. The Shoreline Trail/Park is also envisioned as a unifying element connecting to other parks, open space and community trails throughout the Specific Plan area. The East Bay Regional Parks District (EBRPD) has specified that this trail is to be 10 feet wide, paved in asphalt with 2-foot decomposed granite shoulders on each side and fenced from the Union Pacific Railroad tracks to the west. Because this shoreline area has been blighted significantly in conjunction with the former Pacific Refinery operations, grading is proposed within the area to restore the existing degraded appearance, and provide a safe and attractive setting. Trails also will be accessible for emergency and maintenance vehicles, as necessary. *Exhibits 38 and 39, Conceptual Shoreline Trail/Park Landscape Plans (North and South)*, reflect the vision for this bayshore area.

b. Neighborhood Park

The New Pacific Properties Specific Plan will provide a 1.6-acre Neighborhood Park placed in the coastal section of the property, centrally located adjacent to the Shoreline Trail/Park. This park will contain improved trails connecting the shoreline trail system to the more formal Linear Park, which ultimately connects to the Enhanced Parkway and central Community Trail Park beyond. The Neighborhood Park may include various elements typically found in a small neighborhood park such as open turf areas, shade trees, picnic areas, tot lots, bicycle and pedestrian pathways, and other uses commonly associated with park spaces, as shown in *Exhibit 40, Conceptual Neighborhood Park Landscape Plan*. Structures could include a gazebo, picnic tables, barbeques, etc.

c. Linear Park

The Linear Park is located on the coastal portion of the New Pacific Properties Specific Plan area, and extends in an west/east direction connecting the Shoreline Trail/Park and Neighborhood Park along San Pablo Bay to the center terminus of the loop roadway/Enhanced Parkway beyond. The ±0.9-acre Linear Park is envisioned as a unique neighborhood element of the project. The park could include formal landscape treatment, heavily planted shade trees and turf areas. (See *Exhibit 41, Conceptual Linear Park Landscape Plan*.) One-way streets surrounding the Linear Park, with residential units fronting the street, will help to create a pleasing pedestrian friendly streetscene.

d. Community Trail Park

The Community Trail Park covers 2.3 acres and will create a similar experience to the Linear Trail, being linear in form and serving as a functional and yet aesthetically pleasing community design element. However, it is envisioned as being more informal in terms of design and landscaping. A meandering pedestrian path is conceptually planned, with shade trees and landscaped activity nodes. The Community Trail Park will widen to a more formal semi-circular entry with perhaps a monument feature where it meets the Enhanced Parkway. Cul-de-sacs will open to the Community Trail Park at regular intervals, providing both physical and visual connection to the park. (See *Exhibit 42, Conceptual Community Trail Park Landscape Plan.*)

e. Natural Open Space

The Natural Open Space area covers approximately 3.6 acres of the New Pacific Properties site and includes wetlands in the southwestern portion of the Specific Plan area. The existing trees and vegetation in these areas will be enhanced and reflect a non-manicured natural state. *Exhibit 43, Conceptual Natural Open Space Landscape Plan*, identifies a conceptual design approach for this element of the plan, which will add to the scenic quality of the site.

f. Enhanced Parkway

The Enhanced Parkway open space is comprised of 9.0 acres of improved pedestrian paths set in 12 to 25 feet wide landscape sections along either side of the project's loop roadway. See *Exhibit 29, Loop Roadway/Enhanced Parkway Streetscape Plan*. The Enhanced Parkway connects with other trails, residential areas and community elements, such as parks, the school site, and possible retail area, as well as the sidewalk path along San Pablo Avenue. The Enhanced Parkway will also serve as an important entry statement and community design element throughout the New Pacific Properties Specific Plan area, and will set the tone for this new community as one drives and walks throughout the project.

g. Pedestrian/Bike Trail System

The New Pacific Properties Pedestrian/Bike Trail System serves as an important urban design element to the project, tying neighborhoods and community elements together functionally, socially and aesthetically. Several trails proposed within the New Pacific Properties Specific Plan area also will be incorporated into the broader existing system of trails of the East Bay Regional Park District and the City of Hercules. In more detail:

- The 9.5-acre Shoreline Trail/Park will provide a pedestrian/bicycle trail system connecting the district's existing San Francisco Bay Trail at Lone Tree Park in the Community of Rodeo to the southwestern border of the project.
- The Neighborhood Park trail connects directly to the Shoreline Trail/Park on the west and the Linear Park on the east.
- As discussed above, the 0.9-acre Linear Park connects to the Neighborhood Park in the west and the Enhanced Parkway and Community Trail Park in the east.

- The Community Trail Park provides linkages to the interior of the Specific Plan area.
- The bicycle trail system is envisioned within the entire length of the loop collector roadway, while a pedestrian trail runs along the length of the abutting Enhanced Parkway, both of which loop through the entire center of both sides of the project.
- The loop collector bicycle system will also connect to the bikelane proposed along both sides of San Pablo Avenue.

Each neighborhood has been designed to provide convenient, safe and attractive pedestrian connections to the trail system and project parks and open space elements of the project as shown in *Exhibit 11, Conceptual Open Space and Recreation Plan*, providing excellent linkage not only with the project, but also to the surrounding neighborhoods and communities of Hercules and Rodeo as well.

h. Other Open Space

The New Pacific Properties remaining open space is comprised of interior and exterior slopes and perimeter open space areas primarily found along the Specific Plan's property boundary. Interior slopes will be landscaped to prevent erosion and provide a visual relief within the project. The exterior slopes are envisioned to be left in their current condition providing a high quality aesthetic appearance, to stabilize slopes and minimize erosion, and to provide a visual screen or buffer from surrounding properties. A large stand of existing eucalyptus trees which is located along the Specific Plan's northern boundary will be largely retained to help maintain the visual screen between the community of Rodeo and the Specific Plan area.

B. CIRCULATION PLAN

1. Circulation System

The New Pacific Properties Circulation Plan is a comprehensive roadway, bicycle and pedestrian network which maximizes access between residential areas, potential retail uses and community facilities, while minimizing travel distance and reliance on the automobile. The Circulation Plan has been designed with the following criteria in mind:

- Provide even distribution for both vehicular and non-vehicular users.
- Take advantage of access opportunities from existing San Pablo Avenue.
- Provide an efficient roadway system.
- Allow for good overall access to all development parcels.
- Serve as the framework for strong community design.

The Circulation Plan consists of: a four-lane divided roadway (San Pablo Avenue enhancements), Collector Streets (Enhanced Parkway/loop roadway), Minor Streets, One-Way Minor streets, Cul-de-Sacs, as well as bicycle and pedestrian ways.

It should be noted as well, that the maximum noise level experienced along the eastern freeway boundary while conducting noise measurements was 66 Ldn. The potential for noise levels as high as 85 dBA exists within the Retail/Residential Flex site, as generated by large commercial delivery trucks. Appropriate mitigation measures can bring this down to an acceptable level.

14. Air Quality

Air quality is regulated by the Bay Area Air Quality Management District (BAAQMD) and the California Air Resources Board. New businesses are not allowed to cause an increase in air pollutant emissions. Pacific Refining Company generated air emissions which were permitted by the Bay Area Quality Management District (BAAQMD). The proposed residential development and retail/residential flex site are anticipated to result in an improvement of air quality and reduction in air pollutant emissions compared to the previous Pacific Refinery use, and proposed City of Hercules General Plan designation of Industrial. All development will be designed in accordance with the Bay Area Air Quality Management District standards and requirements.

15. Water Quality

It is anticipated that water quality on the site would improve in conjunction with closure of the Pacific Refinery, dismantling of the refinery improvements and infrastructure, and construction of the proposed New Pacific Properties residential development. The property has two existing drainage outfalls into San Pablo Bay which may continue to be used by New Pacific Property project, and also has non-point stormwater flows from the non-refinery areas of the property. All plans and construction will be in compliance with California Regional Water Quality Control Board permit requirements.

16. Hazardous Wastes

The Pacific Refining Company generated hazardous waste amounting to 57% of the City's hazardous waste shipped out of the City in 1988. The company also generated effluents which were discharged directly into San Pablo Bay under their National Pollutant Discharge Elimination System Permit (NPDES), and other effluents treated at the on-site sewage treatment plant. Generation of these wastes will be eliminated or significantly reduced with dismantling of the refinery. Although the New Pacific Properties Specific Plan area is *not* listed as a known contaminated site in the City of Hercules General Plan, past operations resulted in some areas of petroleum-impacted soil and groundwater in the northwestern portion of the Specific Plan area. A remediation plan will address remediation of contaminated soil, and analyzed ground water and soils. A decommissioning and dismantling plan for the refinery was approved by the City, dismantling of the refinery improvements is underway, and mitigation measures are being implemented in accordance with that plan.

17. Fire Hazards

The Pacific Refinery plant provided its own private fire fighting facilities within the plant site. This facility is in the process of being dismantled along with other refinery improvements.

The Specific Plan area falls within the Rodeo-Hercules Fire District. The fire district maintains two stations: Station 75, the closest to the New Pacific Properties site, is located approximately 1.5± miles to the north in Rodeo on Third Street; while Station 76 is located in Hercules on Refugio Valley Road. In addition, a new station is planned to service properties within the City's Sphere of Influence. Response time to the site is adequate from both existing facilities. The Specific Plan east of San Pablo Avenue also abuts existing PG&E fuel temporary storage tanks, which are situated 50' from the southwestern boundary of the New Pacific Properties property boundary. The Rodeo-Hercules Fire District recommends a 100-foot setback from the tanks to structures.

18. Water and Sewer

Water - The project site is serviced by the East Bay Municipal Utility District (EBMUD). The Pacific Refinery was serviced by an 8" domestic waterline from the direction of the community of Rodeo via Garretson Avenue. In addition, 24" and 36" steel aqueducts run through the southeastern portion of the site within a 20' easement. Water is available in the immediate vicinity of the site.

The New Pacific Properties Specific Plan area falls within the East Bay Municipal Utility District (District) Maloney Pressure Zone. Service is provided up to an elevation of 200 feet above sea level in this zone. In areas over the 200-foot elevation, water service generally is provided by constructing water reservoirs with sufficient capacity to accommodate both normal and emergency water requirements. The New Pacific Properties Specific Plan area falls under the 200-foot elevation except for roughly 130' into the extreme southeast corner of the property, where no development is proposed.

Sewer - The Pacific Refinery had its own wastewater treatment facility. An existing outfall for treated wastewaters, extending 2,000 feet into the Bay at a depth of 15 feet, continues to be maintained.

The New Pacific Properties Specific Plan area has several options available for treatment of wastewater. The most desirable involves connection to the City's existing wastewater treatment facility located in the City of Hercules. Other options include creation of a new treatment plant to be located elsewhere on the property, connection to Hercules/Pinole wastewater treatment facility, connection to a new Joint Power's Authority facility contemplated in the unincorporated community of Rodeo to the north, or connection to the Rodeo Sanitation District.

19. Storm Drainage System

The Specific Plan area currently experiences two general drainage patterns. Storm waters are funneled to two existing storm drain outfalls which currently discharge storm water from the project site into San Pablo Bay: one located approximately 1/3 distance from the northern boundary of the property, and the second just off-site at the extreme southwest corner of the New Pacific Properties Specific Plan area. The portion of the property east of San Pablo Avenue (Inland) drains towards San Pablo Bay, under San Pablo Avenue, and follows a small drainage course running along the southwestern boundary of the property. This is the same drainage course identified in the Wetlands section described earlier in this chapter, which eventually exits onto the southernly adjacent property and empties into San Pablo Bay. A portion of the site west of San Pablo Avenue (Coastal) also drains to this same outlet. Other portions of the Coastal Specific Plan area drain to the northwest, cross under the Union Pacific railroad tracks and empty into San Pablo Bay. A new outfall to San Pablo Bay may be created in the southwestern portion of the site in conjunction with development of the Specific Plan, to replace existing stormwater conveyance facilities in order to avoid impacts to nearby wetlands.

20. Energy Resources and Utilities

The New Pacific Properties site currently is serviced by Pacific Gas and Electric Company for gas and electrical service, and Pacific Bell Company for telephone service. Existing utilities are adequate to service the Specific Plan proposed land uses.

Several utility easements affect the New Pacific Properties site, the most significant being:

- a. Two P.G.&E. Easements running within the project along the I-80 freeway and northeastern (Rodeo) property boundaries. These easements accommodate a 60 kv electrical line which provided transmission service to a New Pacific Properties owned substation. Power from the substation was distributed to service the former refinery's electrical needs. It is anticipated that these easements will be abandoned.
- b. A 10' wide TOSCO easement which runs parallel to the I-80 freeway boundary, approximately 260' west of the easterly project boundary. This easement is used to transport oil from the TOSCO terminal in Richmond to the TOSCO refinery in Rodeo.
- c. A 20' wide East Bay Municipal Utility District easement, again running parallel the I-80 freeway boundary, approximately 280' to the west of the easterly project boundary. This easement covers a 24" water aqueduct and a 36" water aqueduct called the "Crockett Aqueduct" which serves the community of Crockett to the south.
- d. An approximately 25' wide Hercules Powder Company Easement which along the western project boundary, abutting the Union Pacific Railroad property.

It is anticipated that the TOSCO and East Bay Municipal Utility District lines will be relocated to facilitate development.

21. Parks, Open Space and Conservation

Existing Facilities - The City of Hercules has one community park, four neighborhood parks, and two mini-parks offering ballfields, tot lots, large grass play fields, picnic areas, tennis courts, jogging and bike trails. Two entities influence park requirements and

standards potentially affecting the subject property and any development proposed thereon: the City of Hercules, and the East Bay Regional Park District.

City of Hercules - The City of Hercules bases its parks and open space requirements on projected population of the proposed project. Based on a population multiplier of 3.12 persons per dwelling unit, the Specific Plan's 880 dwelling units would result in an estimated population of 2,746 people for the New Pacific Properties project. The City then applies the following standards based on that population:

- 34 acres/1,000 population for open space (private and public City-wide)
- 3.25 acres/1,000 population for community parks
- 1.75 acres/1,000 population for neighborhood parks

It should also be noted that the City of Hercules General Plan designates a "local" trail roughly approximating the alignment of San Pablo Avenue, which corresponds to the route shown on the county's Bicycle Path Plan. A "regional hiking trail" is also shown along the bay shoreline, and is described in the following section.

East Bay Regional Park District - The East Bay Regional Park District (EBRPD) was formed in 1934 by a group of citizens hoping to preserve some of the region's unique natural resources, with the goal of providing regional recreational opportunities and preserving open space resources for the population of Alameda and Contra Costa Counties. In 1987, a plan for the "Bay Trail" was developed, to include a continuous recreation 400 mile corridor with bicycling and hiking trails around the perimeter of San Francisco and San Pablo bays. The EBRPD has published a Preliminary Regional Trail Plan which proposes a bicycle/hiking trail along the shoreline of the New Pacific Properties site.

Specific Plan Parks - The New Pacific Properties Land Use plan provides 57.1 acres of open space comprised of: 1.6-acre Neighborhood Park; 0.9-acre Linear Park; 2.3-acre Community Trail Park; 9.5-acre Shoreline Trail Park which will connect to the region's Bay Trail to the north; 3.6 acres of Natural Open Space; 9.0 acres of Enhanced Parkway along with 4.1 acres of land occupied by roads and 25.6 acres of associated open space. These facilities exceed the minimum park requirements of the City of Hercules, and are in compliance with the East Bay Regional Park District 1997 Master Plan..

22. Schools

The New Pacific Properties site falls within the West Contra Costa Unified School District, which includes numerous elementary, middle and high schools. Private elementary and secondary schools are also available in adjacent communities. Contra Costa College is located approximately 6.5± miles southwest from Hercules in San Pablo. The University of California at Berkeley is located approximately 15± miles to the south, while Stanford University, the University of San Francisco and San Francisco State University are located in and around San Francisco, a short distance to the southwest.

Based on student generation rates provided by the West Contra Costa Unified School District, it is estimated that approximately 685 students will be generated by the proposed residential portion of the project as shown in the charts below. A 7.5-acre site for an

elementary school is proposed within the New Pacific Properties project on the east side of San Pablo Avenue.

Single-Family

| Grade Level | | Student Generation Rate | Student Yield |
|----------------|---|-------------------------|---------------|
| K-6 | x | 0.392 | 299.09 |
| 7-8 | x | 0.085 | 64.85 |
| 9-12 | x | 0.226 | 172.43 |
| Total Students | | | 536.37 |

Multi-Family

| Grade Level | | Student Generation Rate | Student Yield |
|----------------|---|-------------------------|---------------|
| K-6 | x | 0.222 | 25.97 |
| 7-8 | x | 0.036 | 4.21 |
| 9-12 | x | 0.075 | 8.77 |
| Total Students | | | 38.95 |

23. Scenic and Aesthetics Resources

The Specific Plan area was degraded from years of oil refinery use. However, the Specific Plan area offers extraordinary views of San Pablo Bay. Additionally, the Specific Plan area's gentle sloping topography, existing tree clusters along the northern boarder, and opportunities for incorporating some existing drainage areas (portions of the wetlands along the southern boundary of the project) into project design represent significant visual quality/aesthetic enhancement opportunities.

24. Agriculture

The City of Hercules does not include any land used for agricultural purposes. As such, there will be no loss of high quality soils and cropland of significant economic importance with development of the Specific Plan area. According to the General Plan, agriculture is not a significant economic base within the City. Based on 1990 projections, only 0.5% of the population is employed in the agriculture industry.

C. PLANNING OPPORTUNITIES AND CONSTRAINTS

Based on the above discussed existing conditions, *Exhibit 8, Opportunities and Constraints*, was prepared to assist in preliminary layout and design of the Land Use Concept plan. These

opportunities and constraints also have the potential to affect development within the project. The map reflects the following opportunities and constraints:

- Union Pacific Railroad - Located along the western boundary of the New Pacific Properties Specific Plan area, the railroad will generate intermittent noise and affect access and views to San Pablo Bay and its shoreline.
- Wetlands - Located primarily along the southwestern boundary of the Specific Plan area west of San Pablo Road, these areas could be preserved where feasible, development sited so as not to detrimentally impact the wetlands, and any loss of wetland mitigated on site.
- Archaeological Site - Although further testing of the known archaeological site will be needed to determine the significance and level of mitigation which may be necessary to allow development to occur, the site has been identified for information purposes.
- Existing Tree Buffer - Spanning the northwestern Specific Plan area boundary, a stand of eucalyptus trees separates, screens and buffers the existing community of Rodeo from the New Pacific Properties area.
- San Pablo Bay Views - Almost the entire New Pacific Properties Specific Plan area affords exceptional views of the bay.
- Regional Shoreline Trail Link - The entire western boundary of the Specific Plan area provides an excellent opportunity for expansion of and connection to the Bay Trail, a regional pedestrian and bicycle trail proposed to run along the entire San Francisco Bay shoreline. It also provides excellent passive recreational opportunities. Implementation of the Specific Plan will offer connection opportunities to this trail system while none previously existed.
- East Shore Freeway (Interstate 80) - Although the New Pacific Properties Specific Plan area abuts this major transportation corridor the site is considerably higher than the freeway where it abuts such. There are limited visual opportunities for commercial exposure and development along the freeway. Possible freeway noise influences should also be considered.
- Signal Spacing - Access to the Specific Plan site will be limited by two public works standards: minimum spacing requirements must be met from the existing traffic signal on San Pablo Avenue when approaching from the north and the proposed signal at Linus Pauling Avenue when approaching from the south; and horizontal sight distances must be met when approaching the site.
- PG&E Fuel Temporary Storage Tanks - Two PG&E fuel temporary storage tanks are situated approximately 50' to the southeast of the New Pacific Properties site, east of San Pablo Road. The Fire Marshall of the Rodeo/Hercules Fire Department recommends that a 100' building setback from the tanks to residential structures be maintained.
- Oil Cleanup Considerations - The land use concept for the property has been designed to reflect the City approved decommissioning and dismantling plan for the Pacific Refinery. The Specific Plan will be consistent with established Regional Water Quality Control Board (RWQCB) standards for remediation, which are protective of human health for residential, school, commercial and park land uses, as well as protective of the environment including the bay front ecology.

D. MARKET TRENDS AND OVERVIEW

Hercules owes its popularity and phenomenal growth rate to its convenient location on the East Bay just 25 miles northeast of San Francisco. It is one of the fastest growing cities in the Northern California area. In 1950, the population of Hercules was only 344. It became the fastest growing City in the State in late 1970, and by 1980 it had grown to 5,963. By the late 1980s, Hercules had experienced a 15% growth rate while the remainder of the Bay area was

experiencing a 1.4 % growth rate, and the County a 2.5% growth rate. By 1990 the population had risen to 16,892. According to the General Plan, today the population is 19,400. The growth rate is projected to slow slightly to 11%-13%, yet continue to grow at a rate considerably higher than the remainder of the County.

Hercules is comprised of a large number of moderate to above moderate-income young families. Households in Hercules tend to average 3.1 persons, compared to an average of 2.6 persons for the County. The larger household size is attributable to the predominance of detached single-family homes, typically with three and four bedrooms, with over 50% of the households comprised of households with children. According to a market report for the project area, the population within a 2 mile radius tends to be younger than the surrounding communities, with 20.4% being between the ages of 35-44, 14.2% from ages 45-54, and 14.1% from ages 25-34. In 1990, household incomes within a 2-mile radius of the site averaged \$65,227, with a median household income of \$60,000 compared to \$49,000 for Contra Costa County. The higher income levels were attributed to dual-income households, as well as easier access to higher paying employment opportunities in San Francisco.

The General Plan's Economic element states that a primary goal of the City is to "generate increased revenue and business activity while maintaining Hercules' special quality of life." Contra Costa County and the City of Hercules are expected to gain a significant number of new jobs in the next decade with expansion of existing employment entities such as Bio-Rad, and development of new business in such planned commercial projects as the North Shore Business Park and Penterra. Nevertheless, the net outflow of employed residents compared to local jobs will remain substantial. Market analysis conducted for the project area concluded that:

- Residential growth in Hercules will have a major beneficial benefit as existing retail will be bolstered by additional local market support as residential development occurs.
- The addition of homes to the residential base will result in existing retail garnering additional retail sales.
- Commercial office, industrial and retail development will be hard pressed for tenancy in the New Pacific Properties Specific Plan area.
- Removal of the Pacific Refinery creates a much more conducive environment for residential and to some extent to office and R&D locational decisions.
- There is no office market need because these uses are being accommodated in adjacent R&D buildings and retail space.

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- There is no office market need because these uses are being accommodated in adjacent R&D buildings and retail space.

III. SPECIFIC PLAN ELEMENTS

A. LAND USE

The proposed new community for the New Pacific Properties Specific Plan will represent a significant change from its current industrial use. The Land Use plan provides an overall vision and guide for the ultimate development of the site. The design approach takes advantage of the site's gentle slope towards San Pablo Bay, and maximizes view opportunities throughout. The plan proposes to create a blend of residential product types, retail uses and open spaces in a pedestrian friendly setting. Key elements of the plan include residential neighborhoods, a retail center, a public school site, enhanced parkways, neighborhood parks, open space areas and a shoreline regional park and trail providing a critical link to the Bay Trail. Traditional neighborhood design elements help to create strong community identification, and an extensive on-site trail system will encourage pedestrian use and community interaction.

The Land Use plan provides an illustration of the proposed land plan which includes development of single-family detached homes in seven land use categories: P1 (minimum 6,000 square-foot lots), P2 (minimum 5,500 square-foot lots), P3 (minimum 5,000 square-foot lots), P4 (minimum 4,000 square-foot lots), P5 (minimum 3,375 square-foot lots), P6 (minimum 3,037 square-foot lots) and P7 (minimum 2,668 square-foot lots). A total of 763 detached single-family units could be realized. An additional 117± attached units are proposed as a multi-family residential product P8, at a density of 12-30 units per acre, over 7.8 acres, although redesign of up to 45 single-family detached units at a lower density is considered a possibility. A 6.0-acre retail/residential flex site is proposed along San Pablo Avenue, at the perimeter of the project, at a convenient location to service both on- and off-site patrons. Single-family dwelling residential uses compatible with the P5 Residential designation would also be allowed on the flex site should retail uses not come to fruition. To address changing market conditions over the life span of a project of this scope and size, it is the intent of the Specific Plan to be flexible in terms of the product type, actual location, and acreage of the residential products within the site, so that products can be developed and rearranged within the plan as long as the overall 880-unit project density is not exceeded. However, major community framework elements such as parks, open space, trails, a school site and the retail/residential flex site are anticipated to remain in the general locations presented in the Land Use Concept plan.

In accordance with the description and objectives of the Redevelopment Project Area 2 Plan, which includes the New Pacific Properties site, an affordable housing implementation plan shall be prepared for the Specific Plan area. This plan shall be reviewed by the Planning Commission and City Council for approval and will outline the mechanisms to be employed to meet the affordable housing requirements of the Specific Plan area consistent with Community Redevelopment Law. The New Pacific Properties Affordable Housing Plan shall be adopted by the City Council prior to the recordation of any subdivision map for residential development within the Specific Plan area.

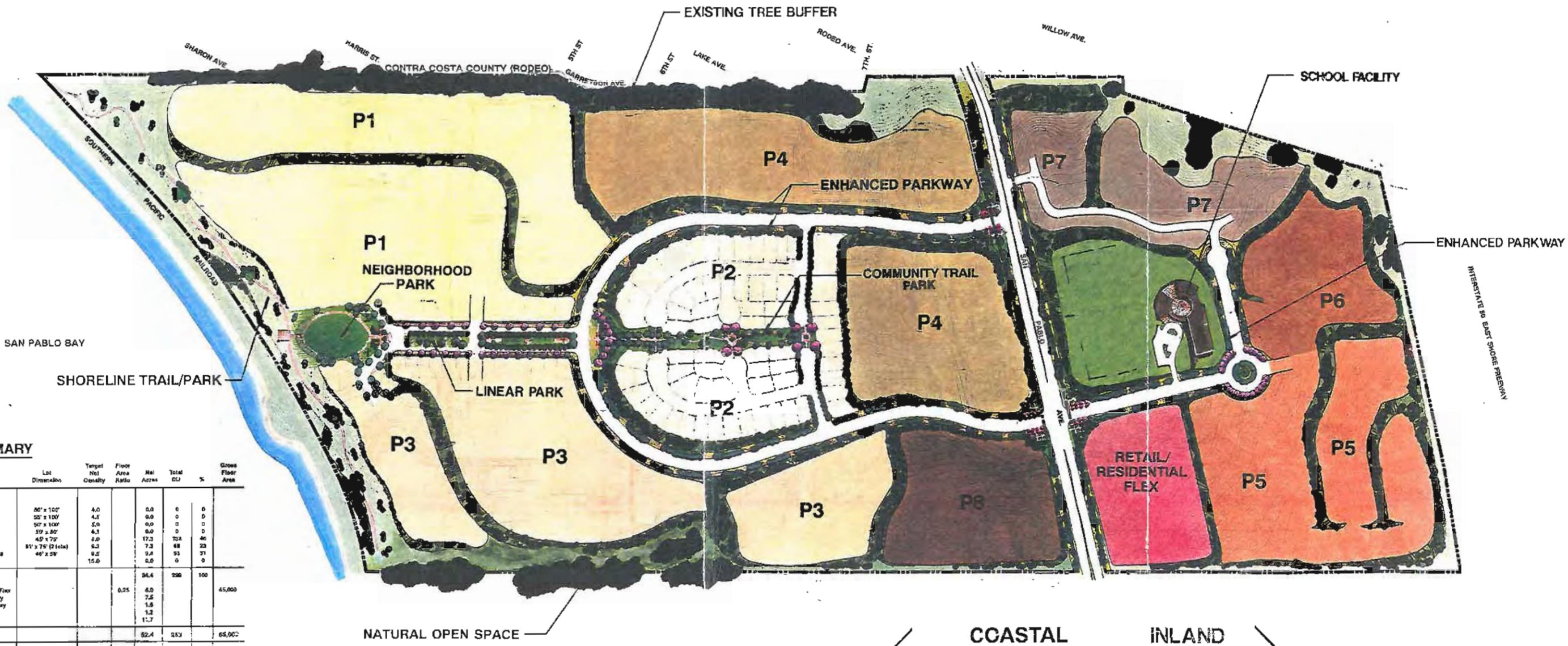
The project also provides for a variety of community facilities to meet the needs of future project residents. A 7.5-acre school site is proposed in the center of the project. A network of community trail, linear and neighborhood parks are located throughout the project, providing

passive, active, informal and formal open space and recreation opportunities for all future residents.

Attractive enhanced parkways and an extensive trail system provides connectivity between all neighborhoods and helps to reinforce community pride and identity for the entire Specific Plan area. The pedestrian trail system also connects to natural open spaces and street sidewalks, providing functional linkages to on-site retail and school sites and the shoreline area, as well as the off-site regional trail system.

Exhibit 9, Land Use, shows the proposed land use designations as arranged throughout the site, which is further illustrated on *Exhibit 10, Conceptual Lotting Plan*. A Land Use summary is provided in *Table 1, Specific Plan Land Use Summary*, below and each land use designation is described on the following pages.

| TABLE 1 SPECIFIC PLAN LAND USE SUMMARY | | |
|---|--|--------------------------|
| Land Use Description | | Proposed Zone Acreage |
| P1 Residential | SF Low Density | 31.9 AC |
| P2 Residential | SF Low Medium Density | 14.6 AC |
| P3 Residential | SF Low Medium Density | 25.2 AC |
| P4 Residential | SF Medium Density | 22.3 AC |
| P5 Residential | SF Medium High Density | 17.3 AC |
| P6 Residential | SF 2-Pack Lots | 7.3 AC |
| P7 Residential | SF Pocket Lots | 9.8 AC |
| P8 Residential | MF High Density | 7.8 AC |
| Retail/Residential Flex | | 6.0 AC |
| School Facility | | 7.5 AC |
| Parks | Neighborhood, Linear Community Trail and Shoreline Trail Parks | 14.3 AC |
| Open Space | Natural Open Space, Enhanced Parkways, Hillside, Slopes | 38.2 AC |
| Streets | Master Circulation System | 4.1 AC |
| Totals | | 206.3 AC |

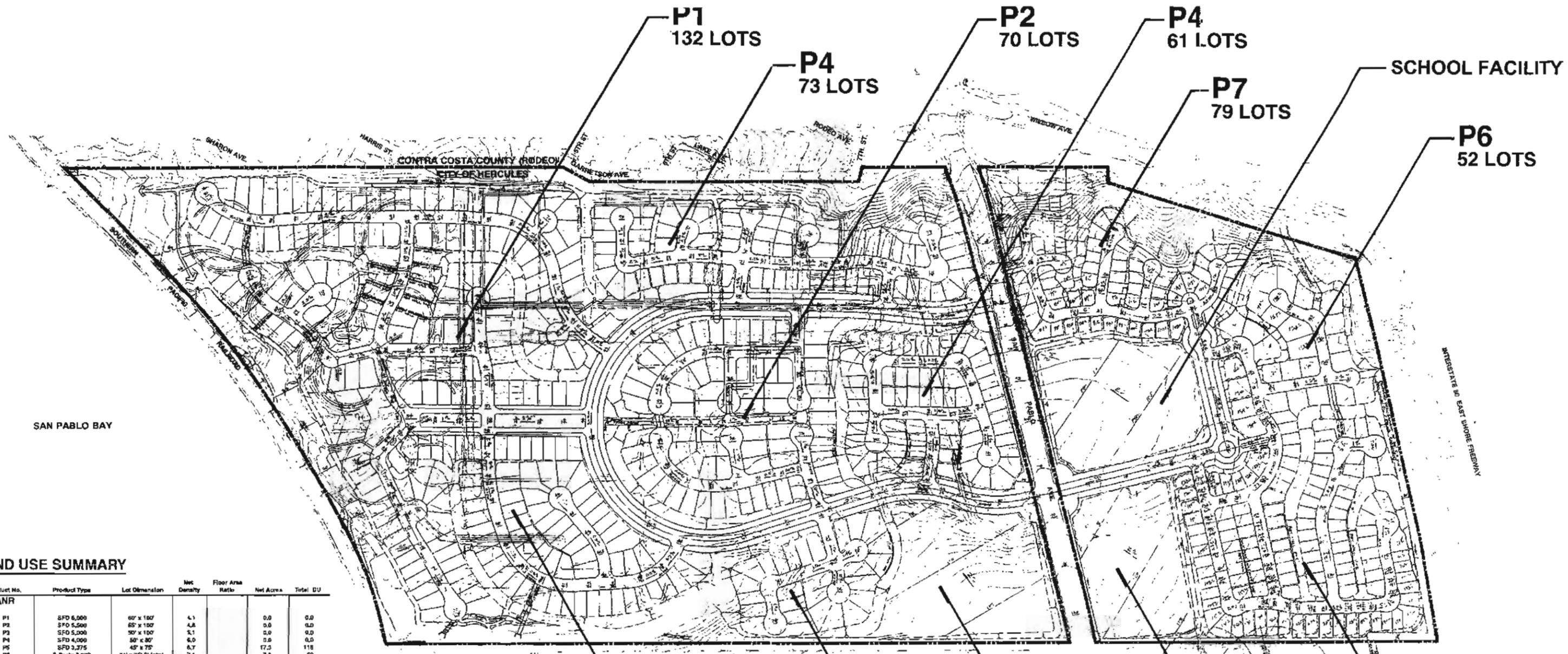


LAND USE SUMMARY

| Product No. | Product Type | Lot Dimension | Target Net Density | Floor Area Ratio | Max Acres | Total DU | % | Gross Floor Area |
|----------------------|-------------------------|--------------------|--------------------|------------------|------------|----------|---|------------------|
| INLAND | | | | | | | | |
| P1 | SFD 8,000 | 60' x 100' | 4.0 | 0.0 | 0 | 0 | | |
| P2 | SFD 5,500 | 50' x 100' | 4.5 | 0.0 | 0 | 0 | | |
| P3 | SFD 5,000 | 50' x 100' | 5.0 | 0.0 | 0 | 0 | | |
| P4 | SFD 4,000 | 50' x 80' | 6.3 | 0.0 | 0 | 0 | | |
| P5 | SFD 3,375 | 45' x 75' | 8.0 | 17.3 | 73.3 | 46 | | |
| P6 | 2 Pkcs 3,037 | 61' x 75' (2 lots) | 9.3 | 7.3 | 68 | 23 | | |
| P7 | Podical Lot 2,668 | 46' x 58' | 9.5 | 9.8 | 93 | 31 | | |
| P8 | MFA | | 15.0 | 5.0 | 0 | 0 | | |
| Sub-Total | | | | 84.4 | 290 | 100 | | 65,000 |
| | Retail/Residential Flex | | | 6.0 | | | | |
| | School Facility | | | 2.5 | | | | |
| | Enhanced Parkway | | | 1.8 | | | | |
| | Roads | | | 1.2 | | | | |
| | Open Space | | | 11.7 | | | | |
| INLAND TOTAL | | | | 82.4 | 383 | | | 65,000 |
| COASTAL | | | | | | | | |
| P1 | SFD 2,000 | 60' x 100' | 4.0 | 87.8 | 127 | 22 | | |
| P2 | SFD 2,500 | 50' x 100' | 4.5 | 14.2 | 95 | 11 | | |
| P3 | SFD 5,000 | 50' x 100' | 5.0 | 15.2 | 124 | 22 | | |
| P4 | SFD 4,000 | 50' x 80' | 6.3 | 22.3 | 145 | 25 | | |
| P5 | SFD 3,375 | 45' x 75' | 8.0 | 0.0 | 0 | 0 | | |
| P6 | 2 Pkcs 3,037 | 61' x 75' (2 lots) | 9.3 | 0.0 | 0 | 0 | | |
| P7 | Podical Lot 2,668 | 46' x 58' | 9.5 | 0.0 | 0 | 0 | | |
| P8 | MFA | | 15.0 | 7.3 | 117 | 20 | | |
| Sub-Total | | | | 101.8 | 581 | 100 | | |
| | Neighborhood Park | | | 1.4 | | | | |
| | Linear Park | | | 0.3 | | | | |
| | Community Trail | | | 2.2 | | | | |
| | Enhanced Parkway | | | 7.4 | | | | |
| | Shoreline Trail/Park | | | 9.5 | | | | |
| | Natural Open Space | | | 3.0 | | | | |
| | Roads | | | 2.9 | | | | |
| | Open Space | | | 10.3 | | | | |
| COASTAL TOTAL | | | | 143.3 | 581 | | | |
| GRAND TOTAL | | | | 225.7 | 830 | | | 65,000 |



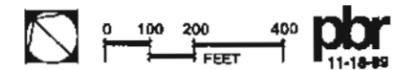
LAND USE CONCEPT
NEW PACIFIC PROPERTIES SPECIFIC PLAN



LAND USE SUMMARY

| Product No. | Product Type | Lot Dimension | Net Density | Floor Area Ratio | Net Area | Total DU |
|----------------------|--|--------------------|-------------|------------------|----------|----------|
| ANR | | | | | | |
| P1 | SFD 6,000 | 60' x 100' | 4.1 | | 0.0 | 0.0 |
| P2 | SFD 5,500 | 65' x 100' | 4.8 | | 0.0 | 0.0 |
| P3 | SFD 5,000 | 50' x 100' | 5.1 | | 0.0 | 0.0 |
| P4 | SFD 4,000 | 50' x 80' | 6.0 | | 0.0 | 0.0 |
| P5 | SFD 3,275 | 45' x 75' | 6.7 | | 17.5 | 118 |
| P6 | 2 Pkcs: 3,037 | 81' x 75' (2 lots) | 7.1 | | 7.3 | 52 |
| P7 | Pocket Lot: 2,668 | 46' x 58' | 6.1 | | 9.8 | 78 |
| P8 | MFA | | 15.0 | | 0.0 | 0.0 |
| Sub-Total | Retail/Residential Flex School Facility Enhanced Parkway Roads Open Space | | | 0.25 | 34.4 | 247 |
| ANR TOTAL | | | | | | 247 |
| COASTAL | | | | | | |
| P1 | SFD 6,000 | 60' x 100' | 4.1 | | 31.9 | 122 |
| P2 | SFD 5,500 | 65' x 100' | 4.8 | | 14.6 | 70 |
| P3 | SFD 5,000 | 50' x 100' | 5.1 | | 25.2 | 128 |
| P4 | SFD 4,000 | 50' x 80' | 6.0 | | 22.3 | 134 |
| P5 | SFD 3,275 | 45' x 75' | 6.7 | | 0.0 | 0.0 |
| P6 | 2 Pkcs: 3,037 | 81' x 75' (2 lots) | 7.1 | | 0.0 | 0.0 |
| P7 | Pocket Lot: 2,668 | 46' x 58' | 6.1 | | 0.0 | 0.0 |
| P8 | MFA | | 15.0 | | 7.8 | 117 |
| Sub Total | Neighborhood Park Linear Park Community Trail Enhanced Parkway Shoreline Trail/Park Natural Open Space Roads Open Space | | | | 101.8 | 581 |
| COASTAL TOTAL | | | | | | 581 |
| GRAND TOTAL | | | | | 206.3 | 828 |

**CONCEPTUAL LOTTING PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN**



2. Project Access

Regional access to the New Pacific Properties Specific Plan area is provided directly by Interstate 80 (East Shore Freeway) to San Pablo Avenue via Sycamore from the south, and Willow Avenue from the north.

3. Street System

The proposed project includes Primary and Secondary access points off of San Pablo Avenue, located north of Linus Pauling Drive as shown in *Exhibit 12, Conceptual Circulation Plan*. The Primary Entry north of Linus Pauling Avenue will be signalized, while the Secondary Entry south of Willow Avenue will have right turn in-right turn out restrictions. The proposed on-site circulation system will create a loop roadway system, extending on either side of San Pablo Avenue. This loop roadway is envisioned to serve as a major project identifier and community image setting feature of the Specific Plan. It leads to all minor streets serving individual neighborhoods within the Specific Plan area, as well as to activity areas such as the school and retail/residential flex site. Circulation in the vicinity of the school site will include a drop-off area set in from the main arterial, a round about and cul-de-sac designed into the roadway, so that vehicles will not queue back onto San Pablo Avenue. The roadway system is consistent with the General Plan Circulation Element, and includes enhancing San Pablo Avenue as well. Sidewalks are provided along all interior project roadways and along portions of San Pablo Avenue, to accommodate safe and efficient pedestrian access. Striped bicycle lanes are accommodated along the local collector roadways and along San Pablo Avenue.

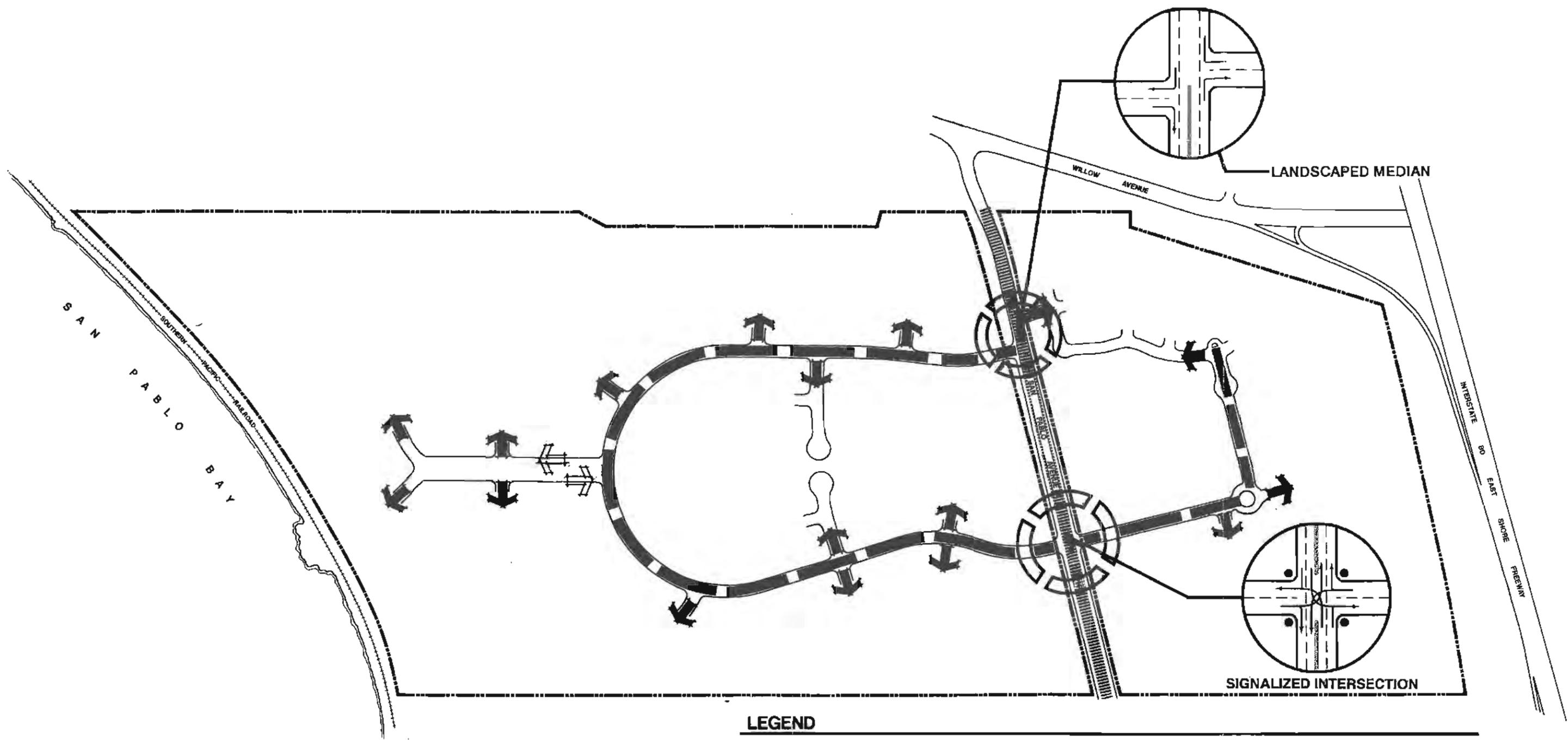
4. Circulation Standards

All circulation facilities (roadways, sidewalks, bicycle lanes, etc.) meet design standard guidelines and City of Hercules Department of Public Works roadway standards in terms of minimum pavement width for travelways and shoulders. However, project roadways are enhanced with expanded landscaped medians, generous landscaped parkways, pedestrian ways, and bike lanes. The City of Hercules requires curbs, gutters, sidewalks and parkways in standard collector, minor and cul-de-sac streets. Residential streets within the project would include curbs, gutters, landscaped parkways and sidewalks as described in the following sections. Curbs, gutter, street lighting, landscaping and underground utilities are provided for the retail/commercial site, as required by the City of Hercules.

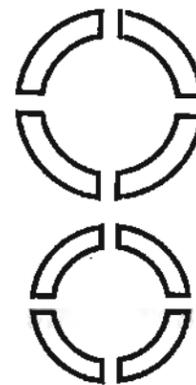
More particularly, the circulation system of the New Pacific Properties includes the following hierarchy of roadways and development standards as described below, and as shown on *Exhibits 13-17, Major Arterial, Collector Street, Minor Street, One-way Minor Street and Cul-de-Sac Street Cross Sections*:

a. Major Arterial/San Pablo Avenue

This major arterial has an existing right-of-way width varying from 121.81 feet at the southern boundary of the New Pacific Properties Specific Plan area, to 153.91 feet at the northern property boundary. In conjunction with implementation of the New Pacific



LEGEND



PRIMARY ENTRY

SECONDARY ENTRY
(RIGHT-IN/OUT ONLY)



4-LANE DIVIDED ROAD
SAN PABLO AVENUE



COLLECTOR STREET
2-LANE UNDIVIDED (74'-90' R.O.W.)
ENHANCED PARKWAY

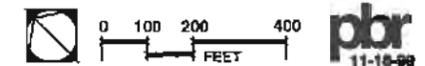


MINOR STREET/ACCESS POINTS (50'-54' R.O.W.)

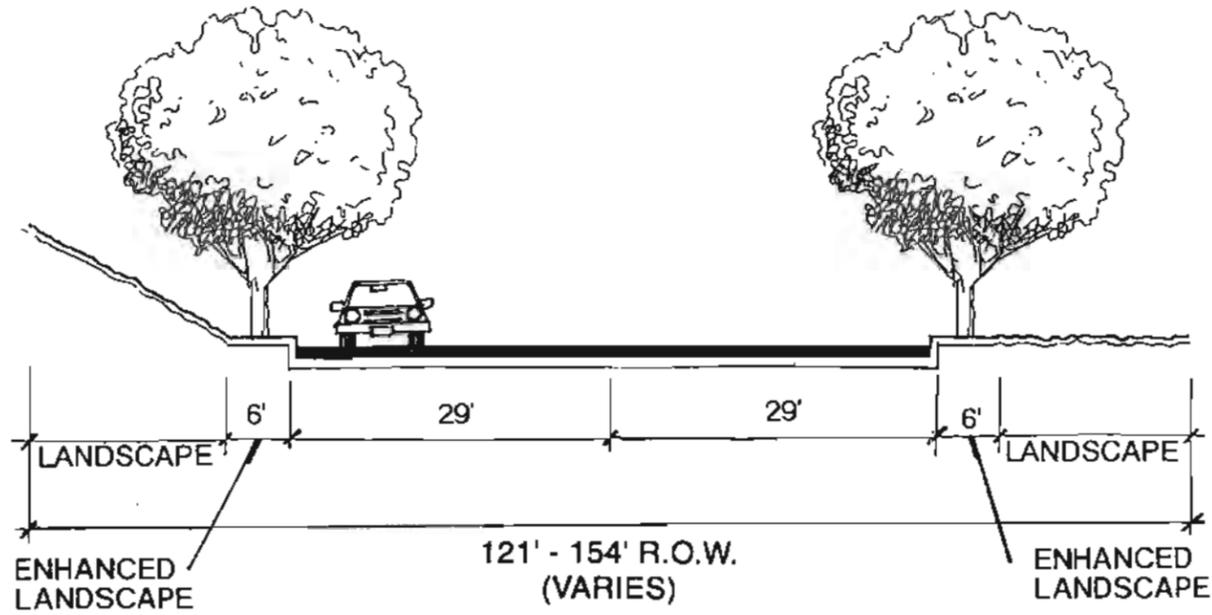


ONE-WAY MINOR STREET (33' R.O.W.)

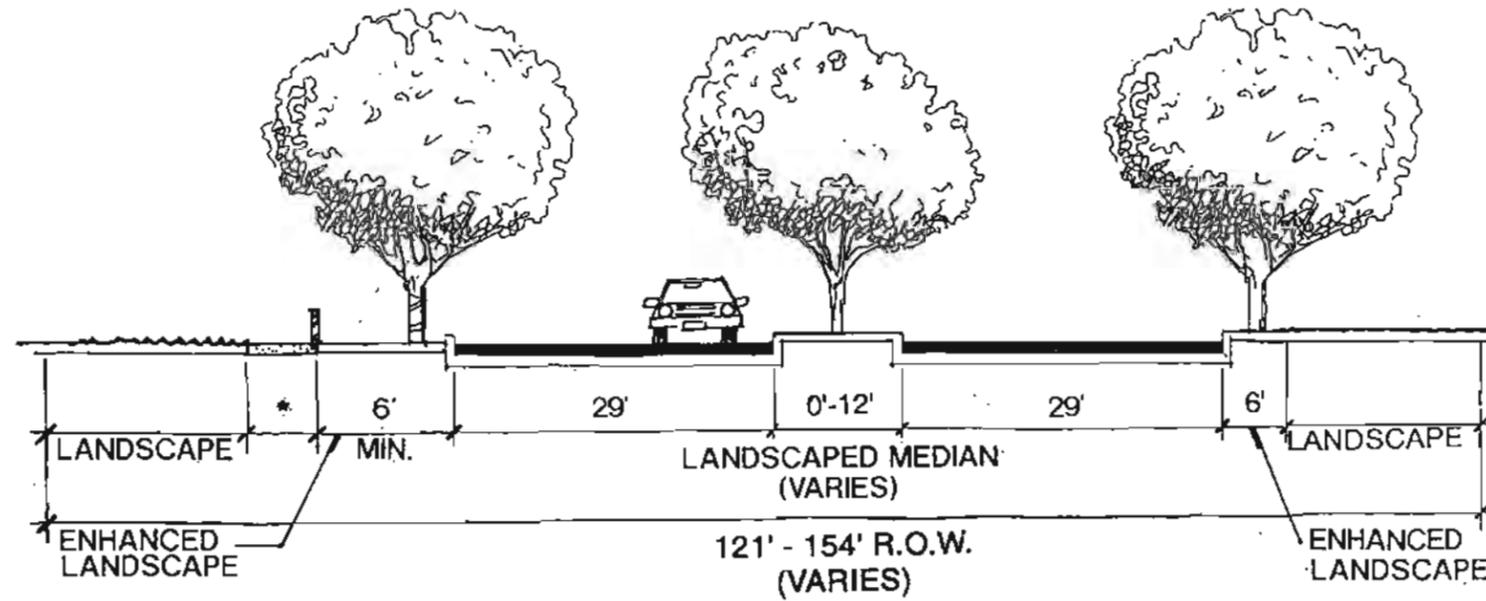
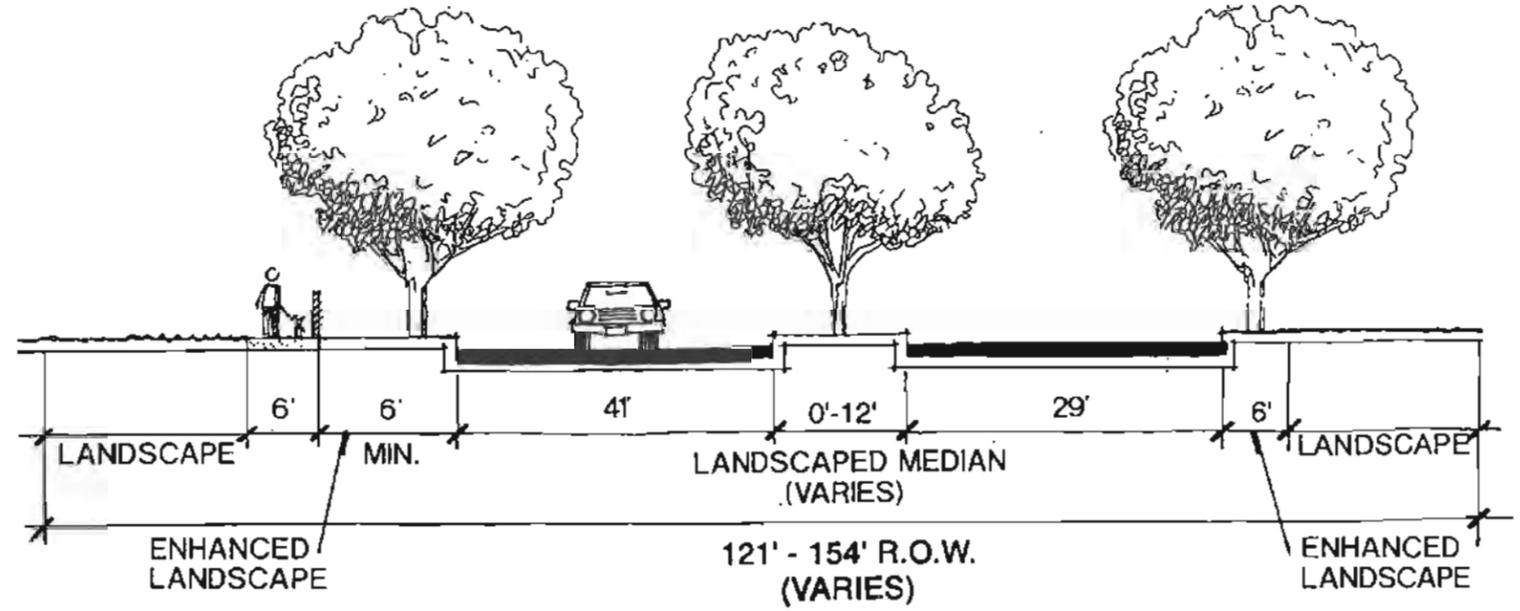
CONCEPTUAL CIRCULATION PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN



PORTION OF SAN PABLO AVENUE
FROM NORTHERN PROJECT
BOUNDARY TO SECONDARY
ENTRANCE



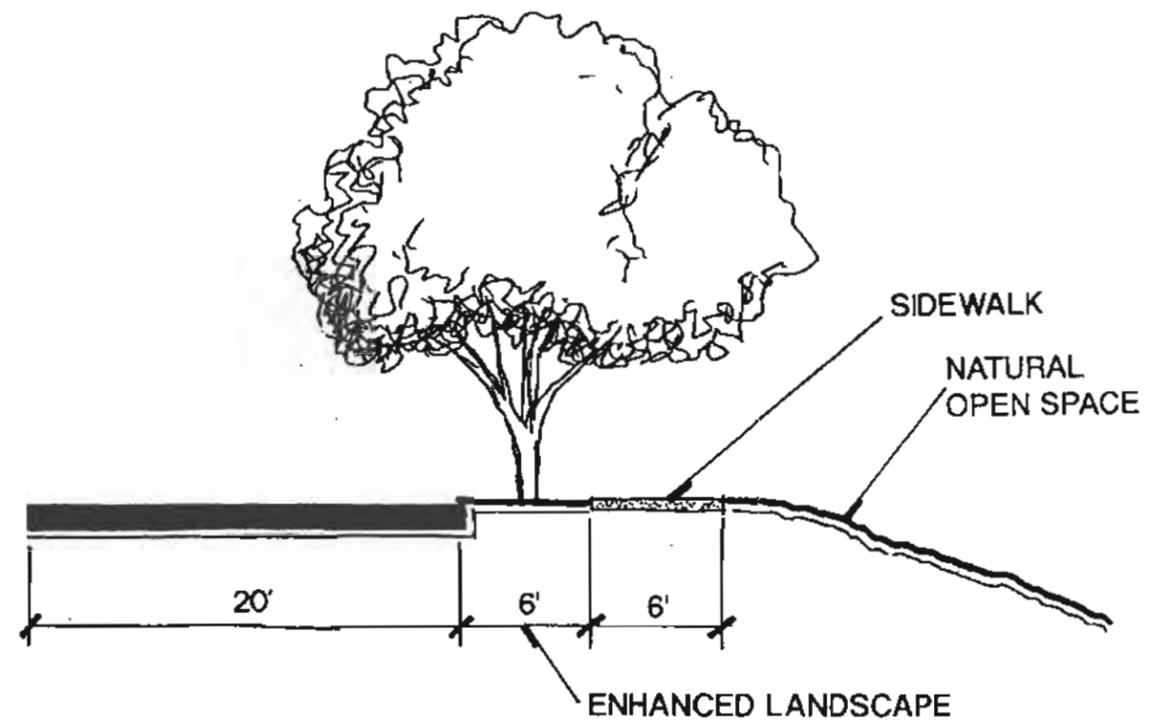
PORTION OF SAN PABLO AVENUE
FROM SECONDARY ENTRANCE
TO PRIMARY ENTRANCE



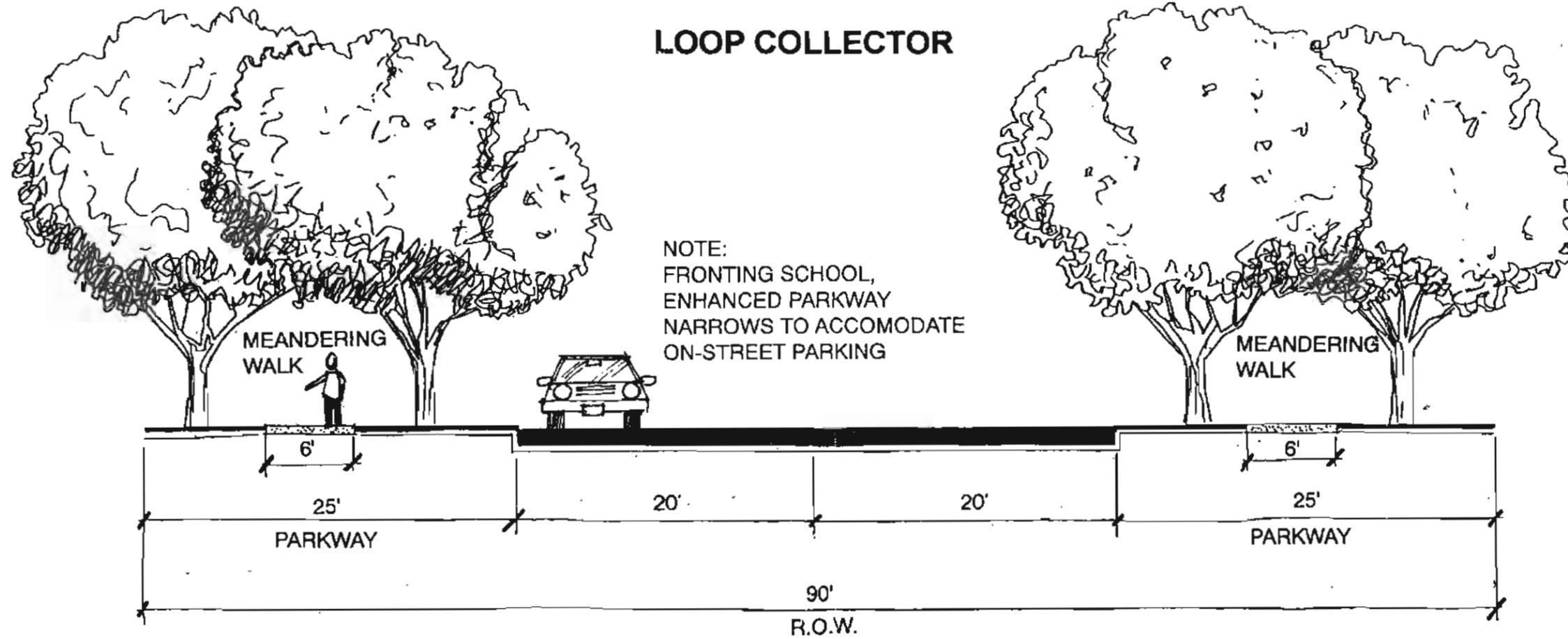
PORTION OF SAN PABLO AVENUE
FROM SOUTHERN BOUNDARY
TO PRIMARY ENTRANCE

*SIDEWALK WILL CONNECT TO AND
BE THE SAME WIDTH AS THE
EXISTING SIDEWALK ALONG
NORTH SHORE BUSINESS PARK

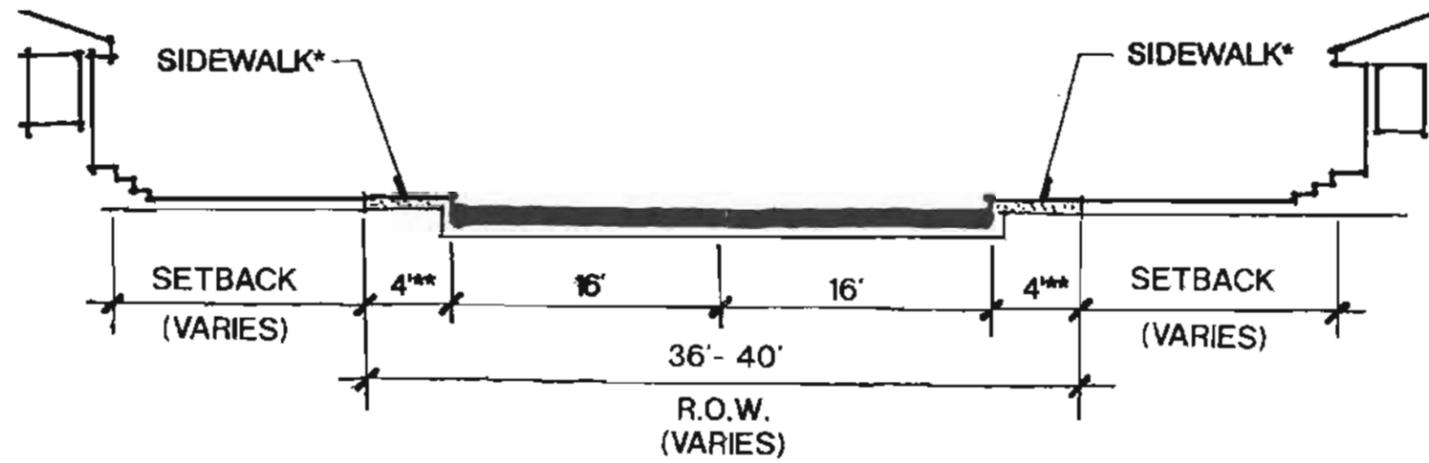
LOOP COLLECTOR AT OPEN SPACE



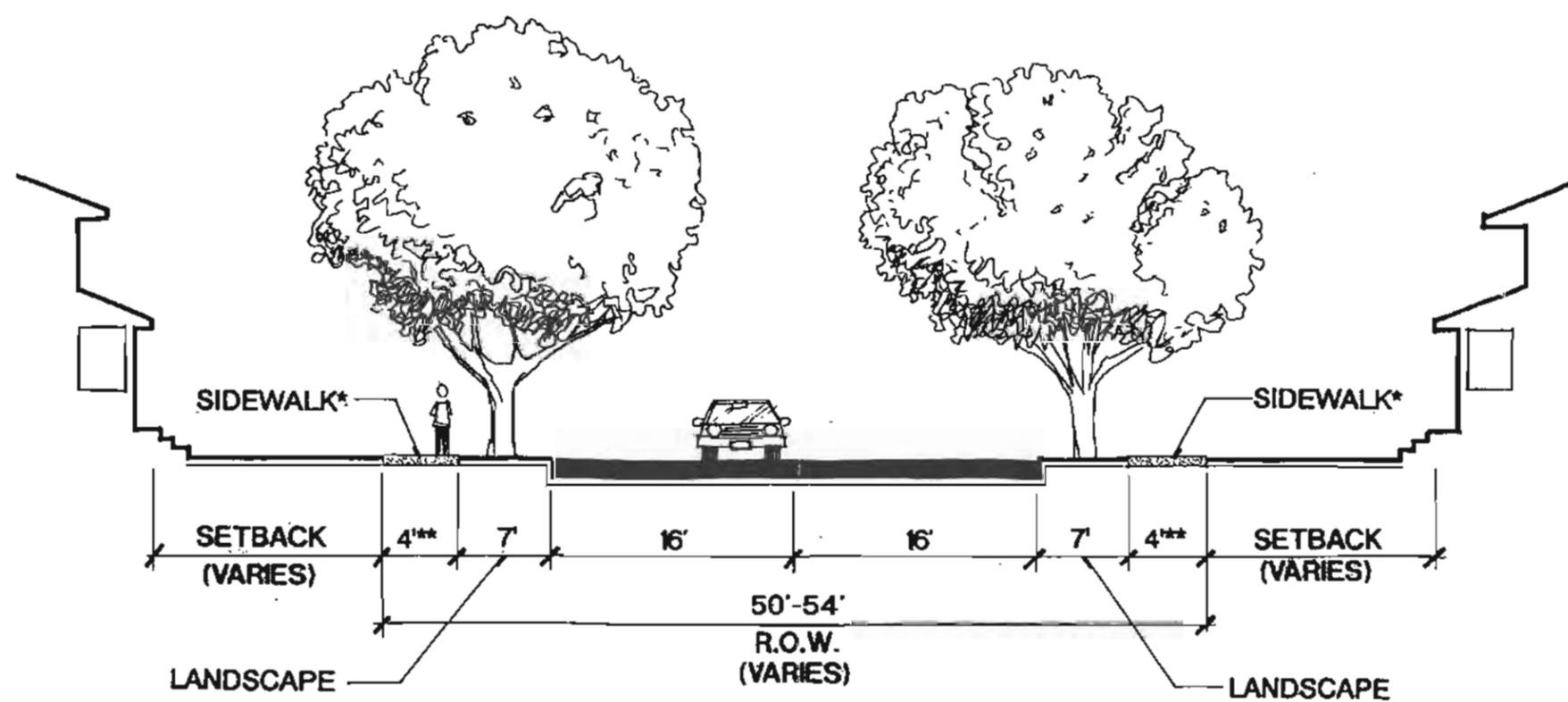
LOOP COLLECTOR



NOTE:
FRONTING SCHOOL,
ENHANCED PARKWAY
NARROWS TO ACCOMMODATE
ON-STREET PARKING



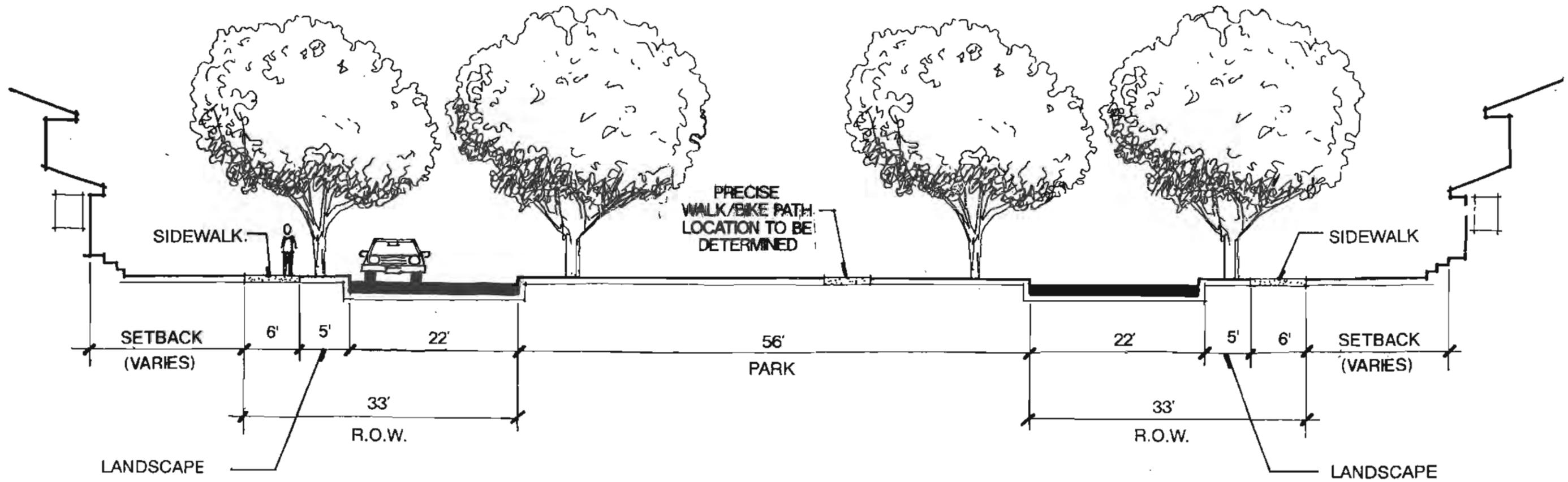
P7 PLANNING AREA ONLY



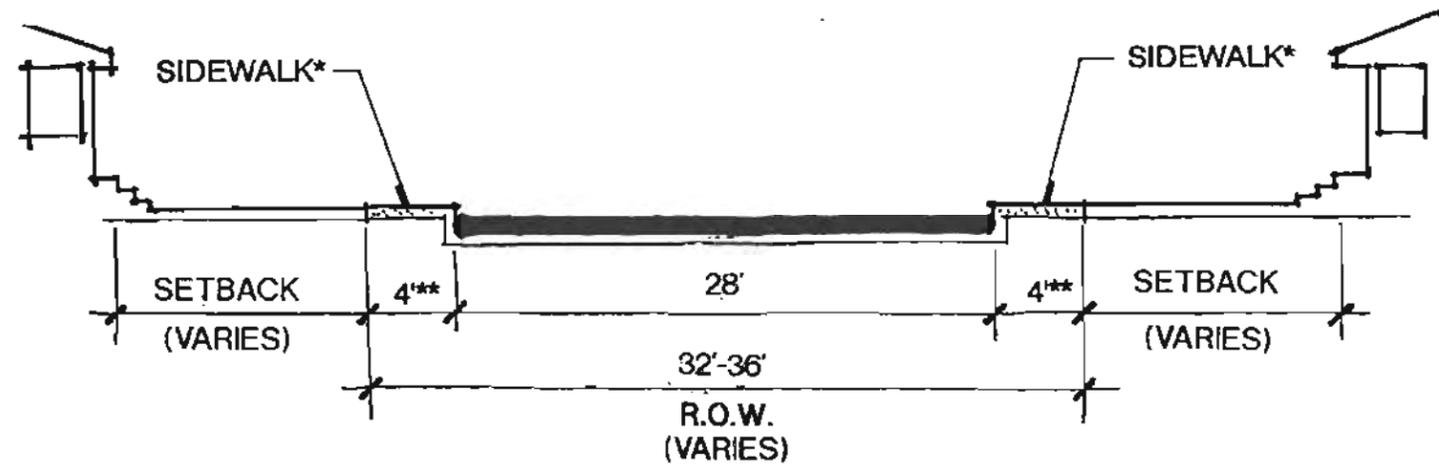
NOTES:

*SIDEWALKS ARE PROPOSED ON ONLY ONE SIDE OF THE STREET AT THE DISCRETION OF THE DEPARTMENT OF COMMUNITY DEVELOPMENT AND PUBLIC SERVICES AND CITY ENGINEER.

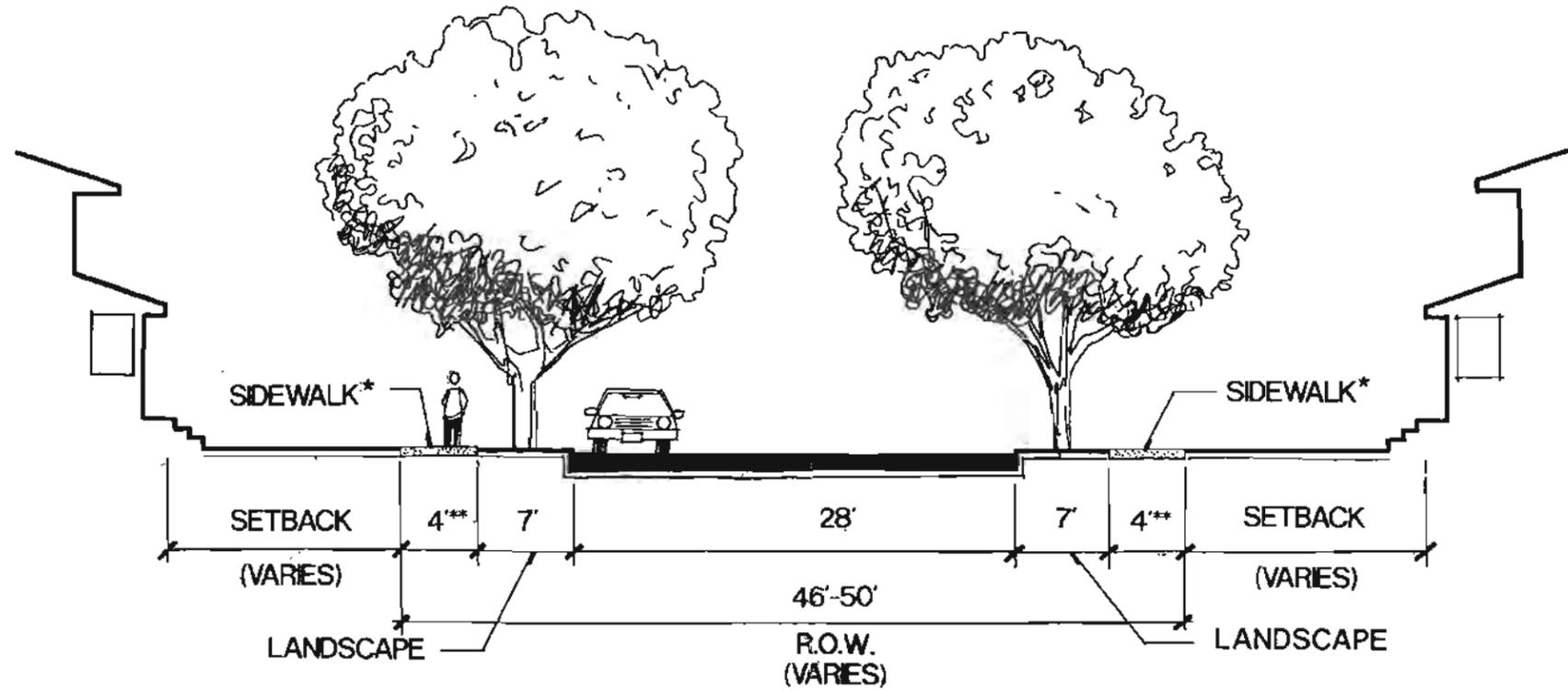
** IN THE P1 AREA AND THE SOUTHWESTERN PORTION OF THE P3 AREA, SIDEWALKS WILL BE 5' WIDE AND LANDSCAPE AREA WILL BE 6' WIDE.



**ONE WAY MINOR STREET CROSS SECTION
NEW PACIFIC PROPERTIES SPECIFIC PLAN**



P7 PLANNING AREA ONLY



NOTES:

* SIDEWALKS ARE PROPOSED ON ONLY ONE SIDE OF THE STREET AT THE DISCRETION OF THE DEPARTMENT OF COMMUNITY DEVELOPMENT AND PUBLIC SERVICES AND CITY ENGINEER.

** IN THE P1 AREA AND THE SOUTHWESTERN PORTION OF THE P3 AREA, SIDEWALKS WILL BE 5' WIDE AND LANDSCAPE AREA WILL BE 6' WIDE

Properties Specific Plan, it is anticipated that San Pablo Avenue will be improved to include a landscape median from the south end of the project to just north of the Secondary project entrance. From the Primary entrance to the northern project boundary, San Pablo Avenue improvements will taper in width due to topography and projected traffic conditions, and the landscape median will diminish in size until it is terminated. The roadway will also be improved to include signalization at the southern most intersection to the project (the Primary entrance) and will incorporate a 5-foot wide bike lane/emergency parking area on both sides of San Pablo Avenue, and a sidewalk on the west side of the avenue between the Secondary entrance and the project's southern boundary. The portion of this sidewalk located between the Secondary and Primary entrances will be 6 feet wide. The portion of this sidewalk located between the project's Primary entrance and southern boundary will match the width of the existing sidewalk fronting the North Shore Business Park. (See *Exhibit 13, Major Arterial Cross Sections.*)

b. Collector Street/Loop Roadway/Enhanced Parkway

This significant roadway has a right-of-way width varying from 74 feet to 90 feet, depending on topography/slopes and abutting uses, and includes bike lanes. A 74-foot right-of-way can be accommodated where the roadway abuts open space areas, 20-foot paved roadways in either direction which include improved shoulders on both sides of the roadway to incorporate bike lanes and emergency parking, and a 12- to 25-foot landscaped parkway including 6-foot sidewalks. Throughout the majority of the Specific Plan area, the loop roadway/Enhanced Parkway will have a 90-foot wide right-of-way, which incorporates the same traffic design standards as with the 74-foot wide right-of-way; however, the landscaped parkway widens from 12 feet to 25 feet. (See *Exhibit 14, Collector Street Cross Sections.*) Fronting the school site, it is envisioned that the Enhanced Parkway may be reduced to accommodate on-street parking and provide additional drop-off capacity.

c. Minor Streets

These roadways take access from the Loop Roadway, and serve as the roadway standard throughout all individual development areas. They will have a 50- to 54-foot right-of-way, which will include a 32-foot wide pavement width (including 10-foot wide travelways in each direction and 6-foot improved shoulders), and an 11-foot wide landscape zone including sidewalks at the perimeter of the right-of-way. In both P1 areas and the two P3 areas nearest the bay, these sidewalks will be 5 feet wide; in all other residential areas, they will be 4 feet wide. Sidewalks will be provided on only one side of the roadway at the discretion of the Department of Community Development and Public Services and City Engineer. In the P7 area, minor streets will not contain a landscaped parkway as shown on *Exhibit 15, Minor Street Cross Sections.*

d. One-Way Minor Streets

These roads gain access from the Loop Roadway, and are located along both sides of the formal Linear Park. Separated by the approximately 56-foot wide Linear Park, the one-way streets have a 12-foot wide travel lane, 10-foot wide shoulders which may be used for on-street parking, and an 11-foot wide landscape zone including a 6-foot wide

sidewalk at the edge of the right-of-way. (See *Exhibit 16, One-Way Minor Street Cross Section.*)

e. Cul-de-Sacs

Cul-de-Sacs within each development area have a right-of-way width varying from 46-50 feet, comprised of a pavement width of 28 feet and an 11-foot landscape zone on both sides of the street which will include sidewalks. In both P1 areas and the two P3 areas nearest the bay, these sidewalks will be 5 feet wide, in all other residential areas, they will be 4 feet wide. Sidewalks will be provided on only one side of the roadway at the discretion of the Department of Community Development and Public Services and the City Engineer. In the P7 area, Cul-de-Sac streets will not contain a landscaped parkway as shown on *Exhibit 17, Cul-de-Sac Street Cross Sections.*

C. GRADING PLAN

Exhibit 18, Preliminary Conceptual Grading Plan, reflects how the property may be graded to accommodate the New Pacific Properties Specific Plan. The overall approach is to take advantage of the site's gentle sloping towards San Pablo Bay. The grading concept is intended to provide bay views and bay orientation to future residents and/or visitors as they drive, walk, reside or recreate within the community. Southwest of San Pablo Avenue, the existing terraced condition which was developed to accommodate the Pacific Refinery will essentially be maintained, with alterations proposed to provide for residential development pads offering views of San Pablo Bay. Sensitive grading will occur along the bay shoreline within the area proposed as the Shoreline Trail/Park in order to reclaim the area degraded by years of oil refinery operations and return it to a safe and inviting area. Northeast of San Pablo Avenue on the inland side, the grading concept also reflects the existing topography by terracing the development envelopes so as to maximize bay view opportunities as well as orient future residential neighborhoods around the school facility as a focal point. Steeper hillside areas along the northeast boundary, along with a 3.6-acre natural open space area along the southwest boundary, are preserved primarily in their natural state as open space, except to allow for slope stabilization and grading. Earthwork is expected to balance independently both on the west and east sides of San Pablo Avenue. 440,000± cubic yards of cut and fill earthwork is estimated for Phase I on the inland side of the project. 1,050,000± cubic yards of cut and fill earthwork is proposed on the coastal side of San Pablo Avenue within Phases I & II. Slopes between terraces will range from 5 feet to 35 feet in height. A 2:1 slope ratio will be maintained and slopes will be vegetated to mitigate any potential for erosion. Some of the existing drainage systems will be altered to accommodate the revised terracing concept. All grading will implement Best Management Practices to reduce impacts to water quality.

D. PUBLIC FACILITIES AND SERVICES

The existing infrastructure servicing the New Pacific Properties Specific Plan area is currently operated by both public and private utility companies. A mixture of public and private utility operators may continue to service the site.

1. Drainage

The New Pacific Properties site is located adjacent to San Pablo Bay separated only by a Union Pacific Railroad levee and right-of-way. Two existing storm drain outfalls currently discharge storm water from the project site into San Pablo Bay: one approximately 1/3 distance from the northern boundary of the property, and the second just off-site at the extreme southwest corner of the New Pacific Properties Specific Plan area.

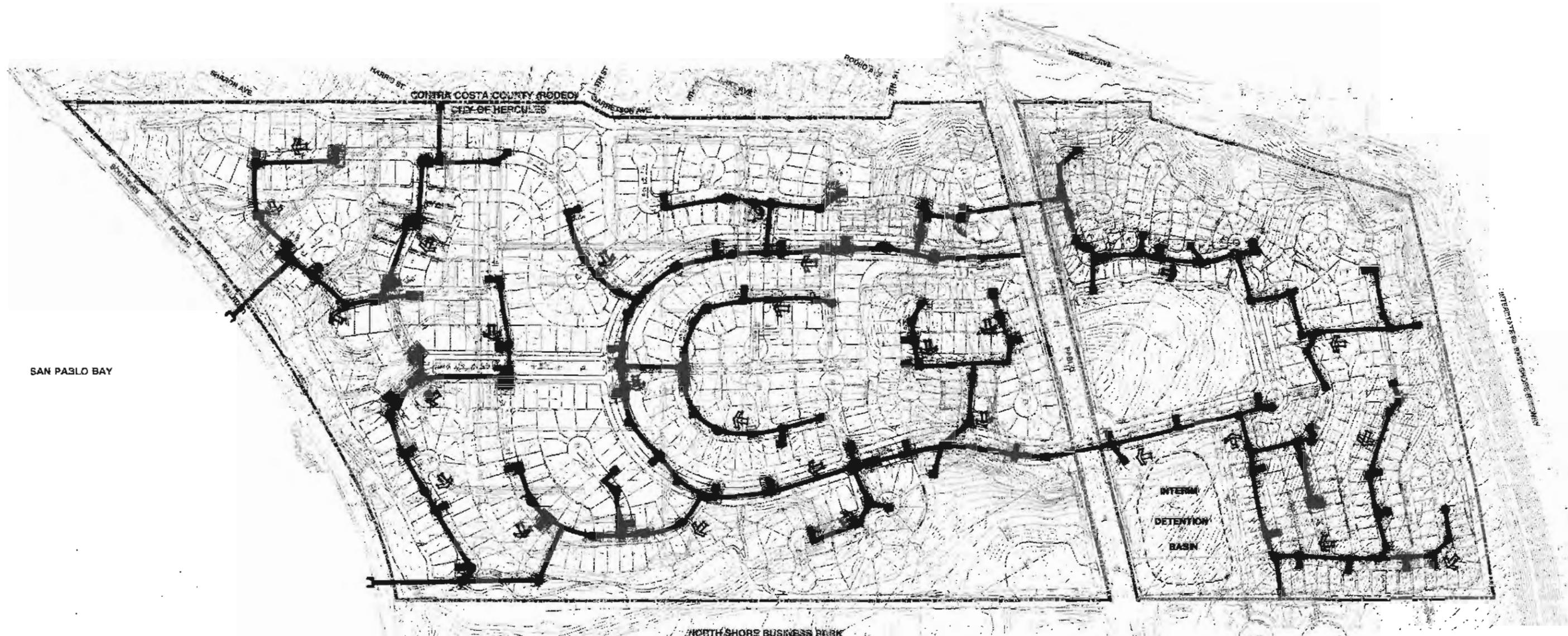
Runoff generated by the New Pacific Properties Specific Plan area will be collected through in-street storm drains. The system is designed to utilize street flow carrying capacity and flow into drains, which network through the streets and lead towards San Pablo Bay (see *Exhibit 19, Conceptual Storm Drain System Plan*). Plans call for approximately 20% of the project drainage to be conveyed through the northern most outfall. This outfall may need to be repaired and/or replaced for maintenance reasons. The remaining 80% of the project drainage may potentially be conveyed through the second existing southern outfall, which crosses the North Shore Business Park property before discharging into the Bay. Although this later drainage crosses property outside the Specific Plan area, it would be allowed to continue along this route under the law of prescriptive easement rights. Approval from the adjacent owner would be required to allow increased flows over their property. Another alternative is to replace these storm water conveyance facilities by constructing facilities under the railroad tracks just north of the southwest corner of the plan area, in order to avoid impacts on adjacent wetlands.

All construction plans for the proposed storm drain improvements will be submitted for review and approval by the City of Hercules Engineering Division. Approval from the State Water Resource Quality Control Board is also required for the increased drainage planned for the outfalls. On behalf of the Army Corp of Engineers, this agency reviews projects which impact certain categories of streambeds and wetlands. All Federal, State and local requirements and mitigation regarding outfall capacities and bay tidal action effects, etc. will be implemented.

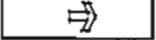
2. Water

The East Bay Municipal Utility District (EBMUD) provides domestic water to the site via an 8" main connection located within Garretson Avenue in the Community of Rodeo immediately to the north of the New Pacific Properties Specific Plan area. (See *Exhibit 20, Conceptual Water System Plan*.) No water mains currently exist within the San Pablo Avenue frontage on site, though a 12" main is located in San Pablo Avenue 3,000 feet south of the site, and two 12" water mains currently run within Willow Street, to just north of the site. An additional water line exists at San Pablo and Linus Pauling. Two new water lines will be extended to service the New Pacific Properties site and interior residential and commercial lots are planned to be serviced by domestic water lines running through the Specific Plan's roadways.

The EBMUD will likely be extending water main service within Willow Avenue for the proposed adjacent senior housing project. An alternative also is being studied by the EBMUD which may up size this main extension to provide a second project point of

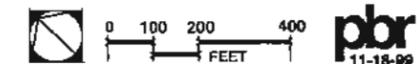


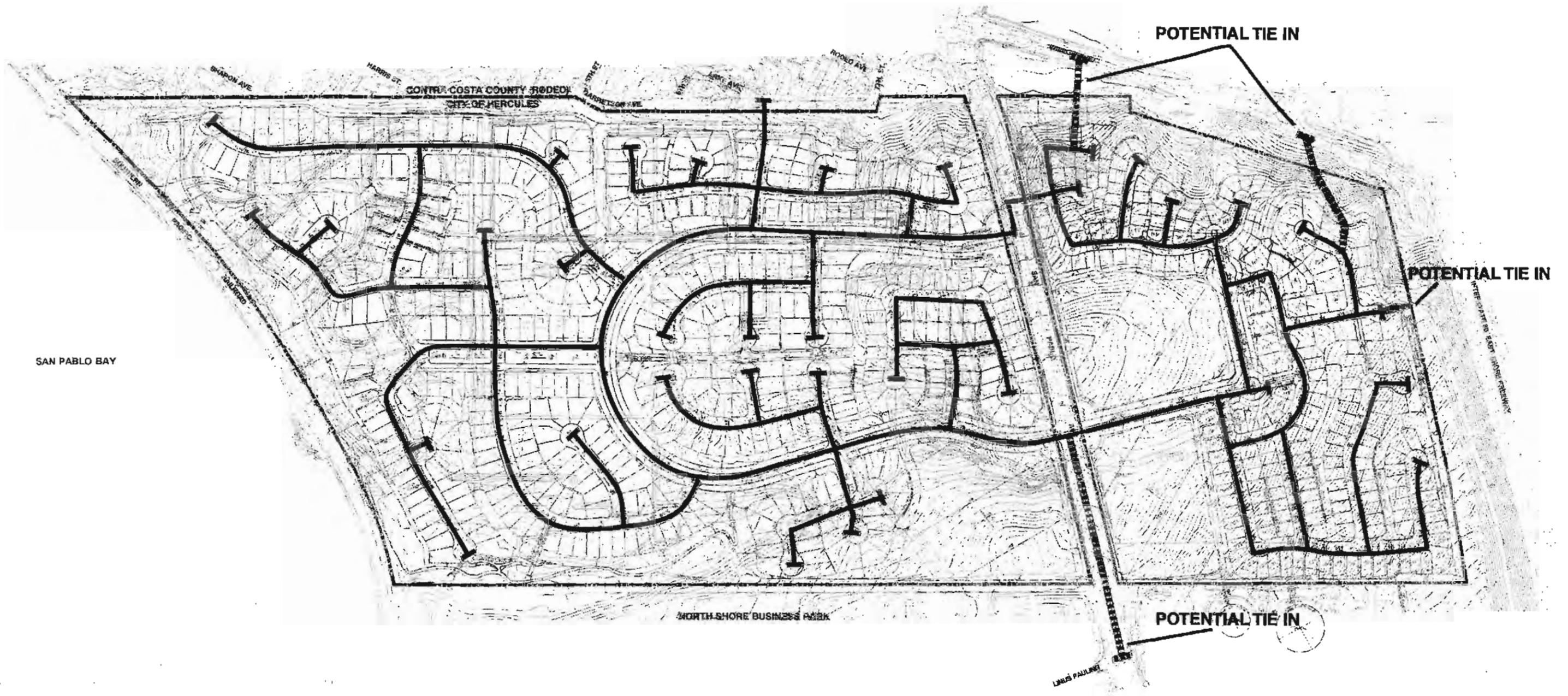
LEGEND

-  CURB INLETS
-  MANHOLES
-  INLET/OUTLET STRUCTURES
-  STORM DRAIN
-  DIRECTION

DESIGN SUBJECT TO CHANGE PER
FINAL GRADING AND LOTTING PLANS.

CONCEPTUAL STORM DRAIN SYSTEM PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN





DESIGN IS SUBJECT TO CHANGE PER EAST BAY MUNICIPAL UTILITIES DISTRICT DESIGN REQUIREMENTS AND FINAL GRADING AND LOTTING PLANS.

LEGEND

-  8" WATER LINE
-  12" WATER LINE

CONCEPTUAL WATER SYSTEM PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN

connection. Utilizing this alternative to service the New Pacific Properties Specific Plan area would require an easement through the Senior housing project since Willow Avenue does not abut the New Pacific Properties project. It is anticipated that the slopes leading into the project can be negotiated to the acceptance of the EBMUD, and that direct service to the Specific Plan area can be provided as shown on *Exhibit 20, Conceptual Water System Plan*. Water will be provided to the New Pacific Properties project in accordance with EBMUD requirements.

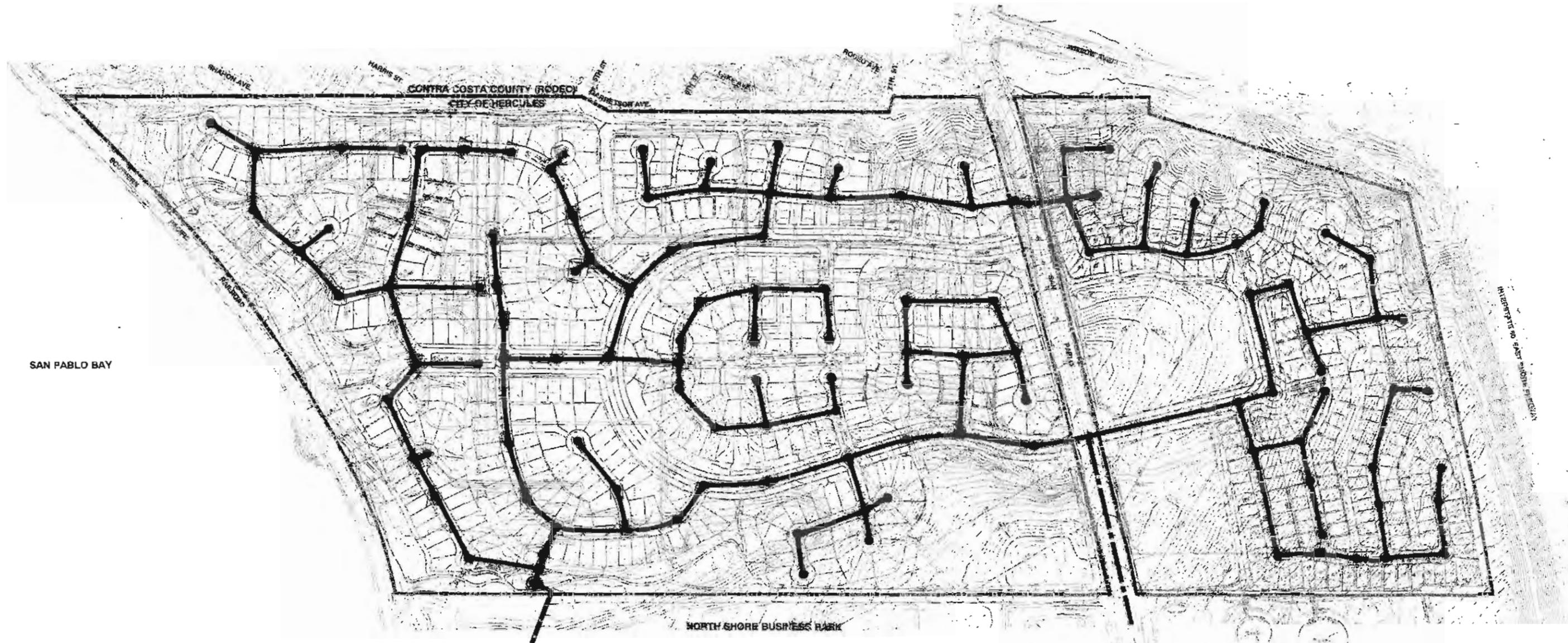
Currently, 24" and 36" steel aqueducts run through the southeastern portion of the site within a 20' wide easement. These lines are critical to the operations of the District's water distribution system. Development plans for the New Pacific Properties project include relocation of those distribution lines to along the I-80 boundary and through the eastern portion of the site's roadways. EBMUD has indicated that a direct tie-in to these relocated lines would enhance service parameters for this project as well as the surrounding service area.

3. Sewer

Historically, sewer service for the site was handled by an on-site sewer treatment plant located at the southeast corner of the New Pacific Property. Several options for treatment of future sewer demands were studied which include: 1) creation of a new treatment plant to be located elsewhere on the property; 2) connection to a new Joint Power's Authority facility contemplated in the unincorporated community of Rodeo to the north; 3) connection to the City sewer system located to the south of the site; 4) direct connection to the Hercules/Pinole wastewater treatment facility; and 5) connection to the Rodeo Sanitation District. The third option is currently the most desired interim alternative. This option requires the use of a pump station as the low point of the site is below existing sewer transmission facilities, causing a dip in the sewer profile. The *Conceptual Sewer System Plan* is shown on *Exhibit 21*. Sewage will be pumped up to San Pablo Avenue and a new line will be constructed running south along San Pablo Avenue connecting to the City's system at its closest point. If timing is favorable, alternative two will be pursued, and will also require use of pump facilities and force mains which will connect to new transmission lines within San Pablo Avenue running north or directly to existing lines in Rodeo. Alternative one remains viable as the existing outfall is being maintained.

4. Solid Waste

Home curbside solid waste collection services are provided to the City of Hercules by Richmond Sanitary Service, a private solid waste disposal company. According to company officials, solid waste is transported directly to the West Contra Costa County sanitary landfill located along the bayshore, at the foot of Parr Boulevard at the Richmond Parkway cross street, in Richmond. According to Richmond Sanitary Service, the existing landfill can accommodate the solid waste anticipated to be generated by the 880 units proposed within the New Pacific Properties Specific Plan area, and no problems are anticipated with regards to solid waste service. Should the Richmond landfill become filled, solid waste will continue to be transported by Richmond Sanitary Service either directly to the Potrero Hills landfill located in the community of Fairfield to the east, or transported to a transfer station



**LOCATION OF FORCE MAIN CONNECTION
SUBJECT TO CITY ENGINEERING REVIEW.**

LEGEND

-  GRAVITY TRUNK
-  FORCE MAIN
-  PUMP STATION LOCATION OF POTENTIAL ON SITE TREATMENT FACILITY, IF APPLICABLE
-  FUTURE LINE

**DESIGN SUBJECT TO CHANGE PER
FINAL GRADING AND LOTTING PLANS.**

**CONCEPTUAL SEWER SYSTEM PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN**

and then carried to the Portrero Hills landfill. That landfill is a relatively new/young facility with an extensive life. Home curbside recycling services also will be offered through West County Recycle.

5. Utilities

Pacific Gas and Electric Company currently provides natural gas and electrical power to the New Pacific Properties site, while Pacific Bell Company provides telephone service.

Gas - Gas lines currently run within San Pablo Avenue, and will connect to and service the project through a network of lines running within the Specific Plan's roadways.

Electric - Electrical lines which service the New Pacific Properties site currently run underground within the San Pablo Avenue right-of-way. Pacific Gas and Electric Company's transmission facilities are adequate to service the New Pacific Properties Specific Plan proposed density. Project design allows for a 12-foot by 10-foot area set aside within the right-of-way to accommodate a new feed box in the future should the electricity provider require such.

A 60 kv major electrical transmission line also runs within the project along the I-80 freeway and northeastern (Rodeo) property boundaries. This line is covered by two P.G.&E. Easements: one 20 foot wide and the other overlaying that being 50 feet wide. The transmission facilities located within these easements serviced the former refinery and are slated for removal and the easements for abandonment.

Telephone - Pacific Bell Company telephone service is currently provided to the New Pacific Properties area by underground lines running within the San Pablo Avenue right-of-way.

TOSCO Oil Line - Another 10 foot wide easement runs through the inland portion of the project parallel to the EBMUD water line easement. This easement accommodates an oil pipeline owned by TOSCO Oil used to transport oil from their bay shore receiving facility located to the north, to their fuel storage facility located adjacent to the New Pacific Properties site to the south. This easement is proposed to be relocated to just inside the eastern boundary of the property along Interstate 80 boundary and within the project's roadways, or within the Interstate 80 right-of-way, similarly to the proposed relocation alignment of the EBMUD water line.

All utilities within the New Pacific Properties Specific Plan area and running within San Pablo Avenue fronting the project will be placed underground and will be distributed through a network of underground lines located within the project's roadways. Some of the existing utility easements may need to be relocated in conjunction with the proposed project, however, whenever possible, they will be accommodated along property boundaries, within project roadways, and with appropriate replacement easements.

E. COMMUNITY FACILITIES AND SERVICES PLAN**1. Parks and Recreation**

As discussed in the Land Use section, the New Pacific Properties Specific Plan has been designed to integrate a comprehensive system of open space, parks and trails. The intent is to:

- provide a wide variety of different park and recreation experiences within easy walking distance to residential neighborhoods,
- help create strong community identification,
- utilize parks and recreation elements as focal points within the community, and
- encourage pedestrian use and community interaction

The Land Use Concept plan includes a total of 64.1 acres of open space over the 206.3-acre property; comprised of 14.3 acres of parks, 3.6 acres of natural open space, 9.0 acres of enhanced parkway, 4.1 acres of roads, 25.6 acres of associated open space, as well as a 7.5-acre School Facility site. These facilities exceed the City of Hercules' Park Standards. The Specific Plan's 14.3 acres of park is broken down into a 1.6-acre Neighborhood Park; 0.9-acre Linear Park; 2.3-acre Community Trail Park; and 9.5-acre Shoreline Trail/Park.

Parks and recreation uses within the New Pacific Properties Specific Plan area are envisioned to allow for a variety of active and passive uses, such as multi-use fields, tot lots, soccer, baseball, passive and interpretive areas, trails, etc. The Community Trail Park, Neighborhood Park, and Shoreline Trail/Park will include open areas with organized landscaping incorporating shade trees and plant palettes as discussed in detail in Chapter V, Section D, *Landscape Architectural Design Guidelines*. The Shoreline Trail/Park is designed to connect with the regional "Bay Trail," a 400-mile continuous recreation corridor with bicycling and hiking trails around the bay shoreline. The Linear Park is planned as a formal experience. The School site includes active opportunities with facilities that may include multi-use fields, ball fields, tot lots, etc. The Natural Open Space areas will be left in their natural state unless enhancement of the wetland areas is required through the entitlement process with the U.S. Army Corps of Engineers.

2. Trail System

As previously discussed, an extensive pedestrian/bicycle trail system is included as an important function of design project component (see Section A. *Land Use Plan*, number 12.c. *Pedestrian/Bike Trail System* of this chapter.) The trail system is also shown on *Exhibit 9, Land Use Concept*, and *Exhibit 11, Conceptual Open Space and Recreation Plan*. Trails within the New Pacific Properties Specific Plan area will be improved and delineated either through improved surfacing, stripping or landscaping. Pedestrian trails will be improved with a minimum width of 4 feet. Pedestrian trails should be separated from roadways by a landscape strip whenever possible. Trails dedicated for off-street bicycle use should be a minimum of 6 feet wide for one-way travel, 8 feet wide for two-way travel, and 12 feet wide minimum for shared use paths.

3. Schools

Based on student generation rates provided by the West Contra Costa Unified School District, it is estimated that approximately 685 students will be generated by the proposed residential portion of the Specific Plan area. A section of the Specific Plan area has been reserved for an elementary school site. Serving as the focal point for this portion of the property, the parcel will be 7.5 acres and located east of San Pablo Avenue in the center of the inland development area. The new school facility will include classroom buildings, athletic facilities, parking and associated utilities, and a park and play field. The school will service students from this project, but may also serve other students within the West Contra Costa Unified School district. Excellent pedestrian access is provided via the Enhanced Parkway throughout the project, with good vehicular access provided by a driveway loop circulation system that runs through the parking area around the front of the school site, to maintain the steady flow of traffic along San Pablo Avenue and create safe entry and exit conditions for the slower traffic in and out of the school site. In further promoting safe circulation, the loop roadway will incorporate a roundabout with a landscape island to facilitate student drop off and pick up along the perimeter of the school site as well. In addition, a cul-de-sac is proposed within the roadway design at the rear of the school site, which will further accommodate turnaround movements and student drop off and pick up during periods of inclement weather and unusually high traffic (see *Exhibit 9, Land Use Concept*).

Particulars of the school layout and architecture itself will be determined at the time of design and approval of the school facility. It is anticipated that the design will incorporate a two-story structure similar to other elementary schools recently constructed in the district, to minimize the land area needed for the facility. Vehicular drop-off/pick-up areas will be included in the circulation pattern to enhance traffic flows and minimize vehicular congestion. Parking will be designed and located to be shared with the school facility park use.

4. Fire

Existing source facilities are sufficient to service the Specific Plan area and provide necessary fire flows, however, new distribution lines, will be needed to service the site. The on-site water infrastructure design is reflected on *Exhibit 20, Conceptual Water System Plan*, and will include lines and fire hydrants sufficient to meet the needs of the community.

The Specific Plan area falls within the Rodeo-Hercules Fire District which covers an area of 25 square miles and includes the town of Rodeo and City of Hercules. The fire district maintains two stations manned 24 hours a day. Station 75, the closest to the New Pacific Properties site, is located approximately 1.5± miles to the north in Rodeo at 326 Third Street, while Station 76 is located in Hercules at 1680 Refugio Valley Road. In addition, a new station is planned to service properties within the City's Sphere of Influence. The General Plan requires that Fire Stations considered for capital improvements be situated to achieve a five minute emergency response time by first response units for 90% of all emergency calls. Response time to the New Pacific Properties Specific Plan area is adequate from both existing facilities, and according to the Fire District, no new station is needed to service the Specific Plan area. Nevertheless, because of the City's location

surrounded by abundant wildland open spaces and grassy hillsides, the City is subject to a very real wildland fire danger. As such, the fire district implements an aggressive weed abatement program to cut fire breaks and provide an edge to combat wildfires. The New Pacific Properties Specific Plan is consistent with the requirements of that program.

The General Plan recommends the use of fire resistant plant material in open space landscaping. In addition, the Specific Plan area east of San Pablo Avenue abuts existing PG&E temporary storage tanks, which are situated 50 feet from the New Pacific Properties' southeast boundary. The Rodeo-Hercules Fire Marshall recommends that structures observe a 100-foot setback from the tanks.

Residential development proposed within the project is over 100 feet from the existing PG&E temporary storage tanks. Landscape Architectural Design Guidelines prepared for the project encourage the use of fire resistant vegetation in landscape design, as discussed in more detail in Chapter V, Section D, *Landscape Architectural Design Guidelines*. Architectural design guidelines also reflect fire resistant design features, such as fireproof materials for roofing, rather than wood shake shingles, etc. These features are discussed in more detail in Chapter V, Section C., *Architectural Design Guidelines* of this document.

5. Police

The City of Hercules and the New Pacific Properties Specific Plan area are serviced by the City of Hercules Police Department located at Sycamore and Civic Boulevard. Hiring of 1-2 new police officers is anticipated in 1998. Response time to the Specific Plan area is estimated to be 3-4 minutes for an emergency, and within 15 minutes for non-emergencies, which is considered acceptable by the Police Department. The Police Department staff do not anticipate any problems in providing public police protection to the Specific Plan area.

6. Hospital and Medical

Several options exist to provide medical services for the New Pacific Properties project. It may be serviced by the Doctors Hospital of Pinole, a 139-bed facility located approximately 2 miles to the south. The Brookside Hospital in Richmond roughly 9 miles to the south offers an alternative 246-bed facility, along with the Kaiser Hospital. Paramedic emergency service is provided by American Medical Response (AMR), and Emergency Medical Technicians are also available through the Rodeo/Hercules Fire Department to assist in emergencies.

IV. DEVELOPMENT STANDARDS

A. GENERAL PROVISIONS

1. Introduction to Land Use Regulations

The New Pacific Properties Specific Plan is intended to promote redevelopment of the Pacific Refinery site into a new, predominantly residential neighborhood. The intent of these Land Use regulations is to guide residential and commercial development within this community, thereby creating a high quality product and environment, establishing a strong tax base for the City, and instilling new vitality into a blighted area. The project will draw upon a design vocabulary from successful residential communities across the nation and set forth permitted uses and development standards for various land use areas referred to as "zones" within the Specific Plan area. This Specific Plan identifies nine separate zones that correspond to the various planning areas within the New Pacific Properties Specific Plan area, as detailed in *Table 2, Zones/Planning Areas*. All residential planning areas are zoned for single-family detached development, except for P8 which is zoned for multi-family attached development.

| TABLE 2 ZONES/PLANNING AREAS | |
|---|-----------------------------------|
| Zone | Proposed Planning Areas |
| Residential Low Density (SP-R-L) | P1 Single-Family Residential |
| Residential Low Medium Density (SP-R-LM) | P2 & P3 Single-Family Residential |
| Residential Medium Density (SP-R-M) | P4 Single-Family Residential |
| Residential Medium High Density (SP-R-MH) | P5 Single-Family Residential |
| Residential Z-Lot Line (SP-R-Z) | P6 Single-Family Residential |
| Residential Pocket Lots (SP-R-PL) | P7 Single-Family Residential |
| Residential Multi-Family (SP-R-MF) | P8 Multi-Family Residential |
| Retail/Residential Flex (SP-R/RF) | Retail/Residential Flex |
| School (SP-S) | School |
| Open Space/Parks (SP-OS) | Parks and Open Space |

Because the New Pacific Properties Specific Plan products propose deviations from the proposed City zoning standards in the interest of a varied and imaginative overall project design, a Change of Zone to the various zones listed above is proposed.

The nine zones described in this section apply only to the New Pacific Properties Specific Plan area. The zones and corresponding standards do not apply outside of the Specific Plan area boundaries. These Development Regulations supersede all provisions, standards and requirements of the City of Hercules Zoning Ordinance, except in those instances where the New Pacific Properties Development Regulations remain silent. In instances where these Development Regulations do not address specific issues or other relevant considerations, then the Hercules Zoning Ordinance shall apply. If a conflict arises between this Specific Plan's regulations and the City of Hercules, then the standards contained in this chapter shall take precedence.

B. ZONING DESCRIPTIONS

This section of the Development Standards describes in detail each of the nine zones located within the project area. The approximate location and configuration of each zone is depicted on *Exhibit 22, Proposed Zoning*. As discussed in Chapter I, Section C, these areas are subject to transfer and redesignation to meet market conditions appropriate to a project of this size and duration, and under the modification procedures outlined in this Specific Plan. It is the intent of this Specific Plan that residential development shall be allowed if the proposed development meets the requirements of the applicable zoning/land use designations or the requirements of lower density zones. A description of each zone's purpose follows:

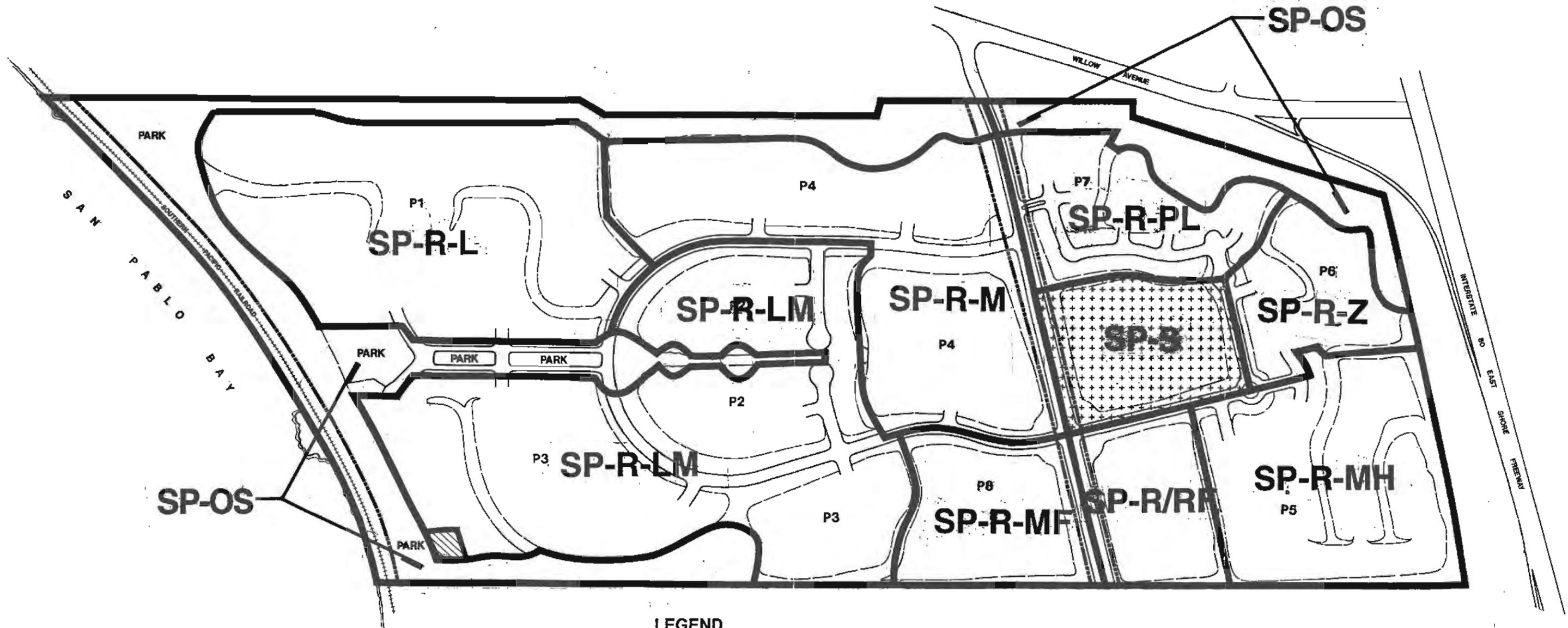
Residential Low Density (SP-R-L): This Zone is intended to accommodate residential development characterized by single-family detached residential units on large lots a minimum of 6,000 square feet in size, with a maximum density of 2.0-8.0 units per acre.

Residential Low Medium Density (SP-R-LM): This Zone is intended to accommodate residential development consisting of single-family detached units on mid-sized lots averaging 5,000 - 5,500 square feet in size, realizing a maximum density of 2.0-9.0 units per acre.

Residential Medium Density (SP-R-M): This Zone is intended for single-family detached residential development on medium sized lots at a parcel minimum of 4,000 square feet, with an average density of up to 13.0 units per acre.

Residential Medium High Density (SP-R-MH): This Zone is intended to accommodate single-family detached residential development on medium sized lots at a minimum of 3,375 square feet, with an average density of up to 13.0 units per acre.

Residential Z-Lot (SP-R-Z): The Z-Lot Zone is a planning tool used to allow separate ownership of the land, but to accommodate a higher density on small lots by creatively designing lot configurations and dwellings. Detached single-family dwelling units and garages are sited so that they maintain the feel, privacy and function of a single-family detached unit on a larger lot. This concept accommodates single-family detached units at higher densities on smaller lots while maximizing open space and preserving privacy. Lots would be a minimum size of 3,037 square feet in area, allowing a density of up to 15.0 units per acre. Mapping may provide for detached condominium application where appropriate.



LEGEND

| | | | |
|---------|---------------------------------|---------------------|------------------------------|
| SP-R-L | RESIDENTIAL LOW DENSITY | SP-R-MF | RESIDENTIAL MULTI-FAMILY |
| SP-R-LM | RESIDENTIAL LOW MEDIUM DENSITY | SP-R/RF | RETAIL/RESIDENTIAL FLEX |
| SP-R-M | RESIDENTIAL MEDIUM DENSITY | SP-S | SCHOOL |
| SP-R-MH | RESIDENTIAL MEDIUM HIGH DENSITY | SP-OS | OPEN SPACE/PARKS |
| SP-R-Z | RESIDENTIAL Z-LOT | [Hatched Box] | TREATMENT PLANT OVERLAY ZONE |
| SP-R-PL | RESIDENTIAL POCKET LOTS | [Cross-hatched Box] | SP-R-PL OVERLAY ZONE |

Residential Pocket Lots (SP-R-PL): This Zone is designed to accommodate small single-family detached dwelling units on regularly shaped small lots averaging 2,668 square feet in size, while maintaining the minimum setbacks from property lines. The density will range up to 17.0 units per acre.

Residential Multi-Family (SP-R-MF): This Zone has been created to accommodate multi-family attached dwelling units, allowing townhomes, condominiums and rental units at a density between 12.0-30.0 units per acre. However, should the market indicate that the need for another product type is stronger than the multi-family residential product, the Specific Plan allows for other product types at a lower density to be designed in this area, provided that a maximum of 880 units for the entire New Pacific Properties Specific Plan area is not exceeded.

Retail/Residential Flex (SP-R/RF): The intent of the Retail/Residential Flex Zone is to accommodate either a retail facility or residential development or a combination thereof. Because the City of Hercules General Plan goal and objectives strive to expand and diversify the City's economic base to generate sufficient revenues for providing public services to its residents, a retail component has been accommodated in the New Pacific Properties Specific Plan Area. However, market demand for commercial development at this location is dependent on competitive influences and other factors within and outside of the City. The plan also allows development of residential uses. If a residential project is pursued, it would most likely be developed at a density of up to 17.0 units per acre and be included in the New Pacific Properties Residential Pocket Lot (SP-R-PL) zone. Multi-family units may also be considered.

School (SP-S): This 7.5-acre Zone is planned to accommodate an elementary school facility, consisting of the school building and vehicular drop off area, with a recreational playground/field park. This site is also subject to the SP-R-PL Overlay Zone as described below.

Open Space/Parks (SP-OS): This Zone is intended to accommodate a variety of open space and park uses, ranging from Enhanced Parkways to natural open space hillsides. Other recreational uses will include a Shoreline Trail/Park, a Neighborhood Park adjoining the Shoreline Trail/Park, a Community Trail Park activity spine, a Linear park and other open space areas throughout the Specific Plan area.

Treatment Plant Overlay Zone: A small portion of the P3 planning area is designated with this overlay zone, as shown on *Exhibit 22*, to provide for wastewater pumping facilities, and for the possible development of a wastewater treatment facility on-site. Should an alternative solution be pursued which does not require an on-site facility, this zone will be developed under its SP-R-LM designation.

SP-R-PL Overlay Zone: An overlay zone has been created for the school site to provide for its development should a school use not be pursued. If the West Contra Costa County Unified School District has not purchased the proposed school site within two years of adoption of this Specific Plan, the standards of the SP-R-PL overlay zone will be applied to this site.

NEW PACIFIC PROPERTIES SPECIFIC PLAN

C. PERMITTED USES IN EACH LAND USE ZONING DISTRICT

The following table sets forth the uses permitted in each Zone within the New Pacific Properties Specific Plan area:

| TABLE 3 PERMITTED USES | | | | | | | | | | |
|---|----------------------|----------------|----------------|----------------|--------|---------|----------------|----------------------|----------------|----------------|
| P = Permitted A = Administrative Use Permit C = Conditional Use Permit X = Not Permitted | Land Use Designation | | | | | | | | | |
| | P1 | P2/P3 | P4 | P5 | P6 | P7 | P8 | R/RF | S | OS |
| | Zone | | | | | | | | | |
| LAND USE | SP-R-L | SP-R-LM | SP-R-M | SP-R-MH | SP-R-Z | SP-R-PL | SP-R-MF | SP-R/RF ¹ | SP-S | SP-OS |
| Accessory Uses & Structures | A ² | A ² | A ² | A ² | X | X | A ² | A ² | A ² | A ² |
| Adult Entertainment | X | X | X | X | X | X | X | X | X | X |
| Ambulance & Emergency Services | X | X | X | X | X | X | X | C | X | X |
| Amusement Centers/Arcades | X | X | X | X | X | X | X | C | X | X |
| Animal Hospital/Clinic | X | X | X | X | X | X | X | C | X | X |
| Antennae, Satellite Dishes and Ham Operations | X | X | X | X | X | X | X | C | X | X |
| Athletic Clubs | X | X | X | X | X | X | C | C | X | X |
| Auto, Mobile Home Equipment Sales/Rental | X | X | X | X | X | X | X | X | X | X |
| Auto Fuel, Service & Mini-marts | X | X | X | X | X | X | X | C | X | X |
| Bars | X | X | X | X | X | X | X | C | X | X |
| Batting Cages | X | X | X | X | X | X | X | X | X | C |
| Bed & Breakfast Inns | X | X | X | X | X | X | C | C | X | X |
| Building Material Sales | X | X | X | X | X | X | X | X | X | X |
| Business Support Services | X | X | X | X | X | X | X | A | X | X |
| Caretaker & Employee Housing | C | C | C | C | C | C | C | X | X | C |
| Clubs, Clubhouses & Lodges | X | X | C | C | C | C | C | C | X | C |
| Commercial Athletic Facilities | X | X | X | X | X | X | C | C | C | C |
| Daycare Facilities | X | X | X | X | X | X | X | C | C | X |
| Drive-in & Drive-through Sales & Services | X | X | X | X | X | X | X | C | X | X |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

TABLE 3
PERMITTED USES

| LAND USE | Land Use Designation | | | | | | | | | |
|---------------------------------------|----------------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|----------------------|---------------------|-----------------|
| | P1 | P2/P3 | P4 | P5 | P6 | P7 | P8 | R/RF | S | OS |
| | Zone | | | | | | | | | |
| | SP-R-L | SP-R-LM | SP-R-M | SP-R-MH | SP-R-Z | SP-R-PL | SP-R-MF | SP-R/RF ¹ | SP-S | SP-OS |
| Dry Cleaners | X | X | X | X | X | X | X | A | X | X |
| Duplex | X | X | C | C | C | C | C | C | X | X |
| Family Day Care Homes | C | C | C | C | C | C | C | C | C | X |
| Golf Course/Driving Range | X | X | X | X | X | X | C | C | C | C |
| Government Offices | X | X | X | X | X | X | X | C | C | X |
| Grocery | X | X | X | X | X | X | X | A | X | X |
| Guest House | X | X | X | X | X | X | X | X | X | X |
| Home Occupation | P | P | P | P | P | P | P | P ⁶ | X | X |
| Hospitals & Laboratories | X | X | X | X | X | X | X | C | X | X |
| Hotels, Motels & Inns | X | X | X | X | X | X | X | C | X | X |
| Libraries, Museums & Galleries | X | X | X | X | X | X | C | C | C | X |
| Liquor Sales | X | X | X | X | X | X | X | C | X | X |
| Mail Order Sales & Vending | X | X | X | X | X | X | X | A | X | X |
| Medical & Dental Clinics | X | X | X | X | X | X | X | A | X | X |
| Mini Storage | X | X | X | X | X | X | X | X | X | X |
| Mobile Home/Trailer Building | X ^{3b} | X ^{3b} | X ^{3b} | X ^{3b} | X ^{3b} | X ^{3b} | X ^{3b} | X ^{3b} | C ^{3a, 3b} | X ^{3b} |
| Mortuaries, Columbariums & Cemeteries | X | X | X | X | X | X | X | X | X | X |
| Multi-Family Dwelling/Condo | C | C | C | C | P | P | P | P | X | X |
| Offices & Banks | X | X | X | X | X | X | X | A | X | X |
| Outdoor Retail Sales & Services | X | X | X | X | X | X | X | C | X | X |
| Parks & Recreation Facilities | A | A | A | A | A | A | A | A | A | A |
| Planned Unit Development | A | A | A | A | A | A | A | A | X | X |
| Public Safety Facilities | A | A | A | A | A | A | A | A | A | A |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| TABLE 3 PERMITTED USES | | | | | | | | | | |
|---|----------------------|------------------|--------|---------|--------|---------|---------|----------------------|------|-------|
| P = Permitted A = Administrative Use Permit C = Conditional Use Permit X = Not Permitted | Land Use Designation | | | | | | | | | |
| | P1 | P2/P3 | P4 | P5 | P6 | P7 | P8 | R/RF | S | OS |
| | Zone | | | | | | | | | |
| LAND USE | SP-R-L | SP-R-LM | SP-R-M | SP-R-MH | SP-R-Z | SP-R-PL | SP-R-MF | SP-R/RF ¹ | SP-S | SP-OS |
| Recreation & Entertainment | X | X | X | X | X | X | X | A | C | C |
| Recycling Centers | X | X | X | X | X | X | X | X | C | X |
| Religious Assembly ⁴ | X | X | X | X | X | X | C | C | C | X |
| Residential Day Care (8 or less children) | A | A | A | A | A | A | A | A | A | X |
| Residential Day Care (7 to 14 children) | X | X | X | X | X | X | C | C | A | X |
| Repair & Maintenance | X | X | X | X | X | X | X | C | X | X |
| Restaurants & Delicatessens | X | X | X | X | X | X | X | A | X | X |
| Restaurants & Delicatessens with Liquor Service | X | X | X | X | X | X | X | C | X | X |
| Retail Stores | X | X | X | X | X | X | X | A | X | X |
| Rooming & Boarding Houses (4 or less people) | X | X | X | X | X | X | X | X | X | X |
| Rooming & Boarding Houses (5 or more people) | X | X | X | X | X | X | X | X | X | X |
| Schools, Public or Private | X | X | X | X | X | X | X | A | P | X |
| Second Residential Unit | A | A | A | A | A | A | A | X | X | X |
| Senior Congregate Care | X | X | X | X | X | X | C | C | X | X |
| Senior Housing | C | C | C | C | C | C | A | X | X | X |
| Service Station | X | X | X | X | X | X | X | A | X | X |
| Shopping Centers | X | X | X | X | X | X | X | C | X | X |
| Single-Family Attached, Duet | C | C | C | C | P | P | P | P | X | X |
| Single-Family Detached | P | P | P | P | P | P | P | P | X | X |
| Tennis Facility (non-commercial) | C | C | C | C | C | C | A | A | A | C |
| Townhouse/Condominium | X | X | X | X | P | P | P | X | X | X |
| Utility Facilities | C | C/A ⁷ | C | C | C | C | C | C | C | C |
| Urgent Medical Care | X | X | X | X | X | X | X | A | C | X |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| TABLE 3 PERMITTED USES | | | | | | | | | | |
|---|----------------------|---------|--------|---------|--------|---------|---------|----------------------|------|-------|
| P = Permitted A = Administrative Use Permit C = Conditional Use Permit X = Not Permitted | Land Use Designation | | | | | | | | | |
| | P1 | P2/P3 | P4 | P5 | P6 | P7 | P8 | R/RF | S | OS |
| | Zone | | | | | | | | | |
| LAND USE | SP-R-L | SP-R-LM | SP-R-M | SP-R-MH | SP-R-Z | SP-R-PL | SP-R-MF | SP-R/RF ¹ | SP-S | SP-OS |
| Vehicle Repair & Maintenance | X | X | X | X | X | X | X | C | X | X |
| Veterinary Clinics & Hospitals | X | X | X | X | X | X | X | C | X | X |
| Video/Media Rentals ⁵ | X | X | X | X | X | X | X | A | X | X |
| Visitor Accommodations | X | X | X | X | X | X | X | C | X | X |
| Warehouses & Storage Facilities | X | X | X | X | X | X | X | X | X | X |
| Wholesale Sales | X | X | X | X | X | X | X | C | X | X |

- ¹ Permitted uses apply to retail option only. If residential option is pursued, permitted uses shall be as noted in the SP-R-PL or SP-R-MF zones.
- ² Single story detached accessory structures under 100 square feet total are exempt from use permit requirements.
- ^{3a} Mobile homes used for temporary classroom purposes only may be considered through a Conditional Use Permit.
- ^{3b} Mobile units are permitted for temporary construction and site development related sales activities.
- ⁴ No religious assembly uses are proposed as part of this Specific Plan. Impacts from future religion assembly uses are not considered in the EIR and would require mitigation resolution at the time of their proposal.
- ⁵ Excluding Adult Entertainment video/media rentals.
- ⁶ Only if located in a residential unit.
- ⁷ Permitted with an Administrative Use Permit within the Treatment Plant Overlay Zone only.

D. DEVELOPMENT STANDARDS

Table 4, Development Standards, on the following pages sets forth the building setbacks, height, lot coverage and other requirements for development of property within the various zones of the New Pacific Properties Specific Plan. Additional development standards within the Specific Plan area follow. Where there is a conflict between the development standards set forth herein and the City of Hercules Zoning Ordinance, the standards of the New Pacific Properties Specific Plan shall apply. Where silent, the development standards of the City of Hercules Zoning Ordinance shall apply.

TABLE 4
DEVELOPMENT STANDARDS

| STANDARD | Land Use Category | DEVELOPMENT STANDARDS | | | | | | | | | | | OS |
|---|--------------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------|----------------------|-------------------|------------------|---|--|----|
| | | P1 | P2 | P3 | P4 | P5 | P6 | P7 | P8 | FLEX | S | | |
| Zone | SP-R-L | SP-R-LM | SP-R-M | SP-R-MH | SP-R-Z | SP-R-PL | SP-R-MF | SP-R/RF ¹ | SP-S ² | | | | |
| Lot Size Minimum (sq. ft.) | 6,000 | 5,500/5,000 | 4,000 | 3,375 | 3,037 | 2,668 | - | 10,000 | - | | | | |
| Avg. Lot Frontage Minimum (ft.) | 30 | 25 | 25 | 25 | 30 | 20 | - | 40 | 200 | | | | |
| Lot Depth Minimum (ft.) | 100 | 100 | 80 | 75 | 72 | 58 | - | 140 | 200 | | | | |
| Avg. Lot Width Minimum (ft.) | 60 | 55/50 | 50 | 45 | 37.8 | 46 | - | 70 | 200 | | | | |
| Maximum (Units/Acre) | 2.0-8.0 | 2.0-9.0 | up to 13.0 | up to 13.0 | up to 15.0 | up to 17.0 | 12.0-30.0 | - | - | | | | |
| Maximum FAR | - | - | - | - | - | - | - | 0.25 | - | | | | |
| Height ² (feet/stories) | 40/22 | 35/22 | 35/22 | 35/22 | 45/22 | 45/22 | 60/4 | 45/2 | 35/2 | | | | |
| Building Setback ³ & ⁷ | | | | | | | | | | | | | |
| Front - to Garage(ft.) | up to 10 or at least 18' | 18/40 ⁸ | 15 | - | 30 | - | | |
| Front with Parkway - to Main Structure (ft.) | 8 | 8 | 8 | 8 | 8 | - | 10/12 ⁸ | 15 | 20 | 20 | - | | |
| Front without Parkway - to Main Structure (ft.) | 10 | 10 | 10 | 10 | 10 | 10 | 10/12 ⁸ | 15 | 20 | 20 | - | | |
| Rear ⁶ (ft.) | 15 | 15 | 10 | 10 | 5 | 5 | 5 | 15 | 0/20 ⁵ | 20 | - | | |
| Side (ft.) | 5 | 5 | 5 | 5 | 4 | 4 | 4 | 15 | 10 | 20 | - | | |
| Corner Side (ft.) | 10 | 10 | 10 | 10 | 10 | 10 | 15 | 15 | 15 | 20 | - | | |
| Lot Coverage Maximum (%) | 50 | 50 | 50 | 50 | 70 | 75 | 60 | City Zoning Ord. | City Zoning Ord. | City Zoning Ord. | - | | |
| Parking Spaces Required | | | | | | | | | | | | | |
| Spaces in a Garage Per Unit | 2 | 2 | 2 | 2 | 2 | 2 | City Zoning Ord. | - | - | - | - | | |
| Standard Garage Dimensions | | | | | | | | | | | | | |
| City Zoning Ordinance | | | | | | | | | | | | | |

*Subject
for review*

TABLE 4
DEVELOPMENT STANDARDS

| STANDARD | Land Use Category | P1 | P2 | P3 | P4 | P5 | P6 | P7 | P8 | FLEX | S | OS |
|---|-------------------|--------|---------|--------|---------|--------|---------|---------|----------------------|------------------|-------------------|------------------|
| | | SP-R-L | SP-R-LM | SP-R-M | SP-R-MH | SP-R-Z | SP-R-PL | SP-R-MF | SP-R/RF ¹ | | SP-S ⁹ | SP-OS |
| Stalls / 100,000 sq. ft. gross floor area + stalls / employee | - | - | - | - | - | - | - | - | - | City Zoning Ord. | - | - |
| Per Gross Floor Area | - | - | - | - | - | - | - | - | - | City Zoning Ord. | - | City Zoning Ord. |

- 1 The Residential Standards of the SP-R-PL zone shall apply if residential development is pursued.
- 2 Towers, spires, cupolas, chimneys, skylights, flagpoles, monuments, and similar structures may be erected to a height not more than 10 feet above the height limit, subject to design review by the Planning Director. TV antennas are prohibited, satellite dish antennas 12@ or less in diameter are permitted provided they blend with the architectural style of the dwelling.
- 3 Architectural features including sills, chimneys, cornices and eaves may extend into a side yard or space between structure no more the 30" and may extend into a front or rear yard not more than 3 feet. Open, unenclosed, uncovered balconies, porches, platforms, stairways, and landing places may extend into a side yard not more than 3 feet unless it is more than 4 feet above ground. If it is greater than 4 feet above ground, if it is covered or is used for occupancy, such as a window seat or green house window projection, the minimum setback requirements must be observed. Porte Cocheres shall meet all setback requirements of a residence. Fences, walls, walks, driveways and retaining walls may occupy setbacks subject to the limitations described in Chapter V, Design Guidelines. Utility poles and utility towers installed by public utility shall not be subject to height limits.
- 4 Front yard setback from the front property line to the garage shall be no greater than 10 feet or no less than 18 feet.
- 5 Minimum rear lot setback for commercial development shall be 20 feet when adjacent to existing residential or residentially zoned areas.
- 6 All buildings shall be setback a minimum of 8 feet from the toe of any slope exceeding 5 feet in height or with a gradient of 10% or more.
- 7 Specific Plan standards may be modified to be less restrictive through a separate application for design review, provided the overall Specific Plan project character and design is maintained.
- 8 Front yard setback shall be no less than 18 feet from the front property line to the garage, and no less than 10 feet from the front property line to the main structure/dwelling for a maximum of one unit per adjoining two-pack lots. Front yard setback for the adjoining unit shall be no less than 40 feet from the front property line to the garage and no less than 12 feet from the front property line to the main structure dwelling. End units, not paired in a two pack configuration, may be plotted using either setback requirement.
- 9 If the school site is not purchased by the West Contra Costa County School District within two years of adoption of this Specific Plan, the standards of the SP-R-PL zone shall apply.

E. Supplemental Standards

1. Project-Wide

- Temporary modular buildings such as portable buildings or mobile homes are prohibited.
- Temporary job site trailers associated with on-site construction and related sales activities are permitted during construction and sales activities.

2. Retail Uses

- All materials, supplies, or products associated with retail use shall be stored inside the building, or a screened permanent structure may be constructed to screen the storage area from public and abutting residential areas.
- No on-street loading or unloading is permitted.
- Open burning, blasting or discharge of explosives in any form is prohibited on site.
- No noxious or offensive activity shall be conducted upon any retail lot. Examples of noxious or offensive activities shall include keeping or raising of farm animals, barking dogs (except for veterinary clinics), excessive vibrations, sounds, electro-mechanical disturbances, radiation, air or water pollution, dust or emission of odorous, toxic or noxious matters.
- No exterior speakers, horns, whistles, bells or other sound devices, or searchlights shall be permitted, except as permitted under a Temporary Use Permit.
- Trash receptacles shall be screened from adjacent roads and development.

3. Parks and Open Space

- Amplification of voice or music shall not be permitted within park facilities except under temporary City permit.
- Park use within the New Pacific Properties Specific Plan area shall primarily be passive rather than active, with the exception of the School Facility park and play field and neighborhood park areas.
- Passive interpretive kiosks shall be permitted within the Natural Open Space park areas.
- Architectural design and landscape standards are described in Chapter V, *Design Guidelines*, of this document.

4. Parking/Loading

Parking Standards of Chapter 32 of the City of Hercules Zoning Ordinance shall apply to development within the New Pacific Properties Specific Plan area, except as enumerated herein. The minimum number of parking stalls shall be as set forth herein, and in Table 32-1 *Off-Street Parking and Loading Requirements* and Table 32-2 *Handicapped Parking Requirements* of the City of Hercules Zoning Ordinance (not included in this document). If a conflict arises between this Specific Plan's regulations and the City of Hercules, then the standards contained in this Chapter shall take precedence. In addition, the following standards shall apply:

a. Parking Standards

- Parking landscape islands shall be a minimum of four feet (4') wide by four feet (4') deep, and be enclosed by a minimum 6" concrete curb.
- Parking area landscape planters shall be a minimum of two feet (2') wide.
- When parking areas front a roadway, they shall be separated with a minimum ten foot (10') wide planter containing a minimum of one tree for every twenty five feet (25').
- Commercial and Multi-family residential parking areas shall be screened from public view to the extent practicable by vegetation a minimum of 3 feet in height.

b. Residential Parking

- Aside from guest parking, it is the intent that vehicles shall not be left on the streets during evening hours.
- Multi-family development within the New Pacific Properties Specific Plan area will be required to provide one (1) space within a garage or carport per unit.
- Accessory vehicles, such as motorcycles, motor homes, boats, personal water craft, all terrain vehicles, etc. shall be parked within an enclosed garage or screened from view.
- Parking areas, enclosed or otherwise, shall observe the setbacks of that zoning district.

c. Retail/Residential Flex Parking

- Off-street parking facilities and driveways may be used temporarily for special events, provided it does not interfere with vehicular or pedestrian circulation or occupy parking spaces.
- Entrance to and vertical clearances within parking structures shall have a minimum vertical clearance of ten feet (10').
- Loading and unloading docks, platforms, and vehicle or other service openings are not permitted along the portion of a building facing a street. Such openings must be at the rear or along the side of a building not facing a street and must be screened from view.
- No on-street loading or unloading is permitted within the New Pacific Properties Specific Plan area.
- No on-street parking is permitted.
- If the residential option is developed, the residential parking standards shall apply.

d. Bicycle Parking/Pathways

Bicycle activity within the New Pacific Properties Specific Plan is to be encouraged to help promote businesses, encourage a healthy lifestyle, reduce vehicular traffic and implement the General Plan and regional/City bikepath plans. The New Pacific Properties Specific Plan incorporates bicycle paths/trails, which will connect to both the City's bikepath plan and the Bay area regional trail system. To further encourage and facilitate bicycle activity within the New Pacific Properties Specific Plan area, the following standards shall apply to all development:

- All development, renovations or improvements to the New Pacific Properties' interior loop roadway and San Pablo Avenue shall include bike lanes on both sides.

- Roadway bike lanes shall connect to openspace and park pedestrian paths, and eventually connect to the regional Shoreline Trail/Park bikepath.

5. Special Treatment Standards

a. Accessory Structures and Uses

- No accessory structure may encroach into the setbacks specified herein, unless so permitted as stated below.
- Aside from patios, decks, trellis, and gazebos, accessory structures shall be screened from view from public and private streets.
- In-ground pools and spas/jacuzzis may be constructed within the rear yard setback. Above ground pools and spa/jacuzzis must meet all minimum setbacks from property lines. In-ground pools and ponds associated with landscaping and not intended for human occupation and use may be constructed within the setback areas up to the property line.

b. Antennae, Satellite Dishes and Ham Radio Operations

- Antennae, satellite dishes and ham radio operations shall be strictly prohibited within the New Pacific Properties Specific Plan area without a Conditional Use Permit.
- Satellite dish antennae 12" or less in diameter are permitted provided that they blend with the architectural style of the dwelling.

c. Home Occupations/Business

- No interior or exterior signage or display of products of any type, size or style shall be permitted in association with any home occupation or business established within a residential dwelling unit within the New Pacific Properties Specific Plan area.
- Home businesses which involve the following shall be prohibited in addition to those home occupations prohibited in Section 35.270 of the City of Hercules Zoning Ordinance:
 - mechanical equipment except as is normally used for domestic or household purposes.
 - selling of any commodity on the premises.
 - any use which is clearly not incidental and secondary to the use of the dwelling for dwelling purposes and/or changes the character thereof.
 - encourage or depend upon frequent visits by the public.
 - involving frequent bulk shipments.

d. Outdoor Retail Sales

Development standards and conditions of Section 35.290 of the City of Hercules Zoning Ordinance shall apply to outdoor retail sales within the New Pacific Properties Specific Plan Retail/Residential Flex Zone.

e. Service Stations

The following development standards shall apply to service stations within the New Pacific Properties Specific Plan in addition to the development standards and conditions of Section 35.330 of the City of Hercules Zoning Ordinance:

- Service Stations shall be conditionally permitted within the Retail/Residential Flex Zone provided all other standards of Section 35.330 of the City of Hercules Zoning Ordinance are followed.
- Minor automobile repair, service or maintenance shall be permitted.
- Service station bays used for repair and maintenance of vehicles shall be oriented away from streets and public views.
- A minimum ten-foot (10') wide landscape planter shall be required along all street boundaries, between the roadway and station gas pumping and vehicular circulation areas.
- Associated repair structures and convenience stores shall be located to the rear of the lot.
- Accessory car wash facilities shall also be located to the rear of the lot, and be setback a minimum of 50' from a residential district.
- Hours of operation of any accessory car wash shall be governed through a Conditional Use Permit.

f. Special Events

- Temporary outdoor special events such as a car wash fund raiser, weekly farmers markets, Christmas Tree sales, Halloween pumpkin sales, etc. shall be located within the parking area of a retail/commercial/school zone provided it does not interfere with vehicular or pedestrian circulation, or occupy needed parking spaces. Such activities shall be required to obtain a Temporary Use Permit.
- Special events shall be limited to no more than two (2) days in duration, with the exception of Christmas Tree and Halloween pumpkin sales which may operate for a time period approved by the City.

V. DESIGN GUIDELINES

A. URBAN DESIGN PLAN OVERVIEW

The New Pacific Properties Specific Plan area is envisioned to be a blend of single- and possibly multi-family residential and retail uses in a pedestrian friendly setting with ample parks and open spaces. Set in a dramatic location which naturally opens itself to San Pablo Bay, design guidelines have been developed to appreciate this asset, and reinforce the linkage to the shoreline and recreational opportunities it offers. Design guidelines provide an overall vision for development of the Specific Plan area, and will guide implementation of the plan as building occurs. An important goal is to encourage a development pattern that is an alternative to suburban sprawl, and to offer guidelines which reinforce traditional neighborhood concepts. This will be achieved by creating an urban design framework which will be carried throughout the entire Specific Plan area, while accommodating variety and individuality in the context the design program. Pedestrian friendly street designs and trail connections will be created while locating neighborhood services so that walking and biking is convenient, fun and safe. The architectural guidelines shall be thematic but shall provide for variety in application.

B. SITE PLAN DESIGN GUIDELINES

Guidelines for site planning, architectural character, landscape architecture and signage are provided to create a cohesive whole while providing for diversity and discovery throughout the community.

1. Lotting Concepts

- The general lotting concept as shown on *Exhibit 10, Conceptual Lotting Plan*, shall be conceptual and flexible in terms of layout, location, and specific acreage so long as the overall project density, residential mix and major community framework elements such as parks, open space, trails, and school site remain at their general locations within the New Pacific Properties Specific Plan.
- Lotting should accommodate pedestrian friendly design, encouraging walking and biking throughout the Specific Plan area, and connecting to trails outside the project.
- Neighborhoods should provide easy pedestrian access to open space trails and parks.
- Cul-de-sacs that open onto landscaped areas, open space, parks, and trails shall be retained to the extent feasible in any future refined lotting scheme.
- Roadways should be aligned to afford views and vistas to San Pablo Bay.

2. Grading Criteria

Final grading shall be reflective of the general terraced condition of the New Pacific Properties Specific Plan as depicted in the Conceptual Grading Plan. A maximum slope of 2:1 will be utilized throughout the project. Some retaining walls will be incorporated into the grading scheme to maximize views and functionality of the site. Retaining walls will not exceed 5 feet in height except as reviewed and approved in conjunction with City standards. Grading on site will be divided into three phases, each of which can balance independently of

the others. To ensure that grading is performed in a manner sensitive to aesthetic values, the following recommendations should be considered in reviewing grading plans:

- All grading and development of parcels shall be designed to direct surface drainage to street frontages or approved drainage devices. Drainage shall not be directed onto neighboring lots.
- All graded slopes shall be revegetated in a timely manner with plant materials consistent with the approved plant materials palette to help minimize erosion.

3. Siting Criteria

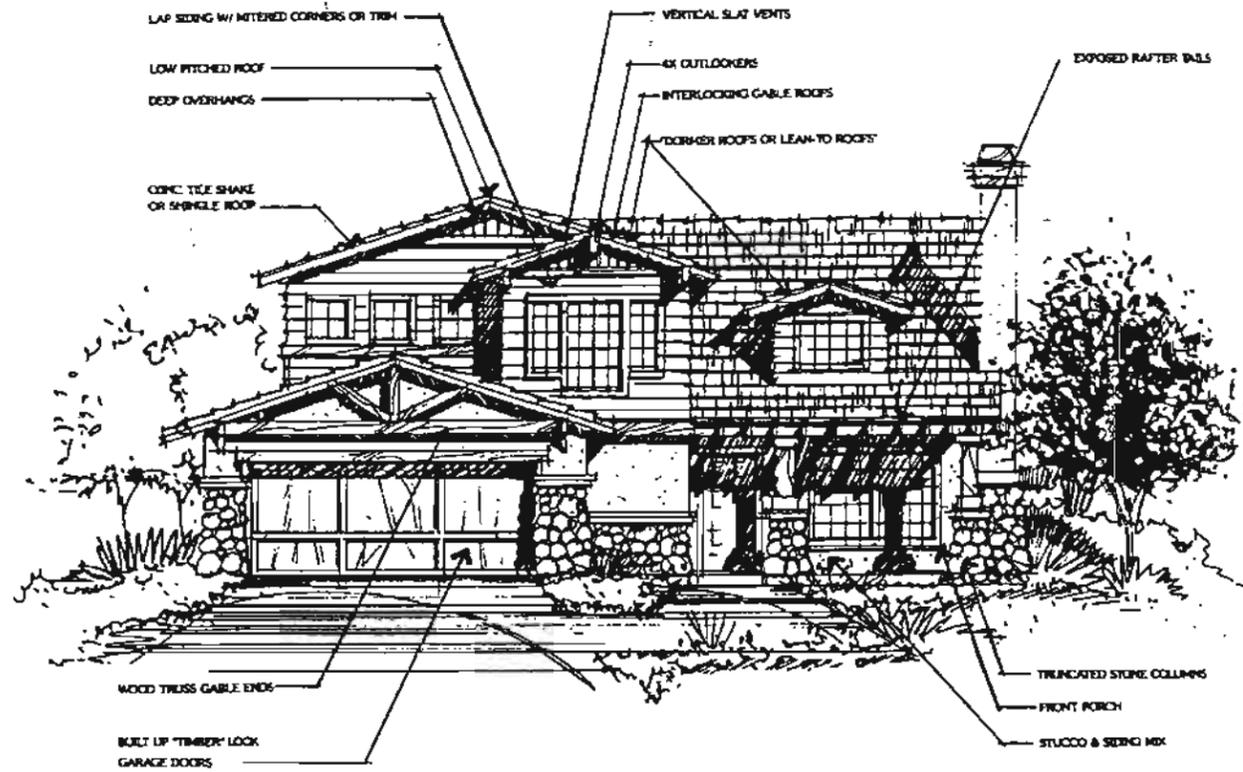
- Units shall be sited to retain/maintain views and vistas to San Pablo Bay to the extent possible.
- Buildings should be sited to front onto the public streets in the neighborhood unless incorporated into an integrated format which provides good design parameters. When buildings side or back onto a public street, the elevations should be articulated with details, windows, projections and other devices to enhance the adjacent street and pedestrian character.
- Varying front/street views shall be considered in siting of various models in residential tracts.
- Special attention shall be given to siting and design of development along San Pablo Avenue.
 - The number of windows facing San Pablo Avenue should be minimized or otherwise designed to mitigate roadway noise impacts to the residential units.
 - Exterior building walls facing San Pablo Avenue should be varied architecturally so as to provide off-sight visual relief.
 - Long, solid, run-on walls facing San Pablo Avenue should be avoided. Although the walls along San Pablo Avenue should be uniform in terms of design and materials uses, it should be sited and staggered to provide a variety in relief. This effect may be obtained through the use of pilasters or other design elements.
- Only one driveway access shall be permitted per residential lot.
- Porte Cocheres shall meet all setback requirements of a residence, as defined in Chapter IV, *Development Standards*, Section D.1.

C. ARCHITECTURAL DESIGN GUIDELINES

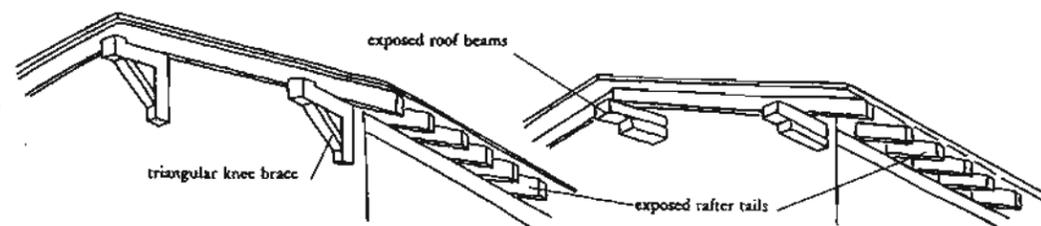
A Traditional American design theme has been selected to provide a conservative, traditional flavor to the community, in keeping with the resurgence of traditional family values. Prominent, functional front porches which encourage interaction with the neighborhood are typical. Special architecture and landscape design criteria have been developed for the residential streets to create warm and friendly street scenes that will encourage walking and socialization within the neighborhood, yet afford a degree of privacy.

The purpose and intent of the architectural guidelines for New Pacific Properties Specific Plan area is to encourage design that will have a distinct identity while expressing a thoughtful integration of building structures in a quality, master-planned community. These guidelines will provide the City of Hercules with the necessary assurances that this community will develop in

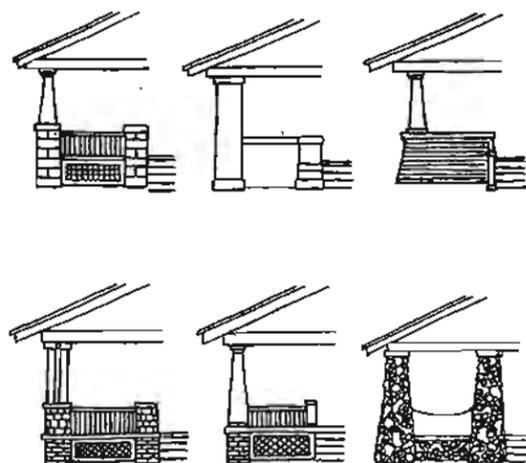
BUNGALOW/CRAFTSMAN ARCHITECTURAL STYLE



BUNGALOW/CRAFTSMAN ARCHITECTURAL ELEMENTS

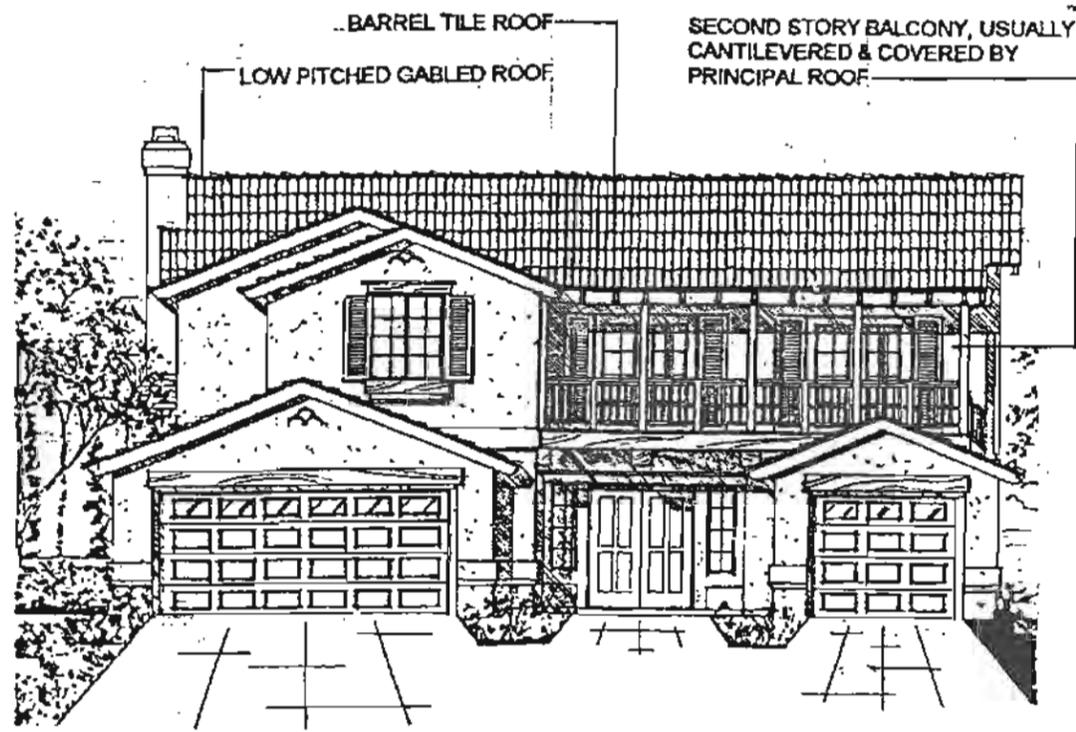


TYPICAL ROOF-WALL JUNCTIONS

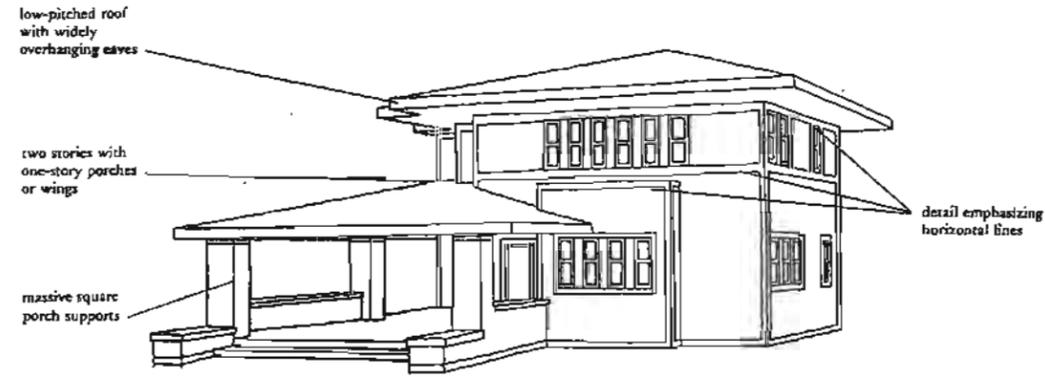


TYPICAL PORCH SUPPORTS AND PORCH RAILINGS

MONTEREY ARCHITECTURAL STYLE



ADDITIONAL APPROPRIATE ARCHITECTURAL STYLES



PRAIRIE ARCHITECTURAL STYLE

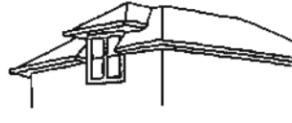


VICTORIAN ARCHITECTURAL STYLE

ADDITIONAL APPROPRIATE ARCHITECTURAL ELEMENTS



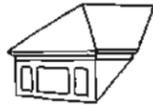
gable



through-cornice

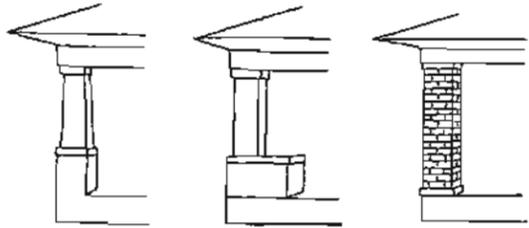


gable
most common

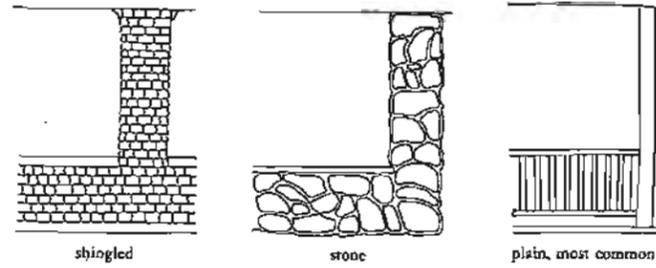


hipped

TYPICAL PRAIRIE
DORMERS



TYPICAL VICTORIAN
DORMERS

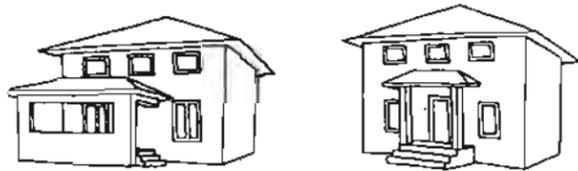


shingled

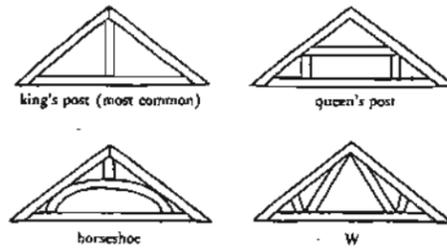
stone

plain, most common

TYPICAL PRAIRIE
PORCH SUPPORTS



TYPICAL VICTORIAN
PORCH SUPPORTS



king's post (most common)

queen's post

horseshoe

W

TYPICAL PRAIRIE
PORCH VARIANTS

TYPICAL VICTORIAN
DECORATIVE TRUSSES

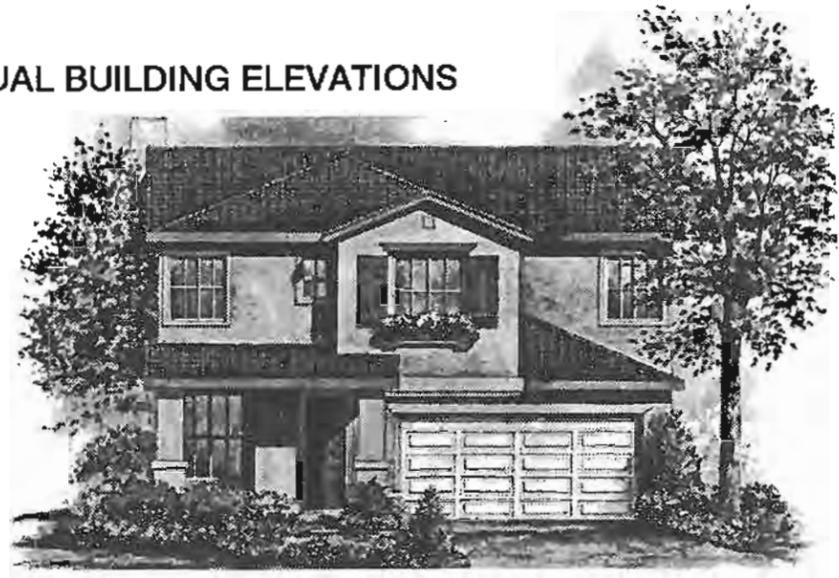
CONCEPTUAL STREET ELEVATIONS



ARCHITECTURAL DESIGN
CHARACTER (RESIDENTIAL)
NEW PACIFIC PROPERTIES

pbr
11-18-09
EXHIBIT 23f

CONCEPTUAL BUILDING ELEVATIONS



ARCHITECTURAL DESIGN
CHARACTER (RESIDENTIAL)
NEW PACIFIC PROPERTIES

pbr
11-10-00
EXHIBIT 23g

a. Building Mass

- A key technique for creating a sense of variety within a residential neighborhood is to vary the heights and forms of the homes as seen from the street. In the case of low and medium density homes or projects, this can be accomplished by utilizing both one- and two-story elements on the same building, varying the roof plan and creating offsets in the vertical planes of the building.
- Building massing should be articulated and complex to the extent practical to reflect the historical context of the design guidelines.
- Building mass shall be broken up when viewed from streets, including San Pablo Avenue, through varied exterior building wall alignment and landscaping.
- Dormers, secondary gables and other projections may be used to break up simple architectural forms, but are not required.
- Single-Family Residential Dwelling units shall be limited to 2 stories in height, as specified in Chapter IV, *Development Standards*.

b. Materials and Colors

To achieve architectural diversity, a variety of materials and colors will be considered acceptable.

- Exterior building materials shall be wood or wood-like siding, cement, stucco or plaster.
- Heavy textured, swirl or heavy troweled cement, stucco or plaster (having patterns of deep trowel marks in the finished stucco surface) is prohibited.
- The following exterior finish materials are prohibited:
 - mica plaster
 - unstuccoed or unfinished standard precision block
 - unfinished concrete "tilt-up", painted or white brick are not acceptable
 - gloss-glazed ceramic tile
 - plywood
 - plastic or vinyl materials*
 - particle board
 - metal (aside from window frames, garage doors, and wrought iron fencing)
- * Vinyl or other synthetic materials may be permitted provided it is designed and textured to appear as wood, or other natural materials.
- Natural wood or wood-like details and trim are encouraged.
- Stone, rock, brick or similar are encouraged for design accent and trim.
- A cohesive program with a diversity of tone and value in building colors is desirable.
- Colored accents are encouraged, especially on balcony rails, awnings and trim so long as the colors are cohesively integrated in an overall design program.
- Concrete, brick, stone, tile or similar paving material shall be permitted for parking and driveways within residential zones of the New Pacific Properties Specific Plan area. Asphalt paving shall be prohibited.

c. Roof Form, Materials and Colors

In keeping with the architectural concept for the area, roofs in general should be sloped.

- Roof colors and composition should vary within the project to add variety to the street scene.
- Changing roof material or form on a given plan is one of the best methods of creating alternative elevations.
- Roof characteristics should be consistent with a chosen historical style.
- Long straight roof lines should be avoided.
- Flat and shed roofs are discouraged except when used in conjunction with a gable or hip roof. Steep mansard and gambrel roofs are prohibited.
- Extreme roof pitches which create overly prominent or out-of-character buildings such as A-frames or chalet style buildings are prohibited.
- Flat roofs for terrace covers, trellises or similar structures are acceptable if designed as an architectural extension of the house.
- Roof materials are encouraged to be concrete, cement or other fire resistant materials. Treated wood shingle and composition shingle are acceptable if appropriate to the overall design. Fiberglass or metal roofing is not allowed.
- Roof colors should complement the wall and facia color. They should be of a generally neutral tone, while avoiding high-contrast colors such as bright red, deep oranges, or ceramic blue.
- Generally, roof colors will be darker than walls.
- Roofs, building materials and colors of garages should be designed to reflect the character and materials of the roof and siding of the primary residential building.

d. Elevations

To the extent historical front elevation elements such as front porches, gable ends, and dormers can be carried into new buildings, the heritage, architectural cohesiveness and pedestrian scale of the neighborhood will be strengthened.

- To the extent possible, elevations should be harmonious and compatible with the design elements of the architectural heritage.
- Although designs will not be authentic reproductions, building articulation, massing, appropriate roof forms, window forms and entries should be reflective of these architectural styles.
- Elevations should be designed with a proper visual balance and sense of cohesion. The design elements, styles and materials should not adversely contrast resulting in visual chaos.
- Front facing gables, porches, front stairs, recessed second story massing and/or dormers should be used when possible on new residential buildings, as they reflect the architectural heritage of the neighborhood.
- Functional covered balconies, decks, and usable, covered, front porches are encouraged.
- Covered arrival areas are encouraged.

e. Garages

- Each single-family residence shall include a minimum of an enclosed two-car garage, with garage door.
- Carports outside of the multi-family area are prohibited. Porte Cocheres are permitted in addition to an enclosed garage.
- Garage doors should not become the dominant element of the elevation. Street facing garage doors are allowed.

f. Articulation and Fenestration

The following design features are encouraged in the New Pacific Properties project:

- Window treatments should be consistent, compatible and authentic with the buildings established style.
- Window and door details should be kept simple and clean.
- Generally, window glass should be clear.
- Windows are encouraged along interior project streets and facing San Pablo Bay.
- Operable windows are encouraged to provide cross-ventilation.
- Shutters and coverings for windows and doors, if used, should reflect the form of the window and door and appear as a functional component rather than an attached design feature.
- Wood trim and pediments (a low triangular ornamented crowning in front of a building and over doors and windows), if used, should be consistent, compatible and authentic with the buildings established style.
- Covered doorways are encouraged to help mitigate the elements of warm summer months, and to provide protection in inclement weather.
- Garage doors shall be simple in design and should continue the architectural theme.

g. Additional Building Components

Gutters and Downspouts

- To the extent feasible, gutters and downspouts are to be placed so as not to detract from the architectural design.
- Exposed gutters and downspouts shall be painted to match adjacent roof, fascia, trim or wall colors.
- Gutters should be of high quality.

Chimneys

- No chimney or vent shall extend more than four feet (4') above the roof line unless specifically required by the City of Hercules code.
- Chimneys are to be simple in design to insure consistency of character and style.
- Simple stucco, stone, brick, slump block, masonry or wood clad chimneys are encouraged.

Antennas

- No exterior antennas or satellite dishes are allowed within the residential zones of the New Pacific Properties Specific Plan area, with the exception of satellite dish antennas that are 12" or less in diameter provided that they blend with the architectural style of the dwelling, or are screened from public view.
- Antennas and satellite dishes may be considered through a Conditional Use Permit within the Retail/Residential Flex and School zones.

Flashing, Sheet Metal and Vents

- Bare metallic surfaces (vents, pipes, gutters, flashing, etc.) should be painted or covered from view in a manner harmonious with the general architectural treatment of the building.
- All flashing, sheet metal, vents and pipes shall be painted to match the adjacent surface.
- Roof penetrations shall be minimized.
- Vents and pipe stacks should be located to the rear of side of the roof farthest from the street where feasible.
- Roof vents and stacks should be of the same color as the surrounding roof surface.

Skylights/Solar Panels

- Skylights are discouraged, however, if desired, they shall be designed as an integral part of the roof.
- All skylights and solar panels shall be located to the greatest extent possible away from the road views.
- Skylights having reflective surfaces or which reflect onto adjacent or neighboring properties shall not be allowed.
- Skylights and roof mounted solar panels are to be parallel to the roof plane and shall be clear, bronze or smoke colored plastic or glass. White glazing is not allowed.
- Frame color should blend with the surrounding roof color. Natural aluminum frames are not allowed.
- The top of the skylight should not exceed 8" in height above the roof.
- Bubble or pyramid skylight forms are prohibited unless integrated into the design elements of the structure.
- There shall be no exposed plumbing associated with solar panels.
- Support solar equipment must be screened from public and neighboring views.

3. Multi-Family Residential Design Guidelines

The design standards applicable to single-family residential dwellings shall apply to multi-family residential units, except as follows:

a. Form, Mass and Height

- Multi-family building massing should not seem boxy, but appear to result from the combination of several compatible geometric forms.
- Large monolithic buildings should be avoided.
- Reducing the height of an end unit, especially at street corners, although not required, diminishes the visual impact both at the adjacent pedestrian level, and from a distance.
- The use of long access balconies or corridors which are monotonous and impersonal should be avoided.
- To the extent possible, the entrances to individual units should be plainly visible.
- Units and buildings shall be aligned and sited with view opportunities to open space.
- Multi-family design is encouraged to provide storage units adjacent to the unit or parking space for storage of bikes, tools, etc.

b. Roof Form, Materials and Colors

- When a project includes more than one building, roofs should vary to some degree from one building to the next where practicable, in order to achieve the desired architectural interest and diversity.

c. Articulation and Fenestration

- Use offset window placement and exterior decks to preserve visual privacy, where feasible.
- To the extent feasible, avoid windows facing onto a neighbor's window or directly into their private outdoor space.

d. Open Space

- Use low walls, arbors, trellises, hedges, colonnades, etc. to define and separate adjacent outdoor spaces.
- Provide a minimum private open space of 6 feet in width adjacent to each unit, at least partially screened from adjacent outdoor spaces.
- Install street furniture to create casual plazas, squares and courts that offer opportunities for social interaction.
- Install night lighting that provides both security and an aesthetic draw for strolling.

e. Parking Lots/Garages

- Parking structures are prohibited.
- Long rows of garages should be avoided.
- Parking lots shall be lighted and designed to ensure pedestrian safety.
- The parking lot design must adhere to the standards enumerated in Chapter IV, *Development Standards*, Section D.3.
- Enclosed parking is not required in association with multi-family residential development.

- If employed, individual garages for multi-family units shall incorporate, to the extent feasible, the design elements discussed in the Single-Family Residential Section 2.e. above.
- Garage doors should be designed to provide horizontal relief in the form of overhangs, balconies, planter boxes, etc. if placed in a wall of two or more stories in height.
- Consider setting aside stall space in the parking area for car washing use by the tenants.

f. Support Facilities

- Support buildings within residential areas such as laundry facilities, recreation buildings and sales/lease offices shall be consistent in architectural design and form with the residential component.

4. Retail/Residential Flex

Since the New Pacific Properties retail flex site is only 6 acres in size, a uniform architectural style is envisioned. A "Traditional American" architectural theme is suggested, in keeping with the residential element of the Specific Plan area. Samples of architectural styles are provided in *Exhibits 24a and 24b, Architectural Design Character (Retail)*. Allowances will be made for a wider range of materials and styles while maintaining the base theme. The overriding objective in the design of the retail zone is to avoid static blandness that is the result of limited detail, accent and color. A multitude of user types are possible within the retail flex zone. To encourage compatibility while allowing for individuality, the following design guidelines are offered:

a. Form, Mass and Height

- Covered front walkways are encouraged.
- Thick support pillars, square posts or columns typical in the architectural theme design are encouraged in the design of commercial buildings.
- Pilasters accented with stone work at entries are encouraged.
- Long, large, solid walls unbroken in relief, form or design facing roadways are not permitted, and should be broken up through varied architectural design and landscaping.
- Landscape treatment should provide accent and separation of walkways from the parking lot.

b. Materials and Colors

- Again, the emphasis will be on cohesive colors, materials and textures.
- Colors and materials introduced into commercial architecture shall be the same as discussed in the Single-Family Residential section of this chapter. Deep red and maroon clay tones are discouraged.
- Pre-cast concrete, concrete block, brick, wood and stucco exterior materials shall be permitted.

- Metal exterior finishes are prohibited, except for roofing.
- Tilt-up building construction is permitted if clean entries and window detail is provided.

c. Roof Form, Materials and Colors

- A minimum roof slope of 1:12 is required, though steeper roof slopes are encouraged.
- Roof venting and other appurtenant equipment must blend with the building profile and otherwise be finished or screened from public and adjacent residential views.

d. Articulation and Fenestration

- Clear glass, dark toned glass and non-reflective glass is permitted.
- Reflective and mirrored glass is prohibited.
- Garage storage should be accessible from the rear of the commercial unit.
- Front office loading and unloading is discouraged.

e. Additional Building Components

- Those architectural design standards discussed in the Single-Family Residential Design Guidelines section, *Additional Building Components*, of this Chapter shall apply to the Retail/Flex site.
- Exposed machinery, essential towers, etc. not contained within a building shall be given special design consideration and shall be designed to enhance the total complex in terms of proportion, location and aesthetic impression.
- Air conditioning units shall not be visible from public or neighboring view unless it is enclosed and treated as an integral part of the total building design.
- All utilities within the retail flex site shall be placed underground.

5. Open Space

- Pedestrian paths should connect neighborhood level services with convenient collection points within the residential area.
- At points of intersection of pedestrian paths with vehicular/pedestrian paths, landscape and hardscape elements shall be provided that define the space, and alert the pedestrian to the intersection.
- Ample lighting shall be provided for security where open spaces provide for group gathering (e.g. bus stops, school entries, kiosks, etc.).
- No night lighting shall be permitted in association with the School Facility park and playfield, except that which is required for minimal public safety.
- Chain link fencing shall be permitted on the School site.
- Fencing along wetlands, where constructed, should be placed as far as possible from the wetland area itself, to afford the maximum amount of protection from human disturbance. Fencing should be no less than 2'-6' high, and be designed to permit views into the open space.
- Architectural improvements should be kept small and at a human scale.

6. Supplemental Architectural Guidelines

a. Mechanical Equipment

- All air conditioning/heating equipment, soft water tanks, gas and electric meters, pool, spa and fountain equipment associated with single-family residential units must be screened from public view with landscaping or fencing.
- Window or rooftop mounted air conditioning units visible from neighboring properties are prohibited on single-family residential units.
- Roof top mechanical equipment on commercial buildings penetrating or occupying the air space above roof lines must be painted or screened.
- All exterior mechanical equipment shall be screened from all sides.
- Where roof top mechanical equipment is visible from higher adjacent properties, it should be painted to match the roof color.
- Sound attenuation of motor operations is required.
- Rooftop solar water tanks are prohibited on single-family residential units.
- Antennas and satellite dishes are prohibited in the residential design of a single-family dwelling, except as noted in *Table 4, Development Standards*. Antennas and satellite dishes may be considered in the Retail/Residential Flex site through a Conditional Use Permit. If permitted, they shall appear as part of the architectural design of the structure, or shall be screened from the view of the public and any property within the New Pacific Properties Specific Plan area.
- Screening materials shall blend with the building materials and design, and any landscaping within the vicinity.
- Screening shall be in a manner consistent with the building facade.

b. Storage

- Exterior storage of motor vehicles, motor homes, RVs, trailers, boats, personal water craft or similar vehicles shall be prohibited unless screened appropriately from public view.
- Exterior storage of equipment, supplies, refuse, garbage, junk or their receptacles shall be prohibited.
- All exterior storage, trash receptacles and dumpsters shall be screened by landscaping, fencing or walls, in accordance with their standards described above.

c. Utilities

- All utilities within the New Pacific Properties Specific Plan area shall be placed underground, aside from street lights.
- The location of above ground appurtenant utility boxes and similar equipment, if required, shall be consolidated whenever possible.
- To the extent possible, utility meters shall be screened from view from public rights-of-way.
- Utility connections to a building must be located to be as unobtrusive as practicable, with the preference being at the side or rear.

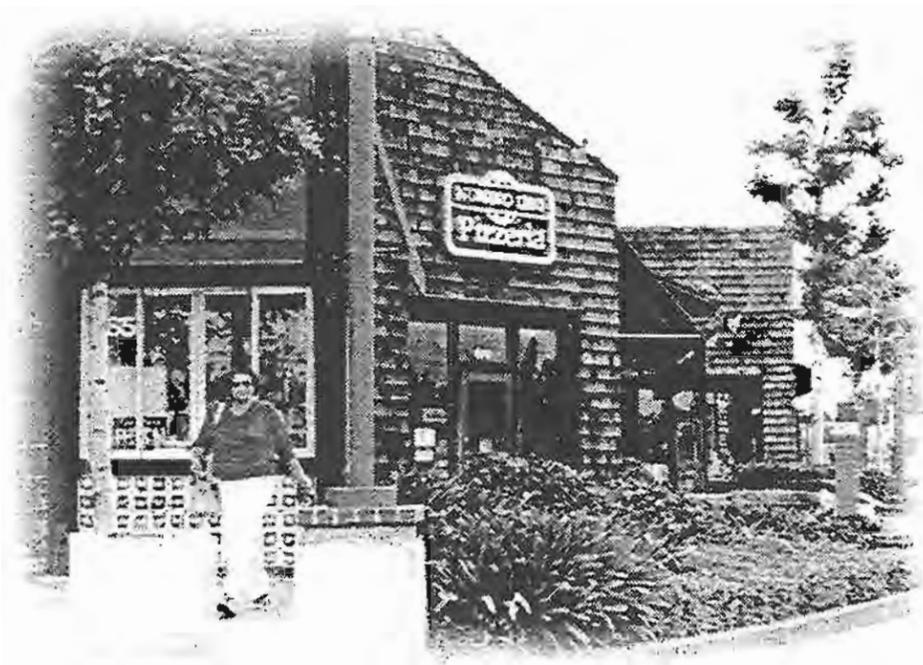
D. LANDSCAPE DESIGN GUIDELINES

1. Community Landscape Concept and Policies

The community landscape concept (see *Exhibit 25, Conceptual Landscape Plan*, and *Exhibit 26, Landscape Components Plan*) is intended to create a distinctive, diverse environment that will lend identity and character to the developed and natural areas of the Specific Plan. In this regard, the landscape concept enhances the natural features of the area through accentuation of distinct land uses, integration of architectural form with the landscape, and repetition of a common theme. Consistent treatment of all landscaped areas is critical to the creation of a high quality outdoor environment. Landscape features are included which are designed to enhance the project's interface with the surrounding environment and the adjacent community. The landscape concept suggests methods of treatments that might be considered; alternative landscape concepts and designs, which accomplish the stated objectives and intentions, are permitted. More detailed landscape design information, including a master list of tree and plant materials is provided in Section C.4. Recommended Plant Palette.

a. General Issues

- Landscape design should emphasize the planting of long-lived plant species that are native to the region or well adapted to the climatic and soils conditions of the site.
- The distinct character of various site areas should be reinforced through the deliberate use of limited plant species. Each area or neighborhood as described by this Specific Plan should have a distinct landscape character, different from other areas.
- Landscape treatments should utilize and adapt landscape elements from the urban patterns traditionally found in the Hercules area, including: hedgerows, regular planted shade trees, naturalized plantings, and "fields" of low growing plant material.
- Landscape treatment of all areas should emphasize the planting of tree-shaded corridors, contrasted with the community open spaces of the school, parks and natural open space areas.
- Landscape treatments should incorporate colorful plant material and evergreen trees in areas where such accents are appropriate.
- Landscape improvements should require minimal maintenance and irrigation. The use of native and/or drought tolerant plant material should be emphasized to the extent possible.
- All public areas, right-of-ways and commercial project landscaping should have an automatic irrigation system. Low volume spray heads and drip irrigation systems should be utilized and be compatible with any reclaimed water systems utilized.
- Landscape plans for any development should consider service lines, traffic safety sight line requirements, and structures on adjacent properties to avoid conflicts as the trees and shrubs mature. Street trees and trees planted in privately owned landscaped areas near public walkways or street curbs should be selected and installed to prevent unreasonable damage to sidewalks, curbs, gutters and other public improvements.
- Tree species with invasive root systems should be oriented away from water lines, sewer lines or public rights-of-way (See Recommended Plant Palette).

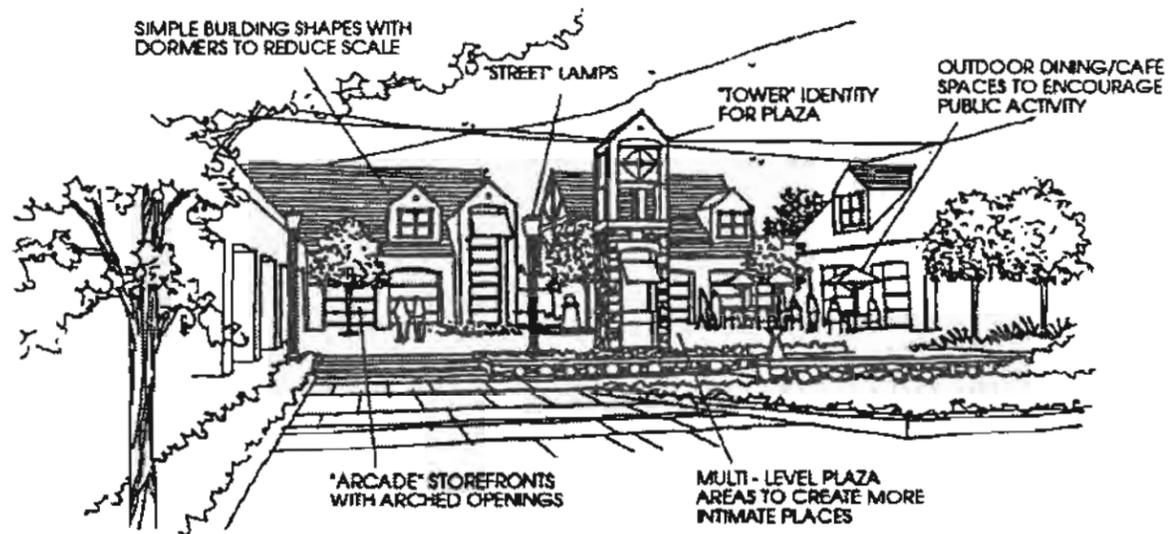


STREET LEVEL VIEW

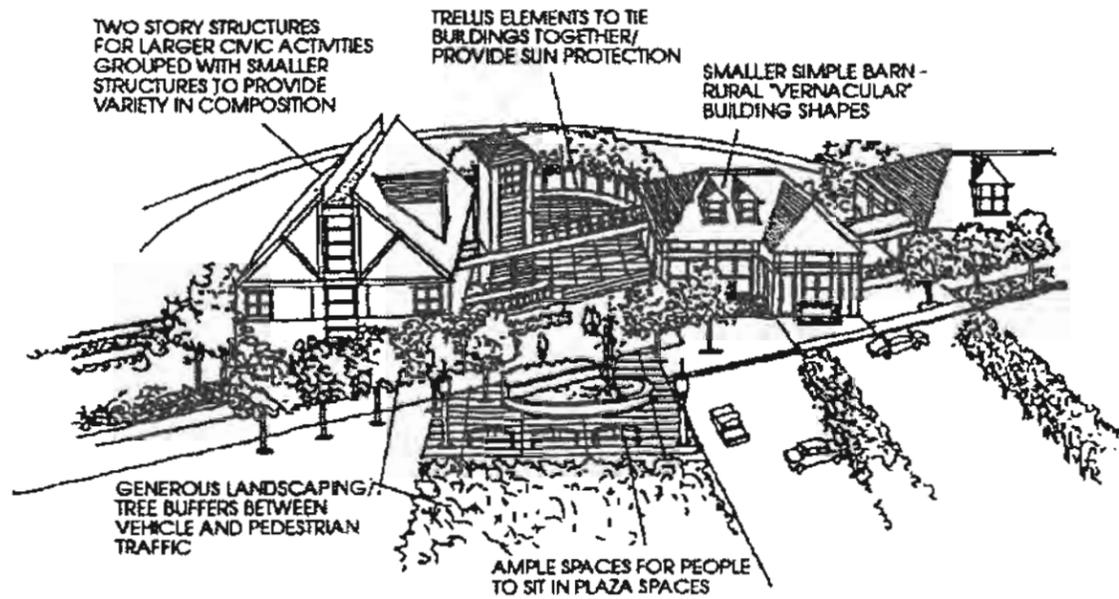


TYPICAL ELEVATION

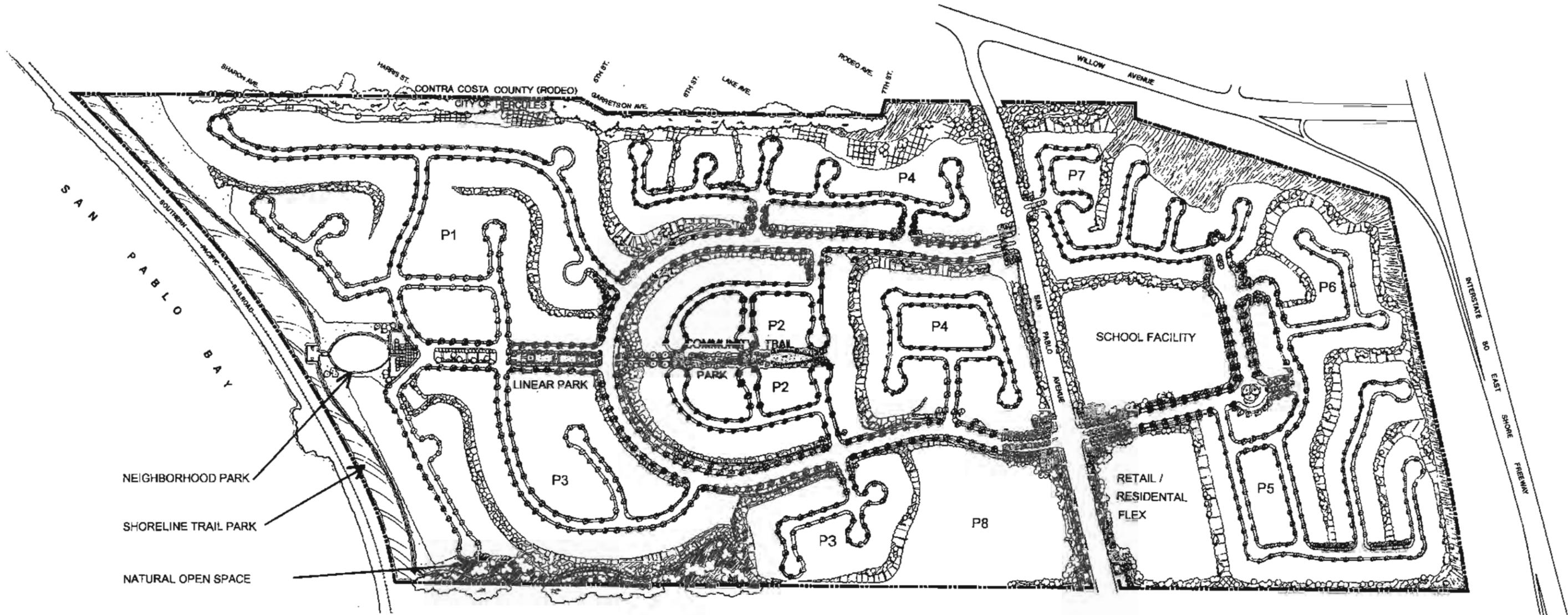
**ARCHITECTURAL DESIGN
CHARACTER (RETAIL)
NEW PACIFIC PROPERTIES SPECIFIC PLAN**



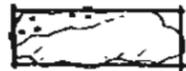
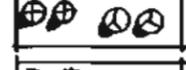
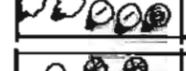
COURTYARD VIEW



BIRD'S EYE VIEW

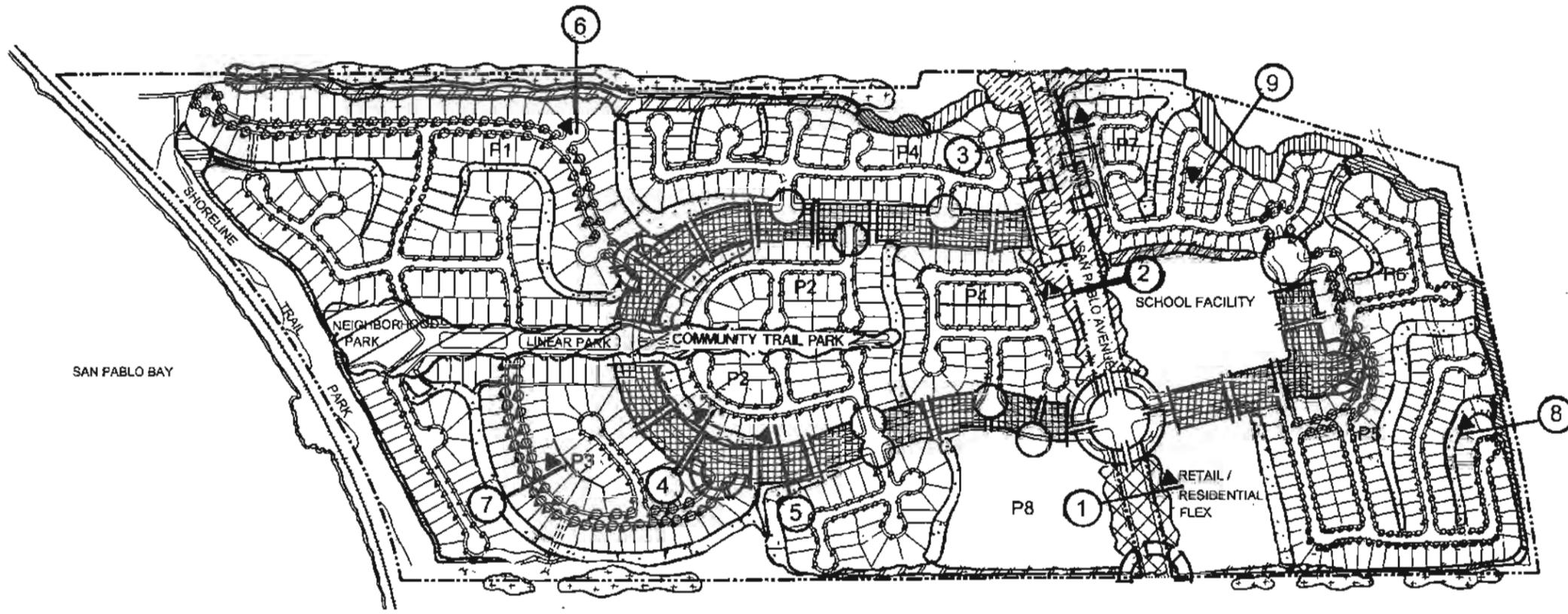


LEGEND

| | | | |
|---|------------------------------|---|---|
|  | SHORELINE TRAIL BUFFER SLOPE |  | COLLECTOR LOOP / ENHANCED PARKWAY STREETSCAPE |
|  | VIEW SLOPE |  | MINOR ROADS AND CUL-DE-SAC STREETSCAPE P1, P3, P5, P6 |
|  | PERIMETER BUFFER SLOPE |  | MINOR ROADS AND CUL-DE-SAC STREETSCAPE P4 & P7 |
|  | NEIGHBORHOOD SHOW SLOPE |  | MINOR ROADS AND CUL-DE-SAC STREETSCAPE P2 |
|  | COMMUNITY SHOW SLOPE |  | RETAIL / RESIDENTIAL FLEX |
|  | EXISTING PERIMETER SLOPE | | |
|  | NATURALIZED AREA | | |
|  | SAN PABLO AVENUE STREETSCAPE | | |

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

CONCEPTUAL LANDSCAPE PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN



SECTION REFERENCE

- ① SEE EXHIBIT 30, SECTION A
- ② SEE EXHIBIT 30, SECTION B
- ③ SEE EXHIBIT 30, SECTION C
- ④ SEE EXHIBIT 32, SECTION A
- ⑤ SEE EXHIBIT 32, SECTION B
- ⑥ SEE EXHIBIT 46
- ⑦ SEE EXHIBIT 47
- ⑧ SEE EXHIBIT 48
- ⑨ SEE EXHIBIT 49

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

LEGEND

| | | | | | |
|--|-------------------------------|--|--------------------------------------|--|--|
| | GATEWAY | | GATEWAY ARTERIAL LANDSCAPE | | VIEW SLOPE |
| | PRIMARY COMMUNITY ENTRY | | ARTERIAL LANDSCAPE | | PERIMETER BUFFER SLOPE |
| | SECONDARY COMMUNITY ENTRY | | LOOP COLLECTOR WITH ENHANCED PARKWAY | | SHORELINE TRAIL / BUFFER SLOPE |
| | NEIGHBORHOOD ENTRY - TYPE "A" | | NEIGHBORHOOD THEME STREET TREES | | HILLSIDE PLANTING (MANUFACTURED SLOPES ADJACENT TO OPEN SPACE) |
| | NEIGHBORHOOD ENTRY - TYPE "B" | | MINOR NEIGHBORHOOD STREET TREES | | HILLSIDE PLANTING NATIVE SLOPES |
| | | | LINEAR PARK CORRIDOR | | |
| | | | EXISTING TREES | | |

- Landscape maintenance practices for public areas are intended to be carried out at regular intervals sufficient to promote plant health and appearance. Maintenance may include discing, mowing, pruning, clearing of debris, weed removal, replacement of dead plant material, and repair and replacement of nonfunctioning irrigation equipment.
- Street trees should not be located so that they prevent effective solar access, unless they are deciduous.
- Planting material selection should be sensitive so as not to interfere with bay and other views upon maturity. Permanent view protection from maturing plants cannot be guaranteed.

2. Streetscapes

Streetscapes are intended to promote a distinctive community character and complement the form and function of the various roadways (i.e., residential and local collector roads). Landscape treatment in medians, parkway strips and adjacent buffer landscaping are major components of the New Pacific Properties' open space system. The primary function of these areas is to convey the immediate impression of the overall community image, provide primary pedestrian and circulation routes and provide a landscape interface between traffic corridors and adjacent land uses. Streetscape treatment will be established along the arterial highways as well as major arterial collector and minor, one-way minor, and cul-de-sac streets within the community. The streets should be treated as linear spaces, with tree placement and massing providing the primary spatial definition of the corridor.

The master streetscape program will consist of trees, shrubs and groundcover selected to establish a network of linear landscape street corridors that physically link all areas of the community. They will also create a distinct identity for each corridor and establish a specific character or theme for each neighborhood. Consistency will be achieved through the use of plant material selected from the Recommended Plan Palette established for each streetscape or neighborhood.

- Landscape treatments of each street within neighborhoods are intended to be consistent throughout the length of the street, as described by *Exhibit 26, Landscape Components Plan*.
- Landscape treatments should generally utilize regularly planted street trees planted 50 feet on center or less, unless otherwise specified.
- Street tree planting, as described below, may generally utilize one or two primary species for each street, with a limited number of additional species used as accents.
- In order to conserve water, landscaping of the ground plane may emphasize the use of shrubs, groundcovers, native grasses and mulches as well as turf.
- Landscaping in public areas should minimize the use of turf except in specially designed corridors or entries.
- Masses of groundcovers and shrubs should be limited to a few, extensively used species for each street. Variations to the basic species should selectively be used for accent and contrast.

a. San Pablo Avenue

The landscape treatment of San Pablo Avenue is intended to create an informal image with a mix of evergreen and deciduous trees lining the roadway. Treatment of San Pablo Avenue may consist of the following (*Exhibits 27-28, San Pablo Avenue Streetscape*):

- Theme trees and/or deciduous accent trees should be planted on both sides of the roadway. Only theme trees are intended for the median.
- Evergreen trees should be planted informally as background planting.
- Low, spreading, drought tolerant shrubs should be planted in the median.
- Grasses or groundcovers should be planted between the roadway edge and right of way line or sidewalk.
- Screening shrubs should be planted against the community wall on both sides of the roadway.

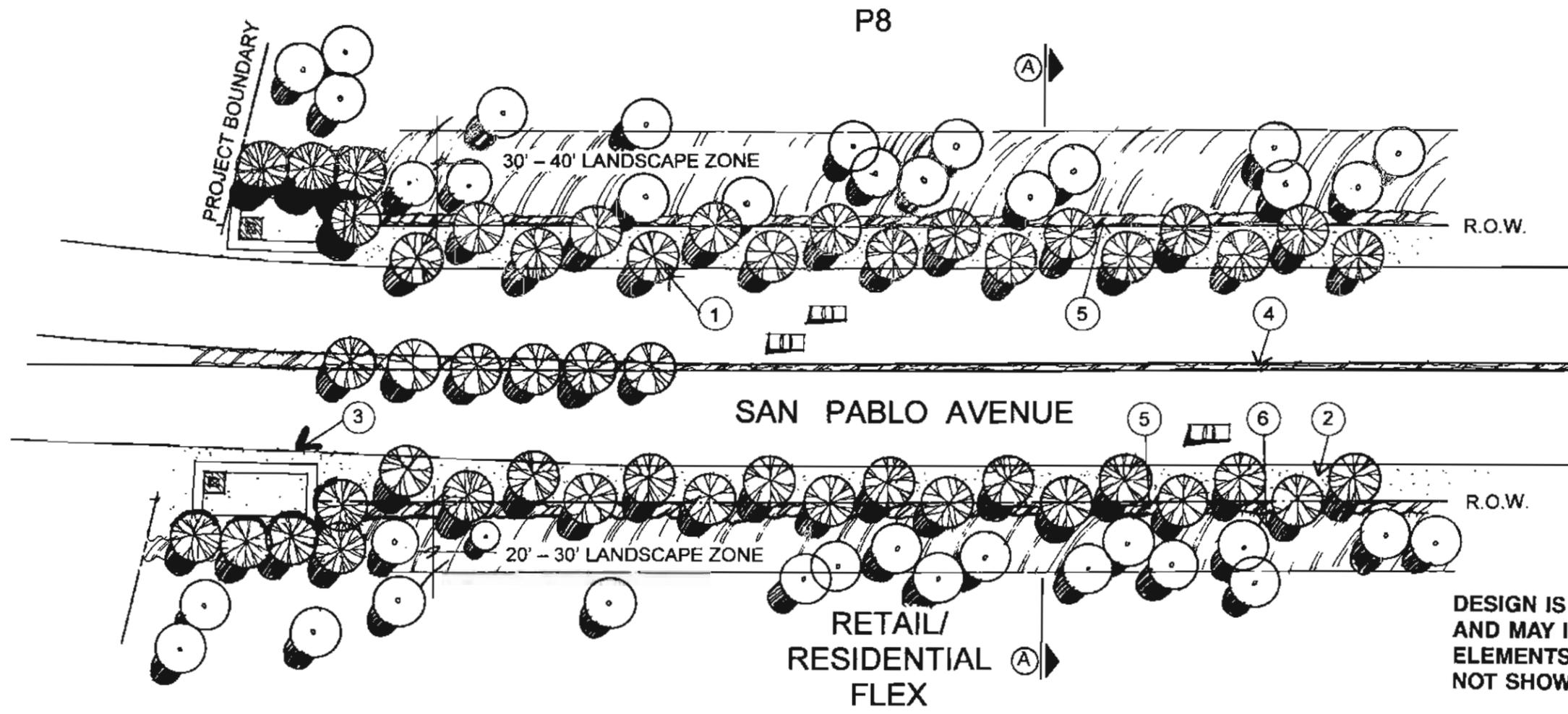
b. Loop Roadway / Enhanced Parkway

- Theme street tree species should be consistent along entire length of Loop Road. These are intended to be trimmed and thinned as necessary so as not to interfere with scenic views.
- Landscaping of the Loop Roadway should create a heavily shaded street corridor, yet not interfere with views of San Pablo Bay.
- Landscaping of the Loop Roadway may consist of the following (*see Exhibits 29 and 30, Loop Roadway/Enhanced Parkway Streetscape*):
 - A specific species of street tree for the Loop Roadway as defined herein.
 - Large, canopy street trees: planted on each side of the roadway near community entries. Elsewhere, flanking the sidewalks, one row planted an average of 50 feet or less on center. Low, spreading groundcover or turf planted in the parkway adjacent to the walks.
 - Sidewalks on both sides of the roadway separated from the roadway by a planted parkway strip.

c. Minor, One-Way Minor, and Cul-de-Sac Streets

The objective for local streets is to create a network of tree-lined, pedestrian scale streets serving residential areas within each neighborhood while utilizing a distinct tree species and character.

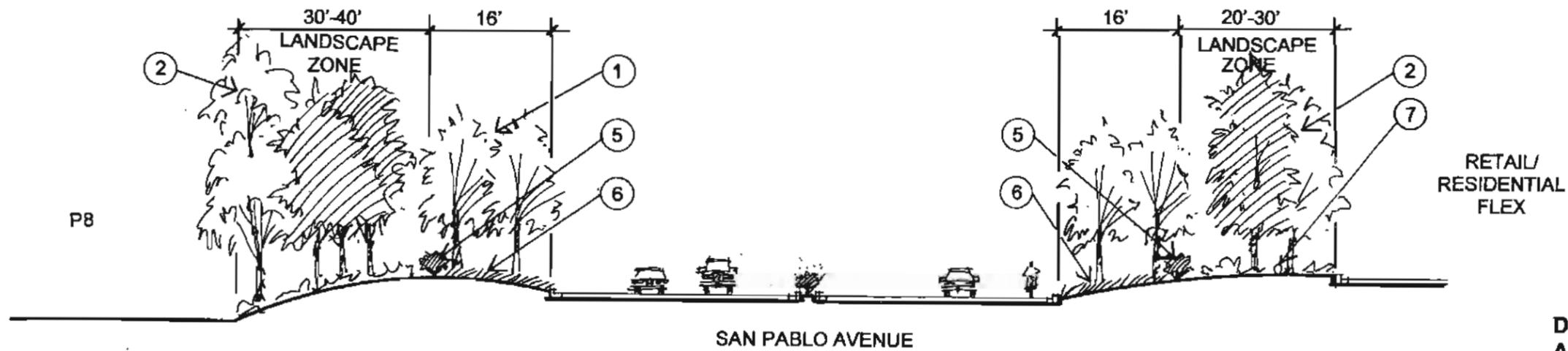
- Landscaping of local streets may consist of the following:
 - A single, consistent species of street tree for each street or sub-area within the neighborhood.
 - A single row of street trees planted on each side of the roadway, centered between the curb and sidewalk. In general, trees may be regularly spaced, no greater than 50 feet apart. Spacing may vary slightly based upon lot width and driveway spacing.
 - Low, spreading groundcover or turf in the parkway adjacent to the walks.
 - Sidewalks separated from the curb edge by a planted parkway.



PLAN VIEW

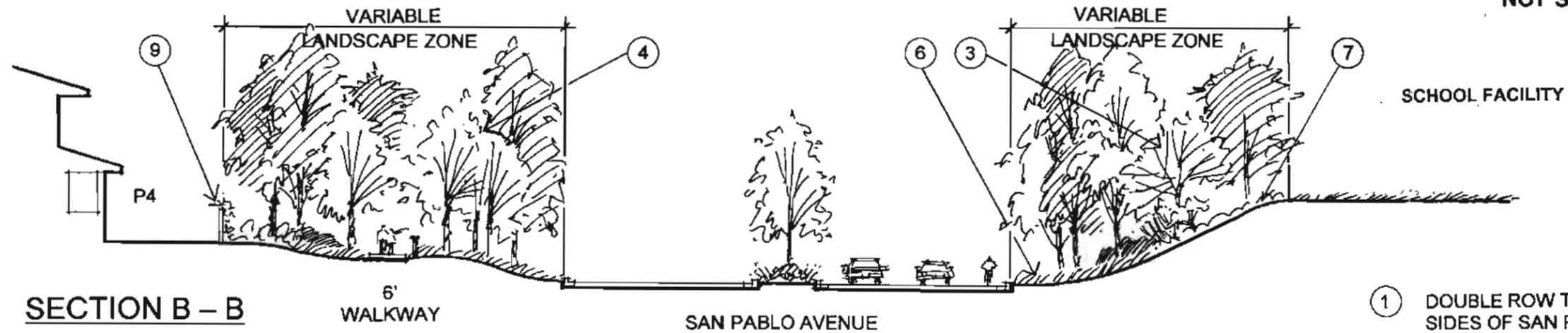
DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

- ① THEME TREE
- ② TURF OR MEADOW GRASS
- ③ GATEWAY LANDSCAPE
- ④ FLOWERING SHRUBS IN MEDIAN
- ⑤ HEDGE
- ⑥ SHRUBS / GROUND COVER / TURF

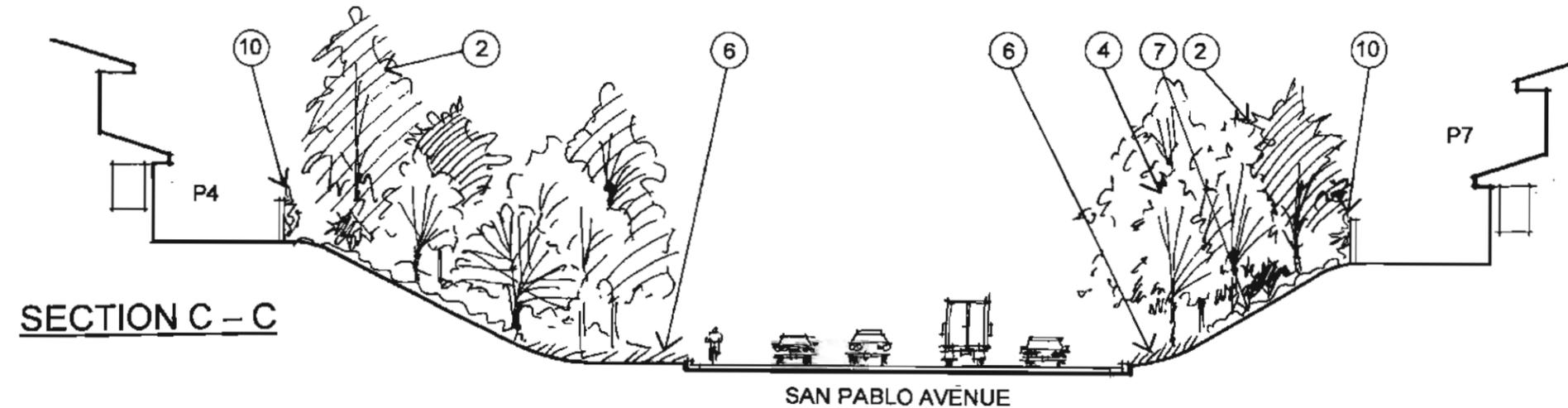


SECTION A - A

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.



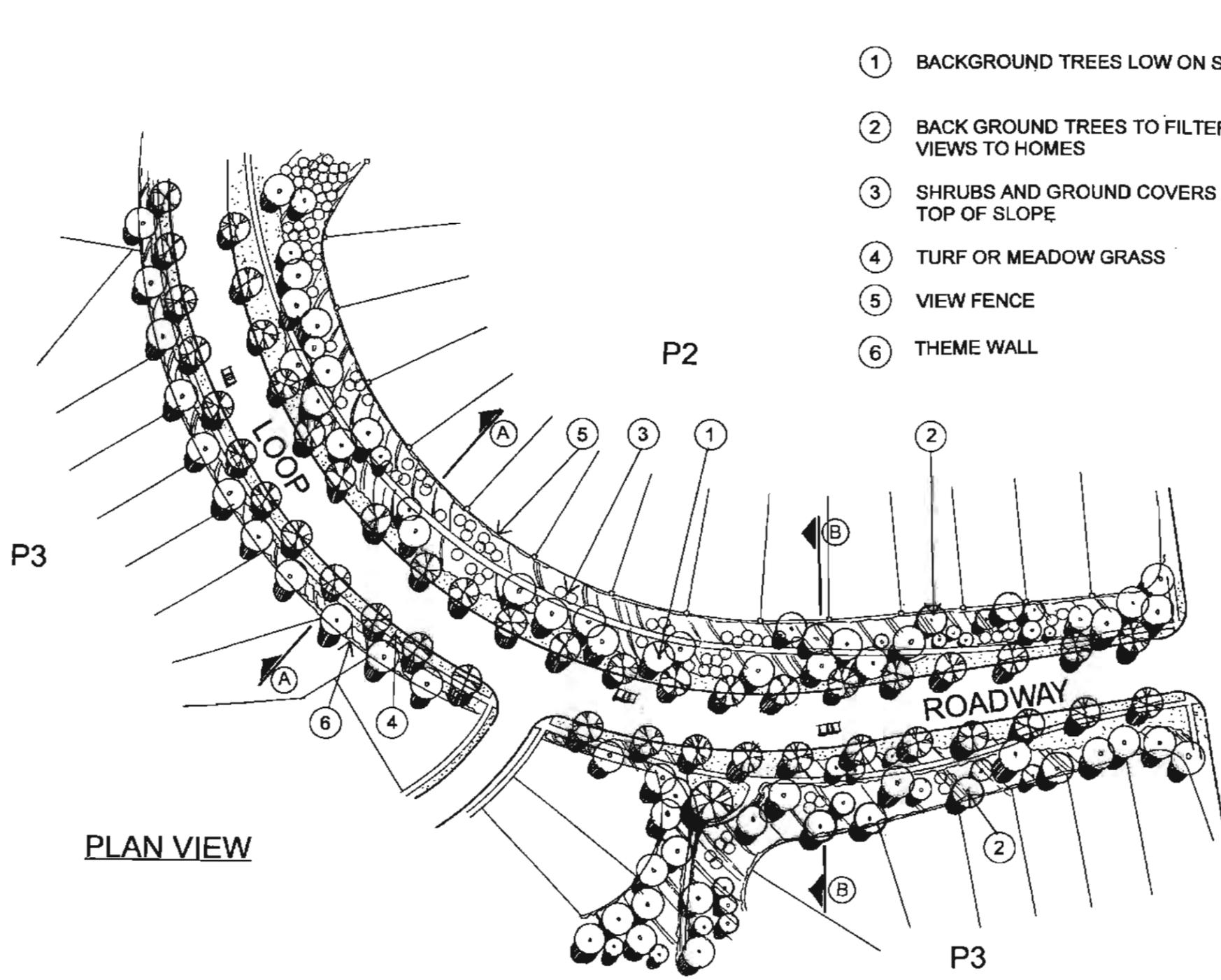
SECTION B - B



SECTION C - C

- ① DOUBLE ROW THEME TREE, BOTH SIDES OF SAN PABLO AVENUE
- ② EVERGREEN BACKGROUND TREES
- ③ LOWER STORY ACCENT TREE
- ④ INFORMAL GROVES OF THEME TREE
- ⑤ HEDGE
- ⑥ TURF OR MEADOW GRASS
- ⑦ SHRUBS AND / OR GROUND COVERS
- ⑧ VIEW FENCE
- ⑨ THEME WALL
- ⑩ VINES

SAN PABLO AVENUE STREETSCAPE SECTIONS
 NEW PACIFIC PROPERTIES SPECIFIC PLAN



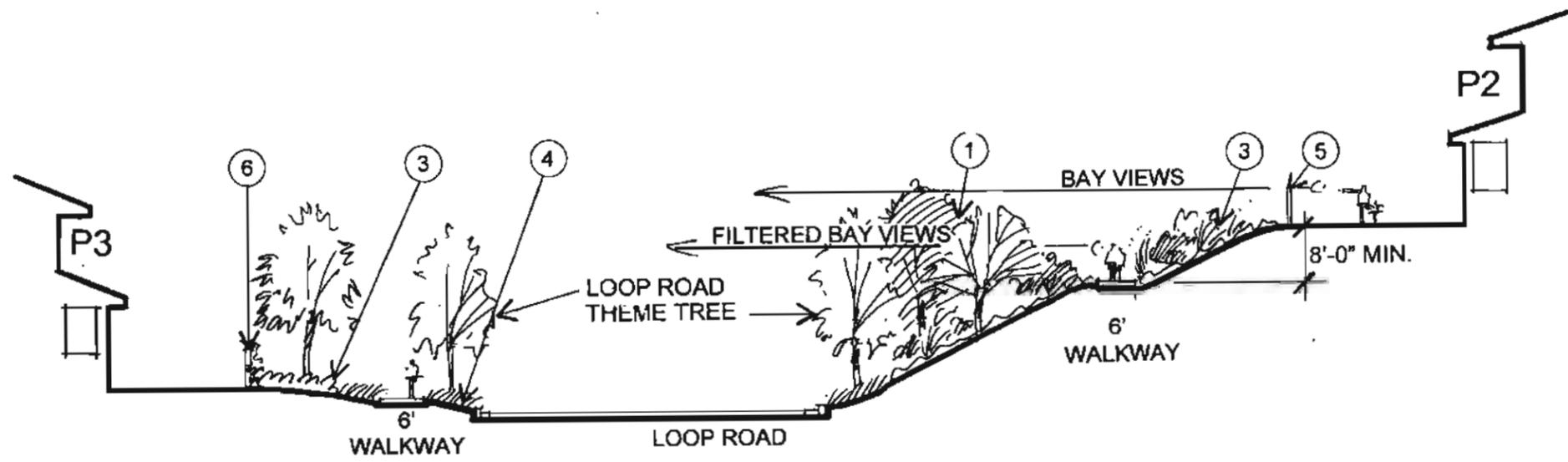
- ① BACKGROUND TREES LOW ON SLOPE
- ② BACK GROUND TREES TO FILTER VIEWS TO HOMES
- ③ SHRUBS AND GROUND COVERS AT TOP OF SLOPE
- ④ TURF OR MEADOW GRASS
- ⑤ VIEW FENCE
- ⑥ THEME WALL

- SECTION REFERENCE
- Ⓐ SEE EXHIBIT 29, SECTION A
 - Ⓑ SEE EXHIBIT 29, SECTION B

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

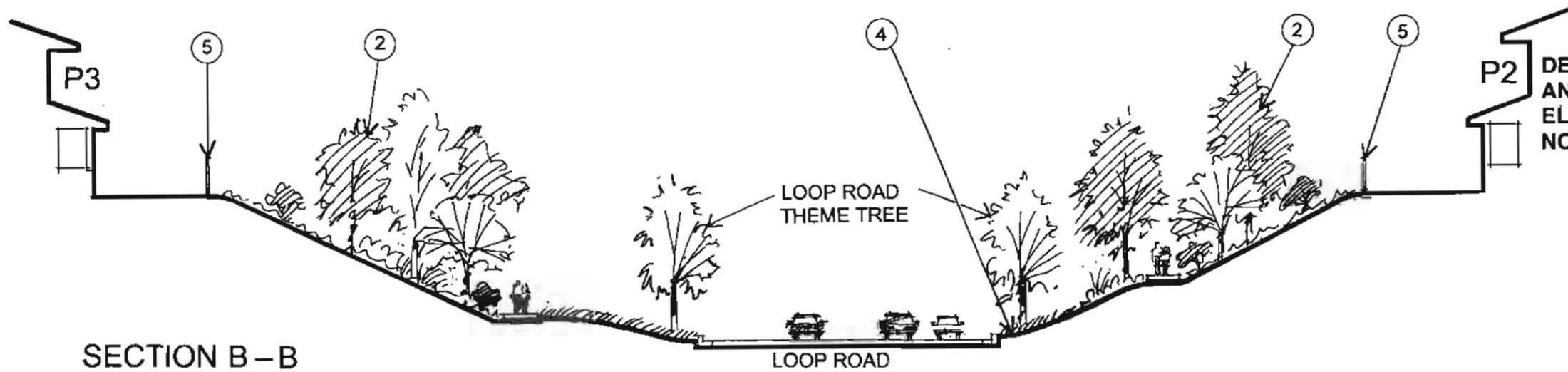
PLAN VIEW

LOOP ROADWAY / ENHANCED PARKWAY STREETSCAPE PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN



SECTION A-A
(VIEW CONDITION)

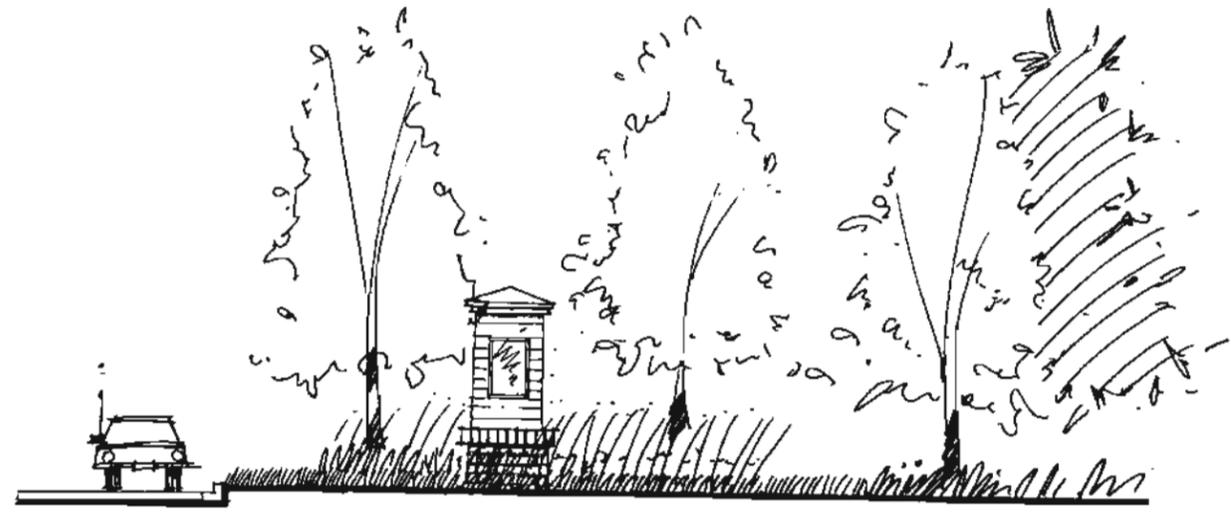
- ① BACKGROUND TREES LOW ON SLOPE
- ② BACK GROUND TREES TO FILTER VIEWS TO HOMES
- ③ SHRUBS AND GROUND COVERS AT TOP OF SLOPE
- ④ TURF OR MEADOW GRASS
- ⑤ VIEW FENCE OR THEME WALL
- ⑥ THEME WALL



SECTION B-B
(LIMITED VIEW CONDITION)

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

LOOP ROADWAY / ENHANCED PARKWAY STREETSCAPE SECTIONS
NEW PACIFIC PROPERTIES SPECIFIC PLAN

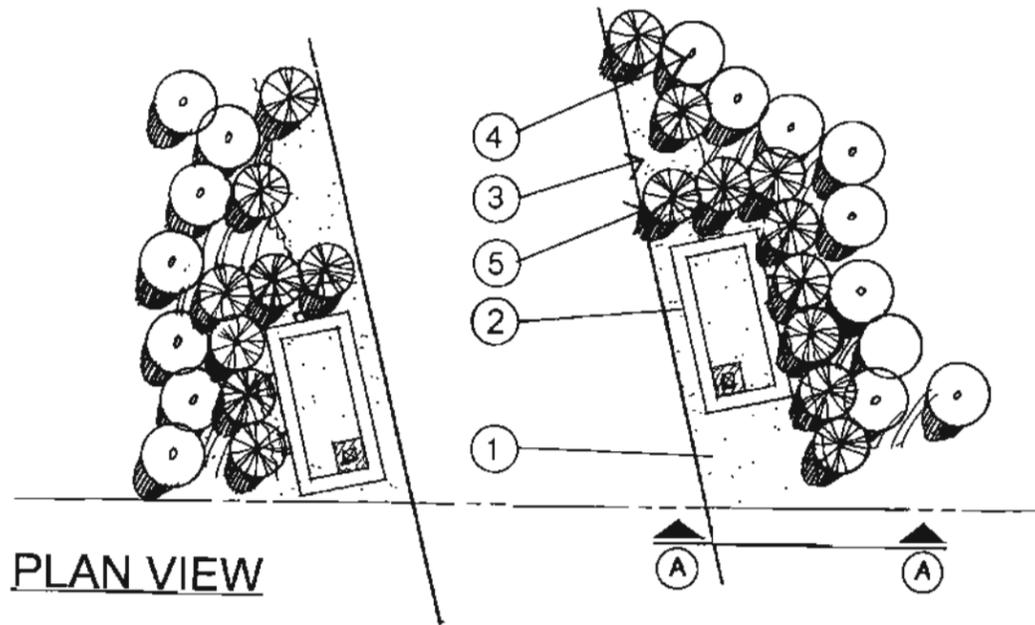


SECTION A - A

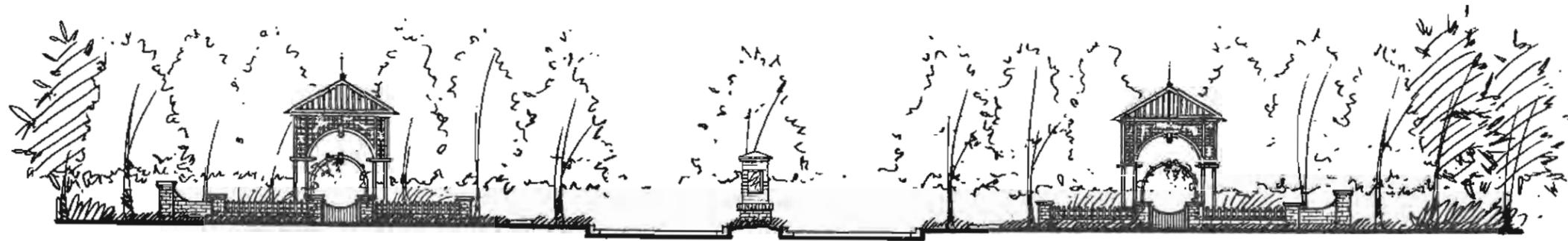


- ① GATEWAY MONUMENT / SIGNAGE
- ② THEME GARDEN
- ③ TURF PARKWAY
- ④ EVERGREEN BACKDROP TREE
- ⑤ THEME TREE

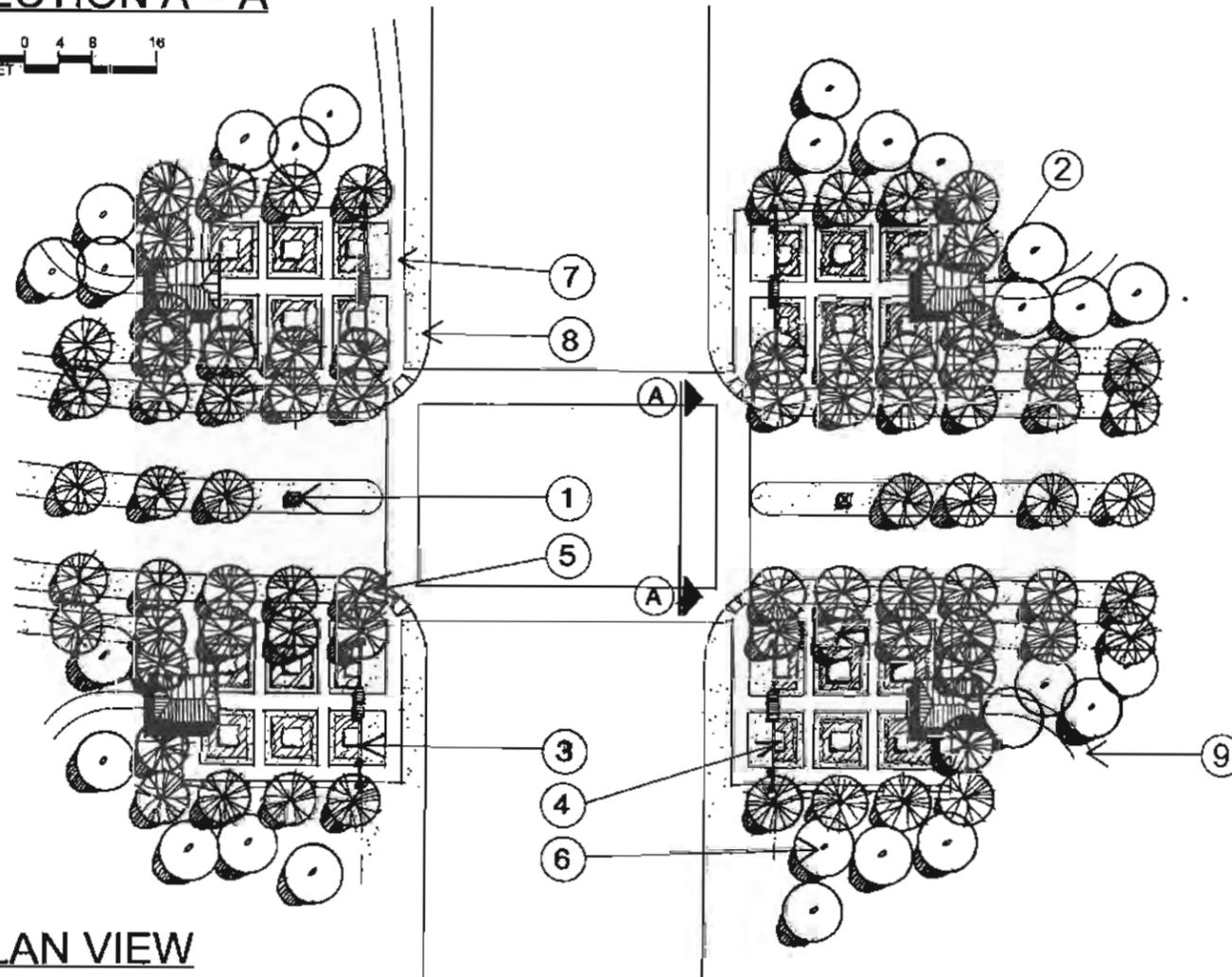
DESIGN IS CONCEPTUAL IN NATURE
AND MAY INCLUDE SOME OF THE
ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.



PLAN VIEW



SECTION A - A



PLAN VIEW

- ① ENTRY MONUMENT / SIGNAGE
- ② GATEWAY STRUCTURE
- ③ ENTRY WALL WITH PICKET FENCE
- ④ THEMATIC GARDEN
- ⑤ THEME TREE
- ⑥ EVERGREEN BACKDROP TREE
- ⑦ WALK
- ⑧ TURF PARKWAY
- ⑨ NEIGHBORHOOD ACCESS

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

- Landscape treatments may utilize the plant materials from the primary entry.
- Landscape treatments are intended to be consistent with *Exhibit 33, Secondary Community Entry*.

d. Neighborhood Entries

- Neighborhood entries occur at the primary entries to each neighborhood at intersections with the Loop Roadway . The landscape treatment should be similar to primary entries, with a less extensive treatment of the ground plane and entry pilasters, but should be consistent within a neighborhood. Treatments may include the following (see *Exhibit 34, Neighborhood Entry "A"*, *Exhibit 35, Neighborhood Entry "B"* and *Exhibit 36, Round-about Neighborhood Entry*):
 - Rows of ornamental trees.
 - Groundcover, turf or gravel.
 - Vertical entry pilasters smaller than primary entries (Graphics may occur).
 - Trellis structures or other distinctive architectural elements may occur.
 - Pedestrian walks.
 - Entry landscape treatments may vary between neighborhoods, thereby creating a distinct image for each area.
 - Each neighborhood may use an ornamental/flowering tree species unique to that neighborhood. The neighborhood accent tree should be used throughout the neighborhood where appropriate.

4. Residential Neighborhoods

In addition to linear landscape corridors that provide continuity throughout the community and entry areas that establish points of arrival, the landscape treatment of neighborhoods and other special areas will establish a unique sense of identity for each area.

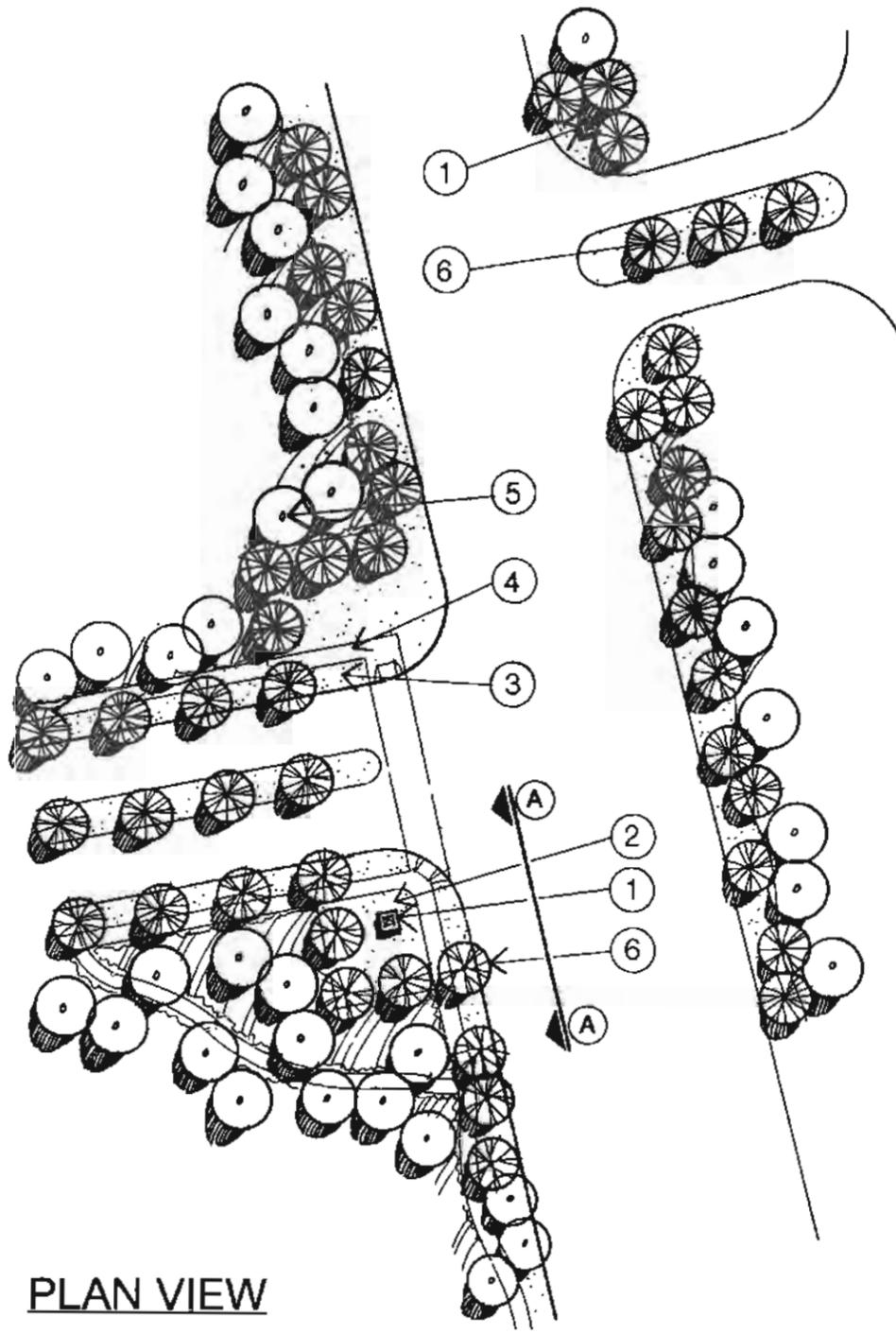
The objective for residential neighborhoods is to establish a unique landscape character for each neighborhood as described below, and to consistently treat all areas within the project.

- The landscape character of each neighborhood should be different from adjoining neighborhoods.
- Each neighborhood may use a single species of large canopy shade trees for all primary circulation routes within the neighborhood (see Plant Recommended Plant Palette).
- Each neighborhood may use a single species of ornamental/flowering tree to be used at all neighborhood entries, the retail/residential flex entry and at all points of pedestrian access to the collector street network.

5. School Facility

The surrounding residents can utilize the active recreational facilities proposed for the school facility site. Refer to *Exhibit 37, Conceptual School Facility Landscape Plan*. Proposed facilities may include two ball fields and a soccer field.

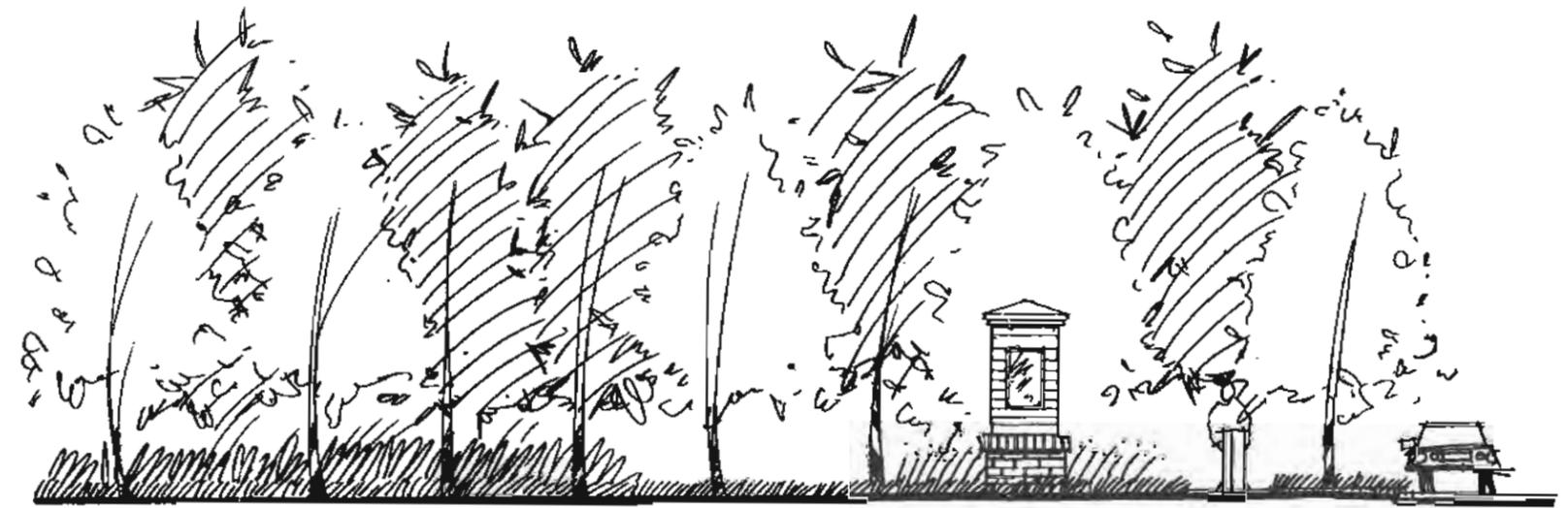
Perimeter planting should be responsive to the surrounding uses. For example:



PLAN VIEW

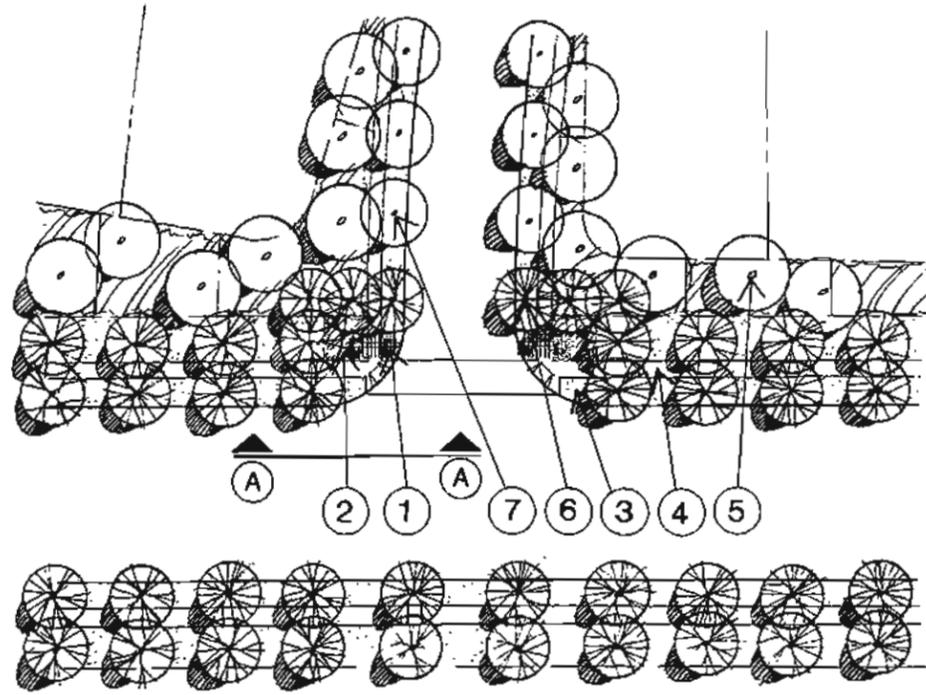
DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

- ① NEIGHBORHOOD GATEWAY / SIGNAGE
- ② ENTRY GARDEN
- ③ TURF PARKWAY
- ④ WALK
- ⑤ EVERGREEN BACKDROP TREE
- ⑥ ENTRY THEME TREE



SECTION A - A

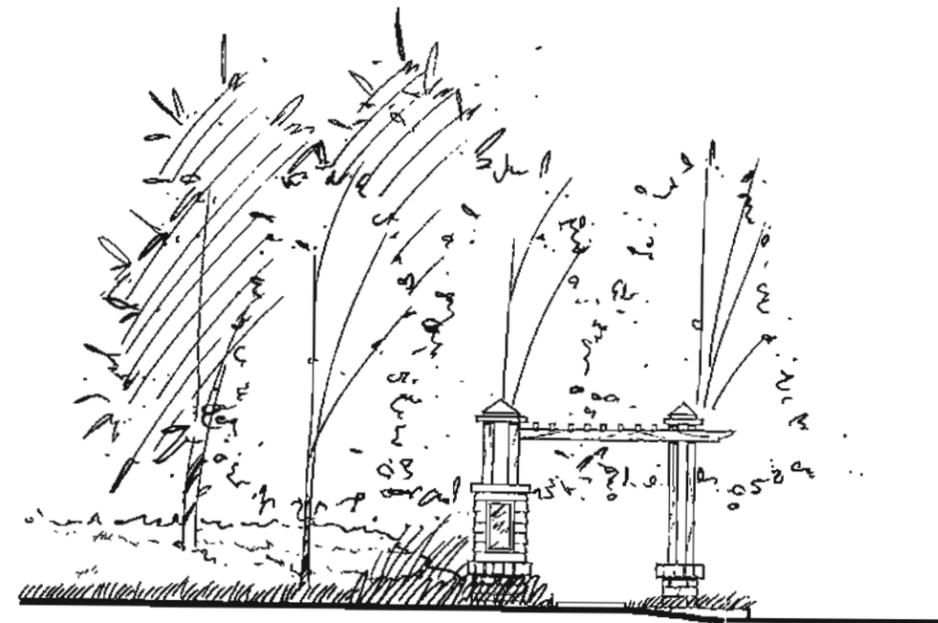




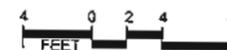
PLAN VIEW

- ① NEIGHBORHOOD GATEWAY / SIGNAGE
- ② ENTRY GARDEN
- ③ TURF PARKWAY
- ④ WALK
- ⑤ EVERGREEN BACKDROP TREE
- ⑥ ENTRY THEME TREE
- ⑦ STREET TREE

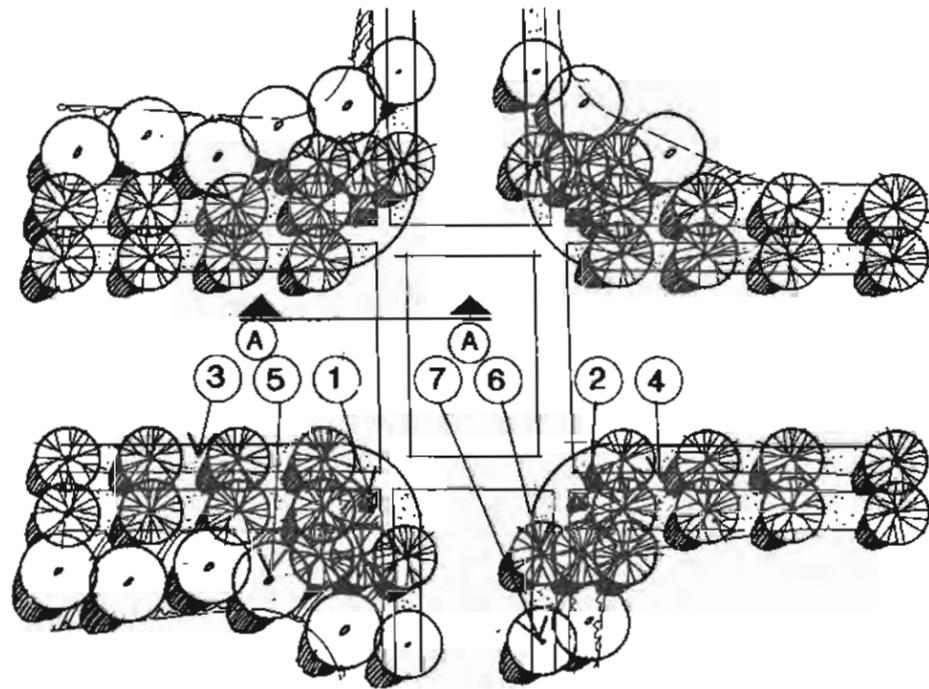
DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.



SECTION A - A

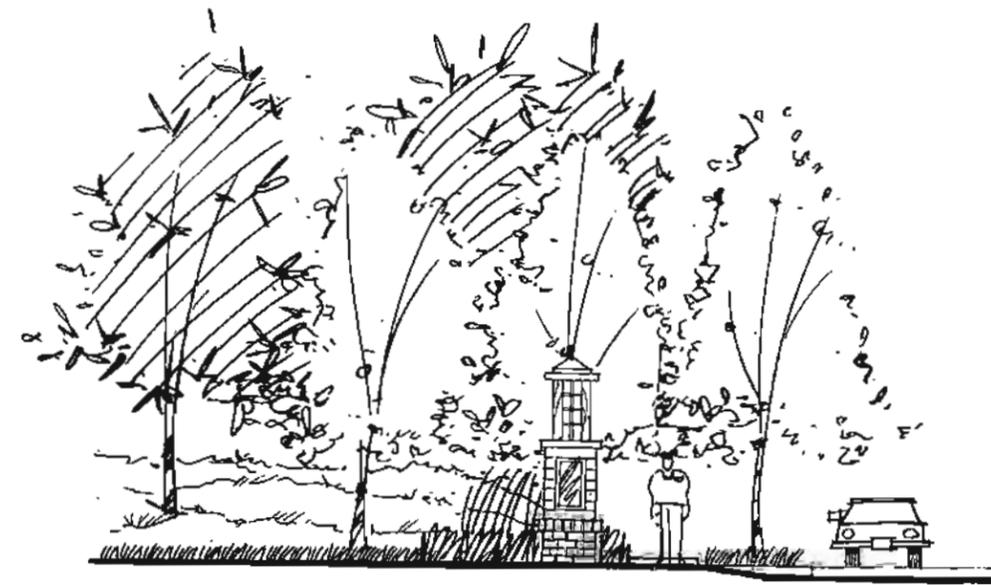


DESIGN IS CONCEPTUAL IN NATURE
AND MAY INCLUDE SOME OF THE
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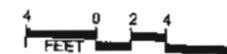


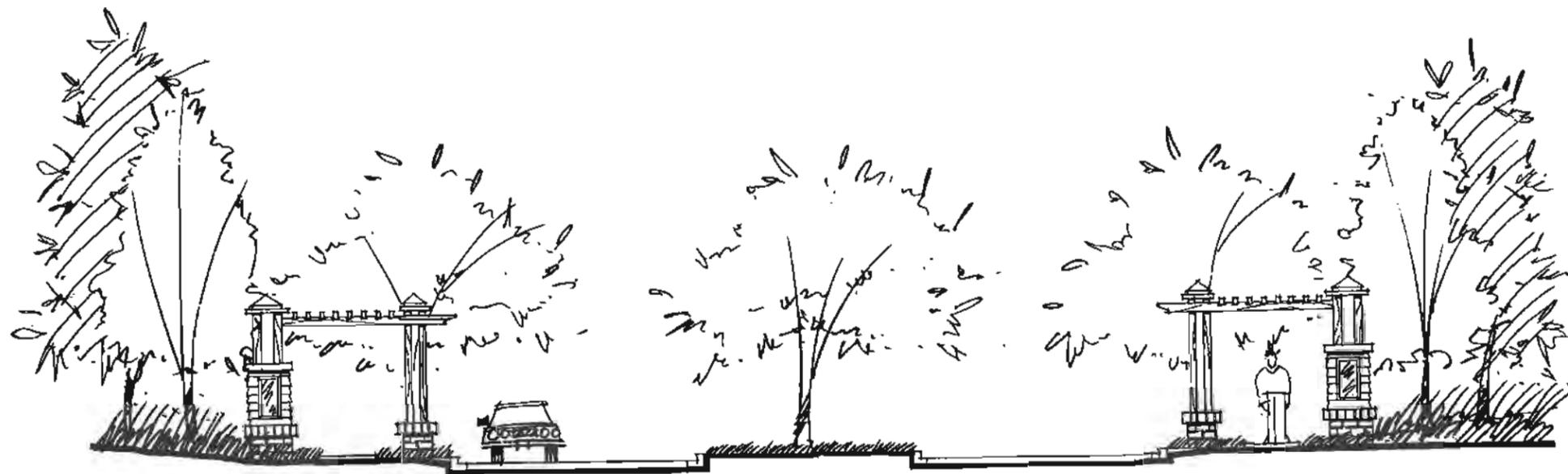
PLAN VIEW

- ① NEIGHBORHOOD GATEWAY / SIGNAGE
- ② ENTRY GARDEN
- ③ TURF PARKWAY
- ④ WALK
- ⑤ EVERGREEN BACKDROP TREE
- ⑥ ENTRY THEME TREE
- ⑦ STREET TREE



SECTION A - A



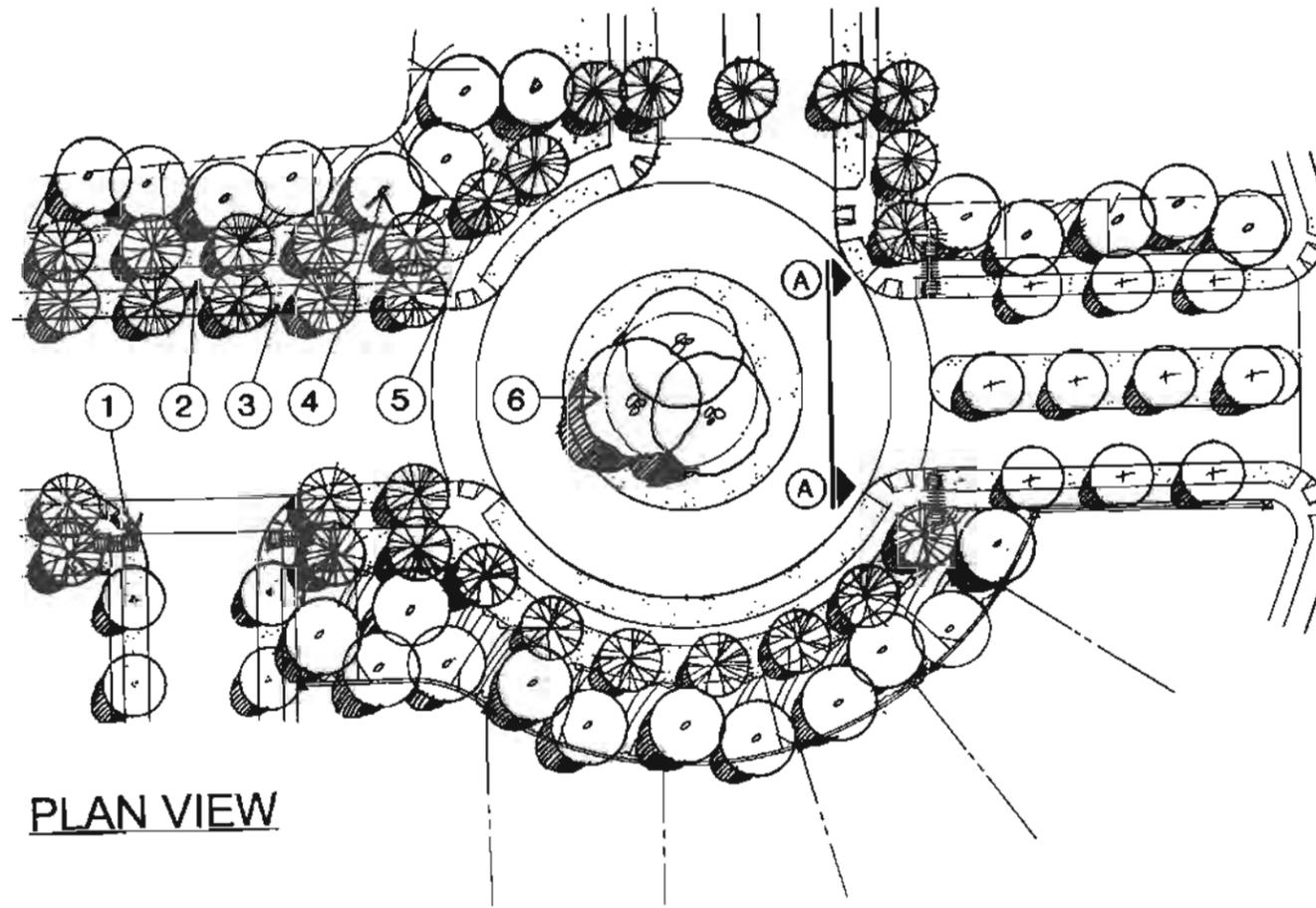


SECTION A-A



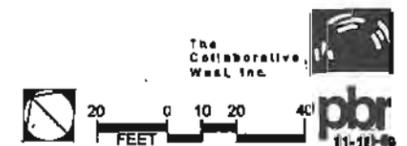
- ① NEIGHBORHOOD GATEWAY / SIGNAGE
- ② WALK
- ③ TURF PARKWAY
- ④ EVERGREEN BACKDROP TREE
- ⑤ LOOP ROAD THEME TREE
- ⑥ SPECIMEN / ACCENT TREE

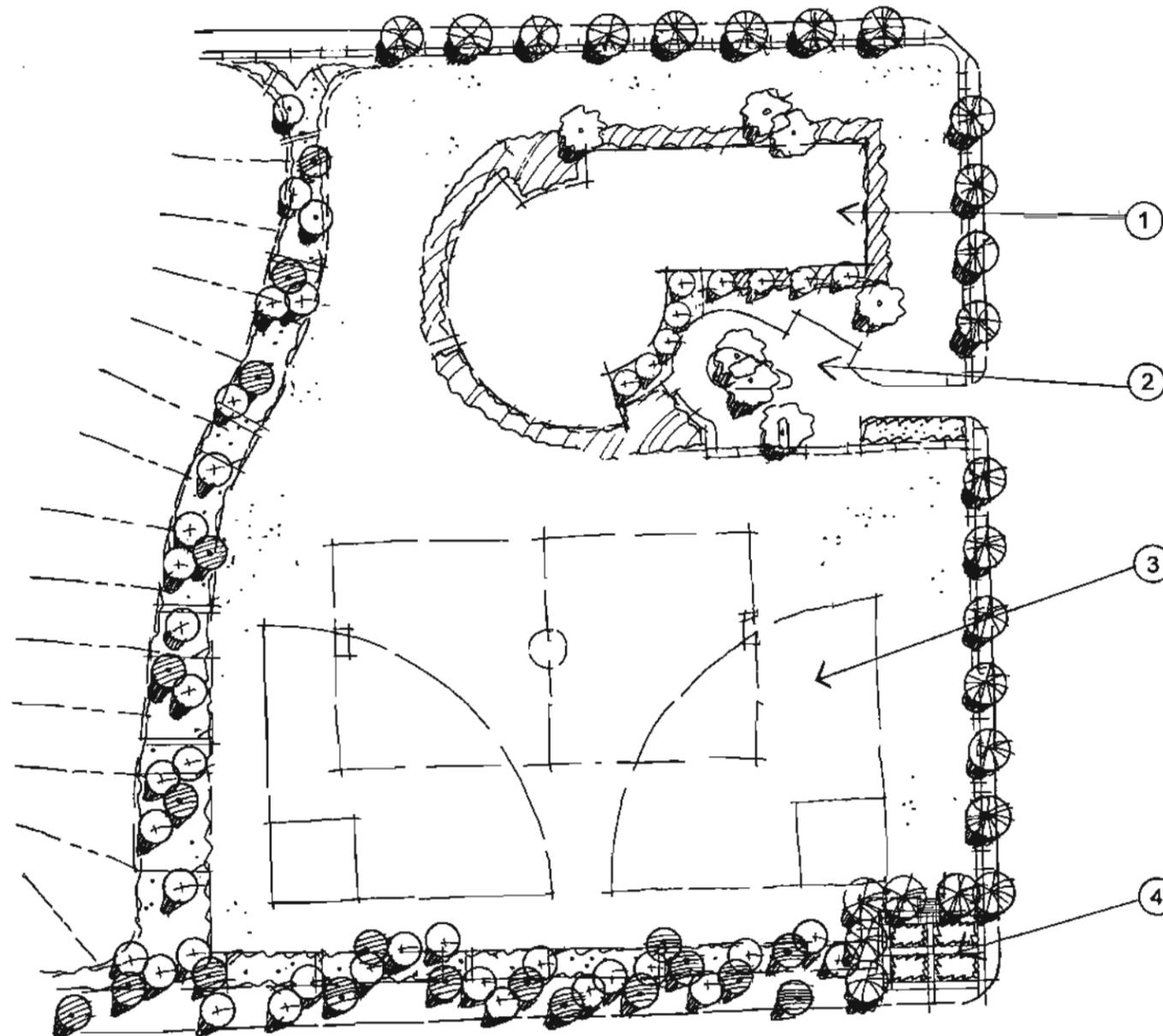
DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.



PLAN VIEW

ROUND-ABOUT NEIGHBORHOOD ENTRY
NEW PACIFIC PROPERTIES SPECIFIC PLAN





SAN PABLO AVENUE

**SCHOOL DESIGN IS CONCEPTUAL ONLY-
AND SUBJECT TO DISTRICT AND CITY REVIEW.**

- ① SCHOOL FACILITY
- ② PARKING
- ③ BALL FIELD
- ④ ENTRY FEATURE

**DESIGN IS CONCEPTUAL IN NATURE
AND MAY INCLUDE SOME OF THE
ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.**

CONCEPTUAL SCHOOL FACILITY LANDSCAPE PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN

- The down slope adjacent to development area P7 should be evergreen buffer planting.
- The slope adjacent San Pablo Avenue should consist of evergreen buffer planting.
- The Loop Roadway/Enhanced Parkway should consist of theme tree planting that will allow views into the park.
- Landscape treatments should emphasize the use of regular tree planting, and trees planted in walk areas.
- Loop Roadway and major parking areas may be planted with the designated collector street tree species.
- The designated accent tree species may be used as the predominant accent tree species for pedestrian gathering areas, walkways, courtyard entries or other areas requiring special emphasis.
- Where applicable, a variable setback within the elementary school site along the collector should be landscaped as an extension of the Loop Roadway landscape.

All planted areas will be irrigated for full coverage.

6. Retail/Residential Flex

The landscape concept for this area emphasizes the consistent treatment of street corridors to demarcate large parcels and major use areas, and the planting of groves of accent trees at entries and gathering areas.

- Final landscape plans will designate a tree species to be used for local roads, parking lots and accent areas.
- Trees should be used to define major use areas, provide shelter from prevailing winds, and to delineate boundaries.
- Accent trees may be used in groups or in regular groves at building entries, patios, courtyards, and employee gathering areas or other special areas.
- All entries into the retail/residential flex site should establish a setback consistent with the secondary community or neighborhood entries, and should use consistent treatment of landscaping, walls, signage, lighting or other elements.
- All service, loading, mechanical or storage areas should be screened from view from public streets or adjacent parcels with walls and/or landscaping to the extent feasible.
- Trash enclosure areas should be screened and softened with landscaping whenever possible.

a. Parking Lots

- Parking lots may be planted with trees to visually mitigate their impact, and to create a shaded, cooler environment. These trees are in addition to perimeter area plantings.
- Hedges, shrubs or berms surrounding parking lots may be used to mitigate visual impacts from adjacent streets or uses. Hedges and shrubs should mature or be maintained no higher than 36 inches in height, measured from the parking lot edge.
- Planting islands within parking areas should be adequately sized to allow parking lot trees and shrubs to thrive, and to allow adequate clearance for car doors, bumper overhangs and visibility.
- All trees planted within parking areas should be a minimum 15-gallon in size.

7. Parks and Open Space

The park design concepts for New Pacific properties are intended to create neighborhood connectivity and interest. As concepts, the program ideas for each park will form the framework for further development and refinement of the design themes. The following descriptions and referenced exhibits are general in nature, and are presented as possible development alternatives. It is the intent of this Specific Plan to provide for parks and open space areas that may accommodate such elements or design concepts as described herein.

a. Shoreline Trail/Park

The Shoreline Trail/Park is envisioned as a naturalized setting for a trail connecting to existing and future pedestrian/bicycle trails to the north and south as proposed by the East Bay Regional Park District. This area has been altered in grade as a result of previous development. To the extent possible, this area will be sensitively graded to maximize the relationship to the shoreline edge and to enhance existing conditions. A pedestrian barrier fence will be installed along the westerly park edge. Fence design may include a wood and wire mesh fence or a 42" high chain link fence on the San Pablo Bay side to serve as a barrier between users of the park and the railway that runs alongside of it. The Wire Mesh and Chain Link fences are to be treated with vinyl coating or a similar measure to prevent rusting. Refer to *Exhibit 50, Typical Wall and Fence Designs*, found later in this Chapter. Stormwater retention may also occur in this area. Refer to *Exhibits 38 and 39, Conceptual Shoreline Trail/Park Landscape Plans (North and South)*.

At the housing edge, buffer planting will be provided to separate the trail from backyard areas. Vegetation will consist of irrigated plantings of low, drought tolerant vegetation, located so as not to hinder views to the bay. These plantings should be compatible with seaside plantings elsewhere seen along San Pablo Bay and are intended to harmonize with natural settings along the coast.

b. Neighborhood Park

The Neighborhood Park is characterized by a large oval shaped "Green." Refer to *Exhibit 40, Conceptual Neighborhood Park Landscape Plan*.

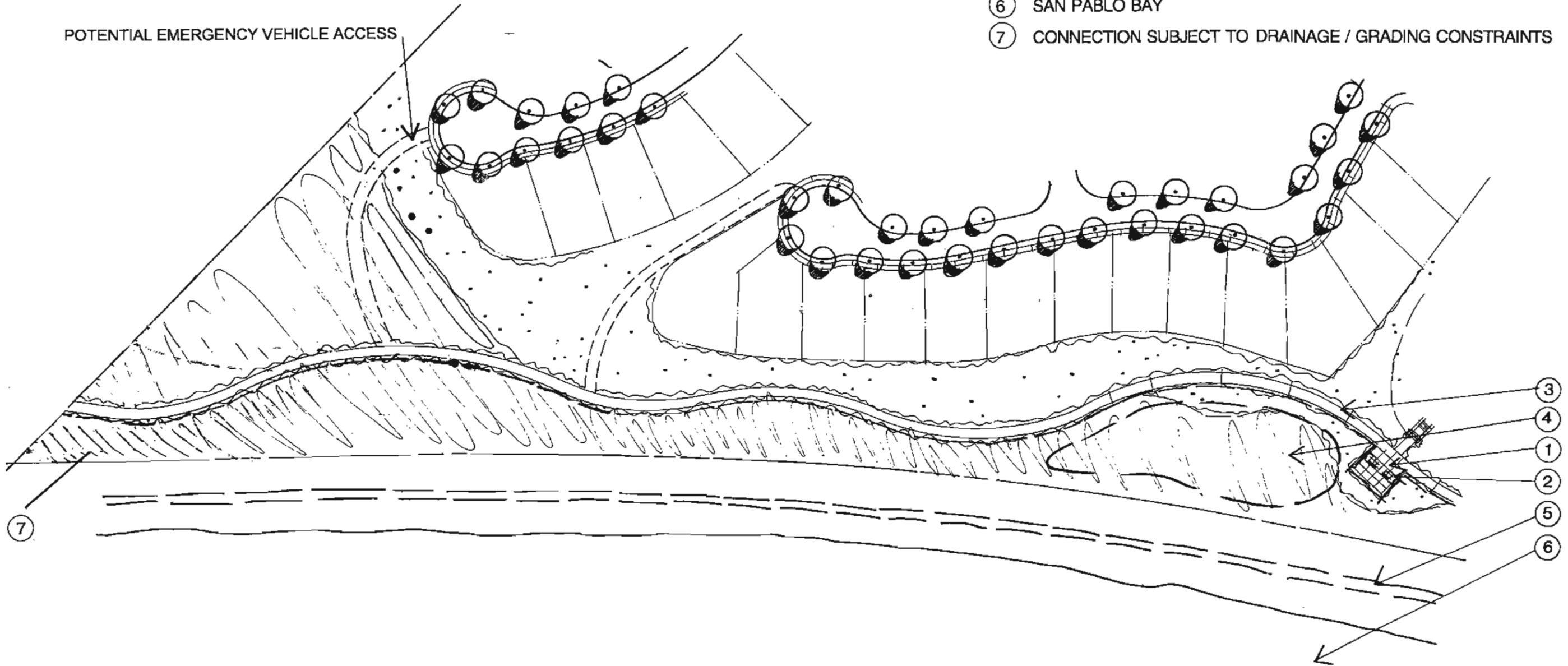
While intended to serve as a passive open lawn, it is large enough to facilitate informal activities such as soccer, throwing a ball or Frisbee or playing a game of touch football. The walk surrounding the oval shaped "Green" is designed to support pedestrian/bicycle traffic and will connect the internal multipurpose trail system to the pedestrian/bicycle trail of the Shoreline Trail Park. Likely facilities within the Neighborhood Park could include bicycle racks, benches, drinking fountains, trash receptacles and area lighting.

The park may be accessed by the project's internal walkway/park system. If implemented, garden structures, as conceptually depicted on *Exhibit 40*, may provide both shade and a focal point for the neighborhood. The park may accommodate child's play/picnic areas with irrigated shade trees, shrubs and ground covers surrounding them. Overall, the planting selection should consist largely of drought tolerant plantings. Other

DESIGN IS CONCEPTUAL IN NATURE
AND MAY INCLUDE SOME OF THE
ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.

- ① VIEW PLATFORM
- ② SEATING
- ③ PEDESTRIAN / BICYCLE TRAIL
- ④ POTENTIAL RETENTION AREA, PER FINAL DRAINAGE PLANS
- ⑤ EXISTING RAILROAD
- ⑥ SAN PABLO BAY
- ⑦ CONNECTION SUBJECT TO DRAINAGE / GRADING CONSTRAINTS

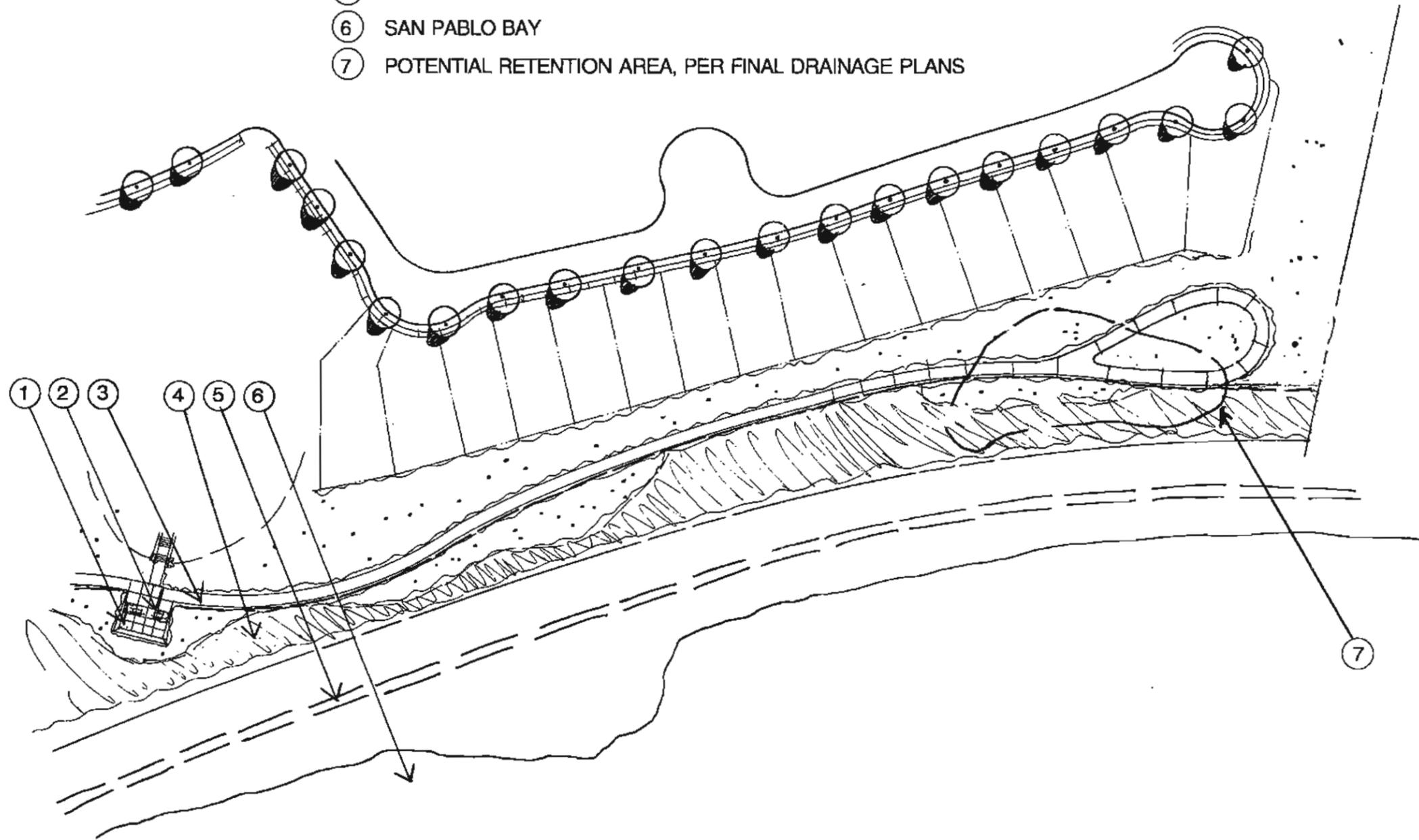
POTENTIAL EMERGENCY VEHICLE ACCESS



CONCEPTUAL SHORELINE TRAIL / PARK LANDSCAPE PLAN (NORTH)
NEW PACIFIC PROPERTIES SPECIFIC PLAN

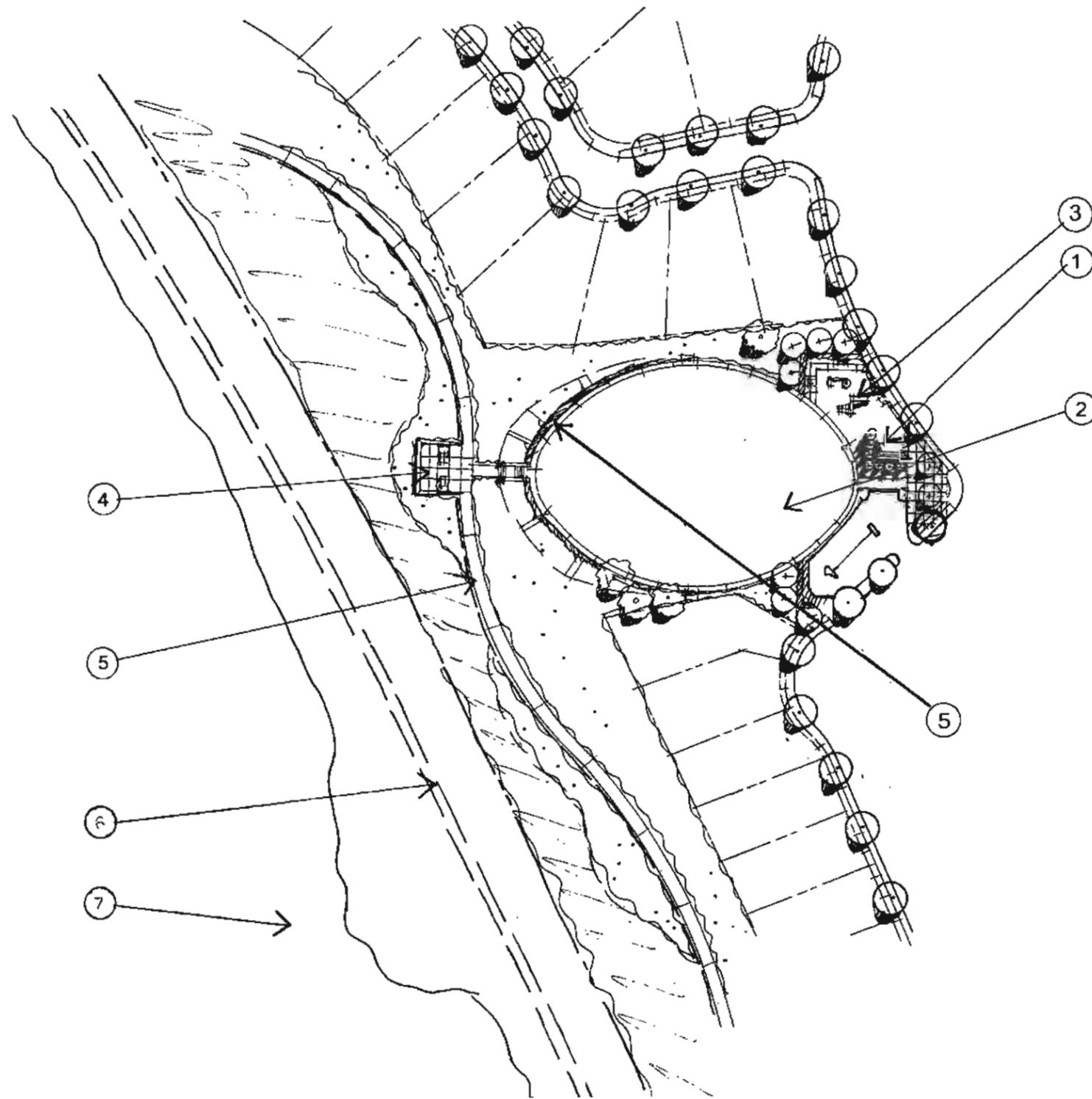
- ① VIEW PLATFORM
- ② SEATING
- ③ PEDESTRIAN / BICYCLE TRAIL
- ④ SLOPE
- ⑤ EXISTING RAILROAD
- ⑥ SAN PABLO BAY
- ⑦ POTENTIAL RETENTION AREA, PER FINAL DRAINAGE PLANS

DESIGN IS CONCEPTUAL IN NATURE
AND MAY INCLUDE SOME OF THE
ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.



CONCEPTUAL SHORELINE TRAIL / PARK LANDSCAPE PLAN (SOUTH)

NEW PACIFIC PROPERTIES SPECIFIC PLAN



**PARKING AREA IS SUBJECT TO REDESIGN
PER FINAL ENGINEERING PLANS**

- ① GARDEN FEATURE
- ② FORMAL LAWN
- ③ PLAY AREA
- ④ VIEW OVERLOOK
- ⑤ PEDESTRIAN / BICYCLE PATH
- ⑥ RAILROAD
- ⑦ SAN PABLO BAY

**DESIGN IS CONCEPTUAL IN NATURE
AND MAY INCLUDE SOME OF THE
ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.**

**CONCEPTUAL NEIGHBORHOOD PARK LANDSCAPE PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN**

than at the child's play/picnic areas, trees may be planted on the north and south edge of the park with special consideration given to scenic views from adjacent lots and leaving open vistas over the Shoreline Trail Park to San Pablo Bay.

c. Linear Park

The Linear Park is intended be developed as a classic urban type passive park designed as one element or divided into two segments, each bounded on all sides with pedestrian and vehicular circulation. Refer to *Exhibit 41, Conceptual Linear Park Landscape Plan*.

Tree lined pedestrian/bicycle trails passing through the Linear Park will link the Community Trail Park to the Neighborhood Park. Direct access to the Linear Park will be via pedestrian crosswalks at each corner.

The eastern segment is the most immediate connection to the Community Trail Park, linked by garden elements which visually tie the two parks together. This eastern segment is proposed as a passive green bounded on the north and south side with shade trees, seating, trash receptacles, lighting and focal elements. The large open turf green may be used in a variety of ways, including neighborhood events, family gatherings, or quieter passive uses.

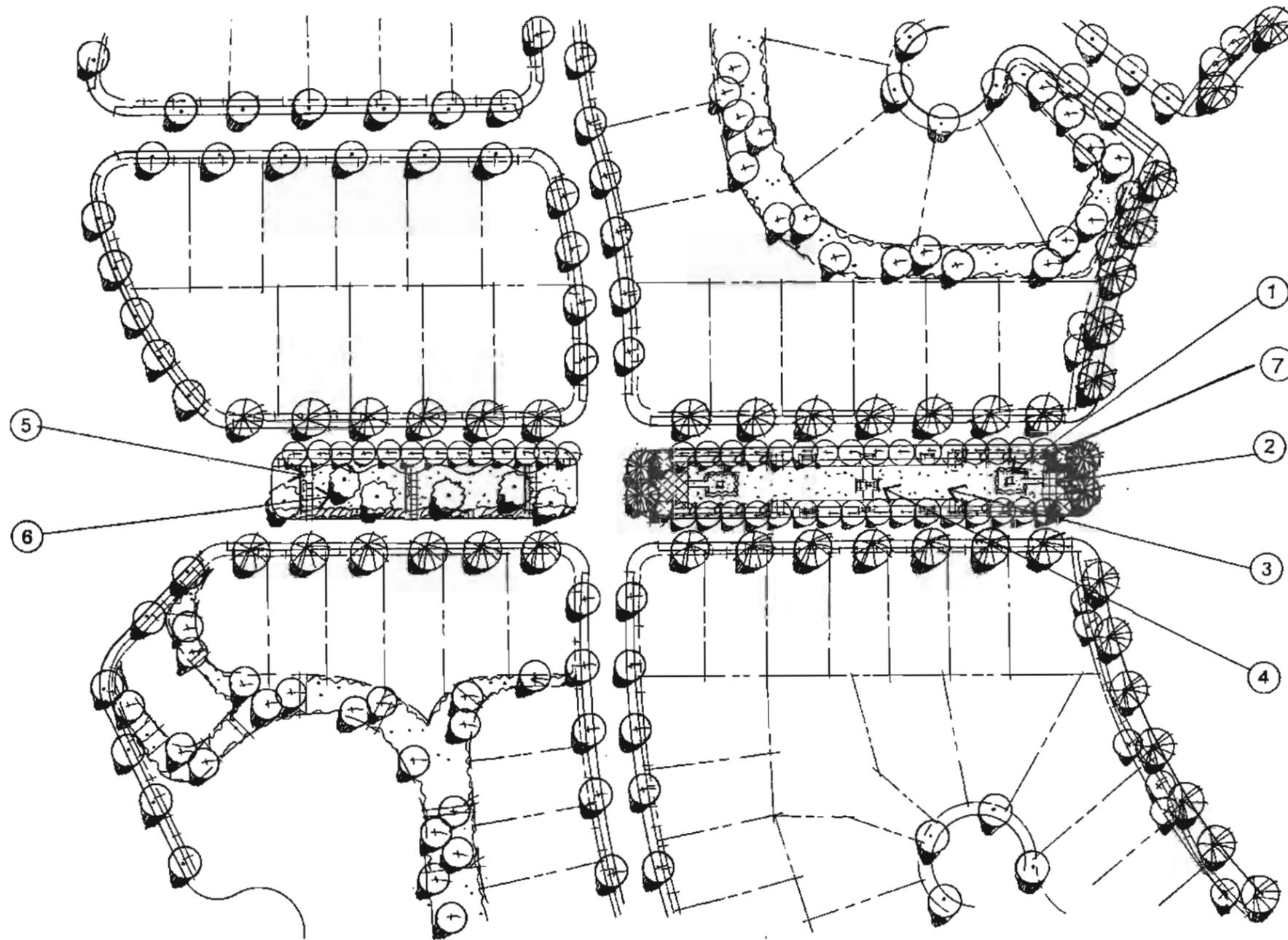
The western segment has the capacity for passive enjoyment, and is intended to be more contemplative in spirit. Access can be from all sides leading to a pedestrian/bicycle trail connecting elements from east to west. This portion of the Linear Park may be comprised of a series of spaces separated by physical elements such as hedges, screens, art features, or garden ornaments. The landscaping of this park will be irrigated and furnished with lighting and focal elements.

d. Community Trail Park

The Community Trail Park is intended to be a unifying pedestrian open space element connecting internal residential areas to other amenities, neighborhoods and public facilities within the community. Trails, trees and attractive landscaping area envisioned throughout the Community Trail Park. The Community Trail Park component could also be divided into zones that change relative to surrounding neighborhood elements. Refer to *Exhibit 42, Conceptual Community Trail Park Landscape Plan*. Each of the zones could be distinguished with differing landscape elements or even by garden themes that will determine planting, hardscape and furnishings to characterize each zone.

General ideas for the distinct spaces created by the Community Trail Park's placement within the P4 area could include:

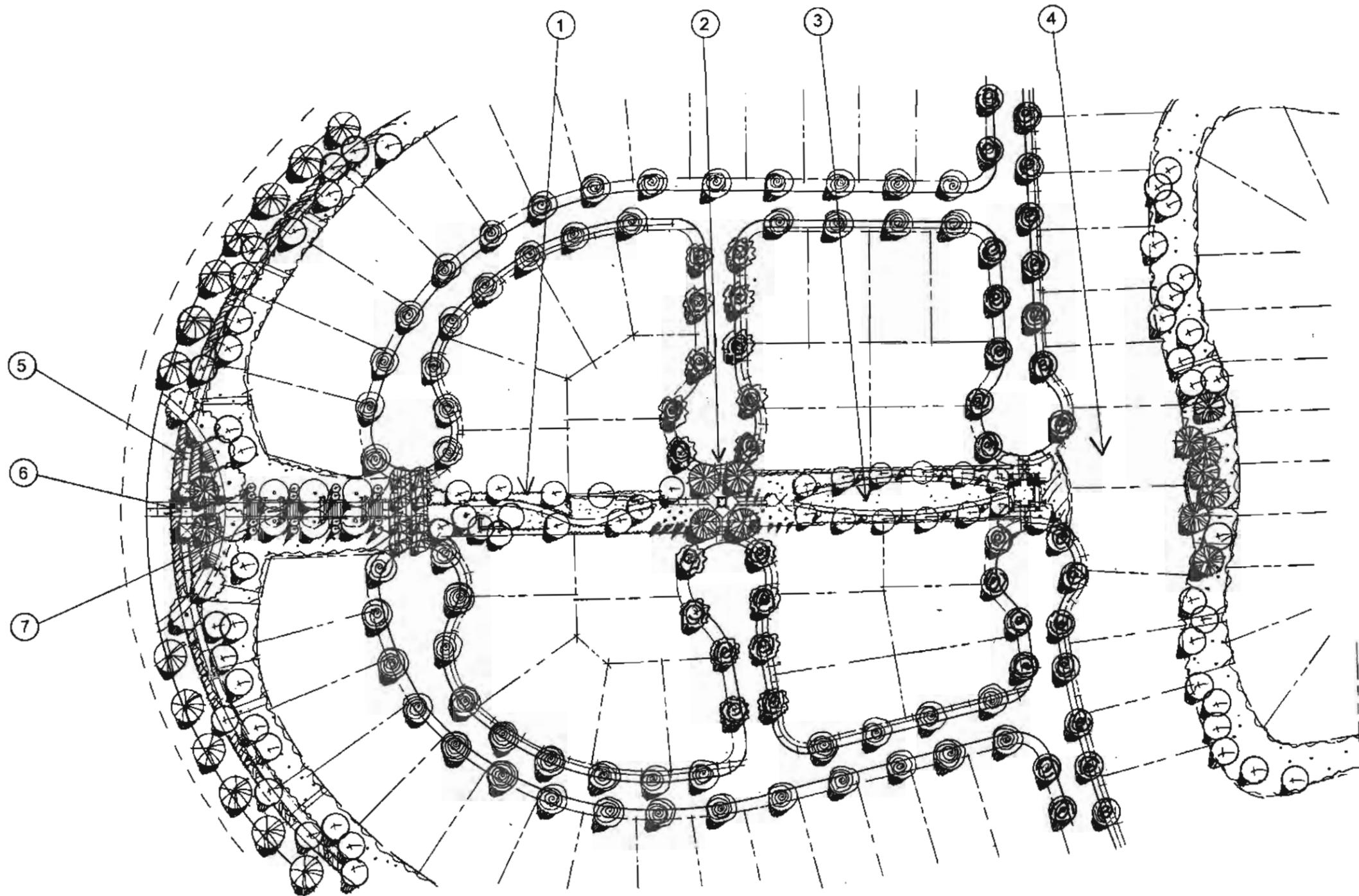
- Passive Open Space Concept - A landscaped area of trees, vegetation, turf and meandering trails.



- ① FOCAL ELEMENT
- ② PLAZA
- ③ LAWN GARDEN
- ④ SEATING AREA
- ⑤ WALKWAY
- ⑥ INFORMAL GARDEN
- ⑦ BICYCLE PATH

DESIGN IS CONCEPTUAL IN NATURE
AND MAY INCLUDE SOME OF THE
ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.

CONCEPTUAL LINEAR PARK LANDSCAPE PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN



- ① SHRUBS
- ② ARRIVAL AREA
- ③ LAWN OPEN SPACE
- ④ RESIDENTIAL
- ⑤ GARDEN WALKWAY
- ⑥ CRESCENT ACCESS GARDEN
- ⑦ GRAND STAIRCASE

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

CONCEPTUAL COMMUNITY TRAIL PARK LANDSCAPE PLAN

NEW PACIFIC PROPERTIES SPECIFIC PLAN

- Crescent Access Garden Concept - The western most garden, this space may be distinguished by a crescent-shaped walkway leading to the remainder of the Community Trail Park. This crescent may be emphasized by a row of background trees, which frame the space. Within the crescent, garden elements may occur, similar to those in the eastern most segment of the Linear Park. These would visually tie both parks together by reinforcing a major community node. The crescent walkway would lend access to the first courtyard as well as the remainder of the Community Trail Park.
- Meandering Garden Concept - Immediately east of the crescent access garden, a meandering garden concept could occur which would possibly be characterized by a curvilinear path winding through intimate seating areas.
- Contemplative Garden Concept - A contemplative garden could be located east of the meandering garden and adjacent to the sloped bank bounding the west side of P4. This garden may be characterized by formal walkways and garden ornaments placed along the garden axis.

Each access point, where opposing cul-de-sacs meet in P2, is suggested as a courtyard with a distinct landscape design. All planted areas will be fully irrigated and furnished with pedestrian amenities such as night lighting, seating, shade trees, trash receptacles and special focal elements needed to characterize each zone.

e. Natural Open Space

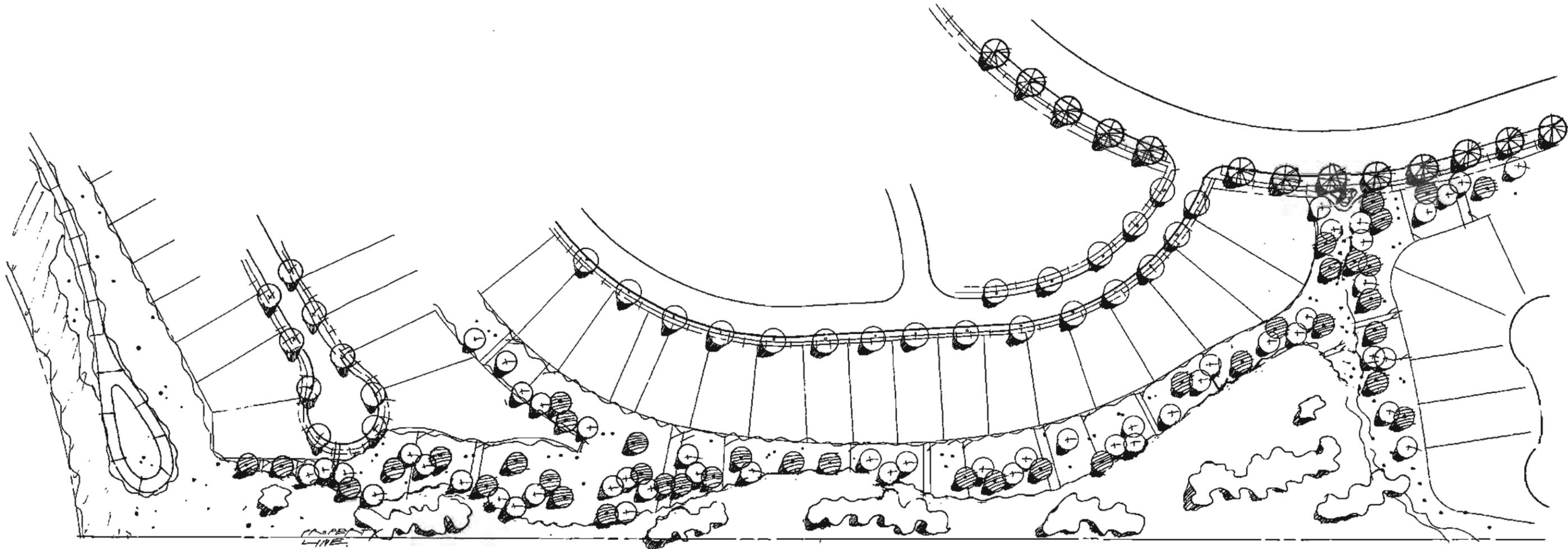
The New Pacific Properties landscape area includes uses adjacent to the natural area near the southwestern portion of the site. Refer to *Exhibit 43, Conceptual Natural Open Space Landscape Plan*.

- All areas identified within this zone should establish a "riparian" landscape character using selected plant materials common to the natural corridor within. All planting material to occur within and/or adjacent to the natural corridor and wetland area is to be naturalized and in conformance with the recommendations of the Environmental Impact Report.
- Areas immediately adjacent to the watercourse should be developed with landscape treatments which reinforce the natural plant materials at this site. The wetland area is intended to be naturalized in accordance with agency criteria.

f. Enhanced Parkway

The Loop Roadway that ties all neighborhoods together within New Pacific Properties will be enhanced with an enlarged landscaped parkway. Refer to *Exhibits 29 and 30, Loop Roadway/Enhanced Parkway Streetscape Plan and Sections*. Six-foot walkways will wind along both sides of the Collector Road. Where appropriate, amenities may include theme lighting, seating opportunities and special planting treatments for visual

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ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.



FINAL DESIGN FOR THIS AREA IS
SUBJECT TO AGENCY APPROVAL

CONCEPTUAL NATURAL OPEN SPACE LANDSCAPE PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN

interest. It is anticipated that this will be a highly utilized community element within the project encouraging neighbors to come out of their homes for recreation and interaction.

g. Edge and Buffer Treatments

Existing tree plantings bound the project on the northern boundary. Refer to *Exhibit 44, Buffer Treatment Landscape Cross Section*. Refer to *Exhibit 26* for locations of these conditions. It is the intent of this Specific Plan that all planting on these slopes be temporarily irrigated to naturalize with indigenous plant material.

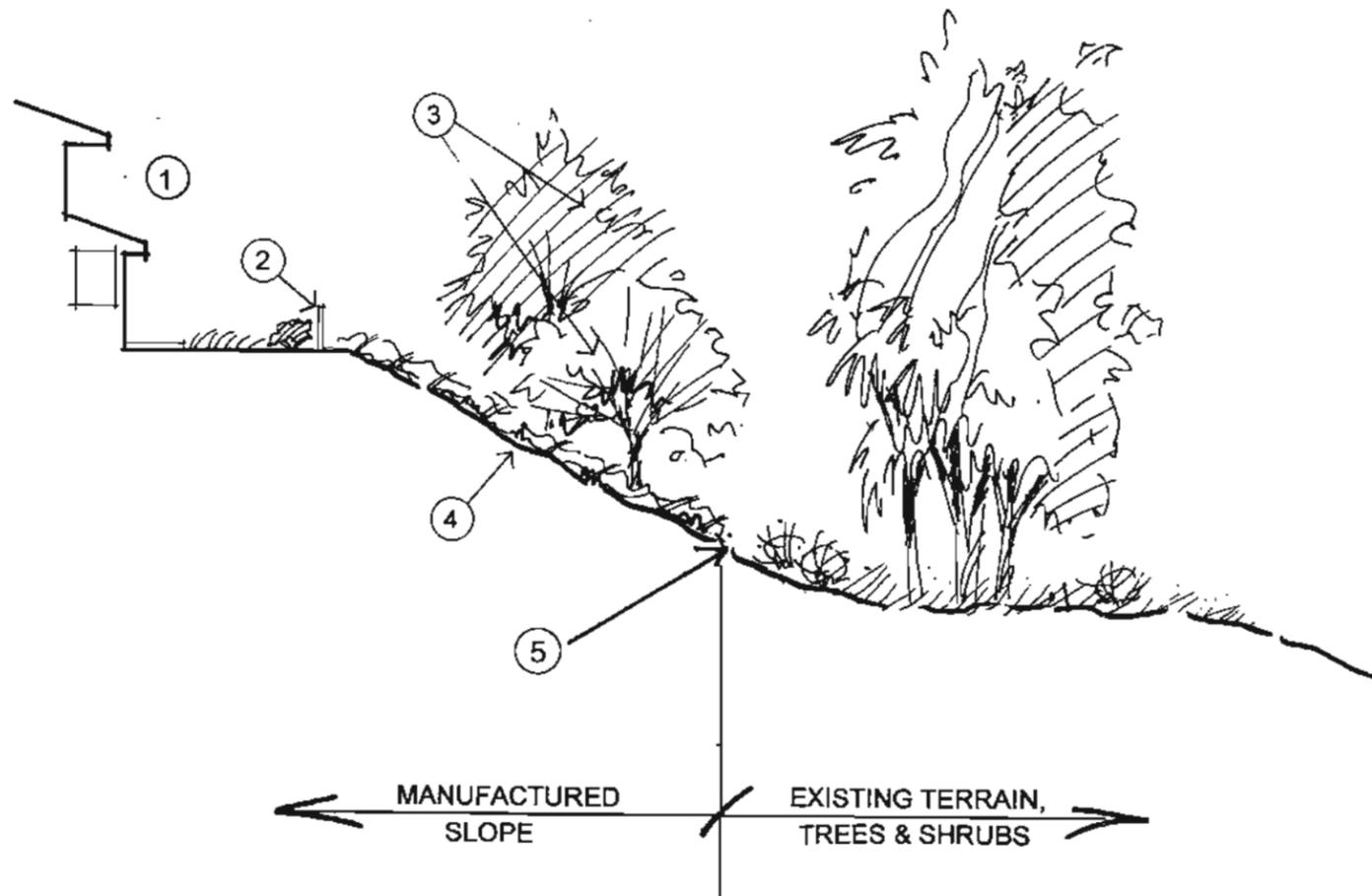
h. Other Open Space

In addition to the above, New Pacific Properties includes Community View Slopes, Hillside Planting/Manufactured Slopes Adjacent Open Space and Hillside Planting/Native Slopes. Refer to *Exhibit 45, Community View Slopes Section*, and *Exhibits 46 and 47, Hillside Planting/Manufactured Slopes and Native Slopes*, for specific treatment of these areas. These areas are located on *Exhibit 26, Landscape Components Plan*.

8. Recommended Plant Palette

a. Landscape Corridor Theme Trees

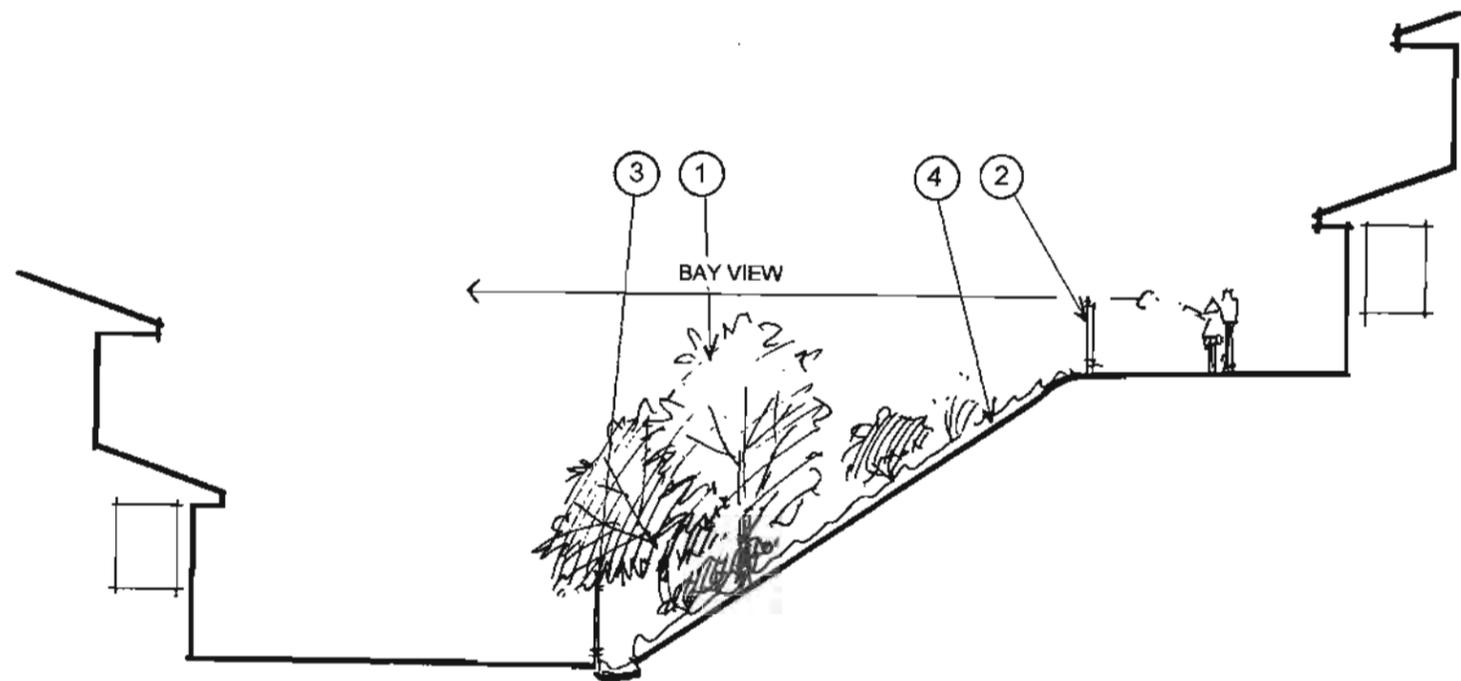
| <u>Botanical Name</u> | <u>Common Name</u> |
|---------------------------------------|----------------------------|
| • Shoreline Trail Buffer Slope | |
| → <i>Cupressus macrocarpa</i> | Monterey Cypress |
| → <i>Melaleuca nesophila</i> | Pink Melaleuca |
| → <i>Metrosideros excelsus</i> | New Zealand Christmas Tree |
| → <i>Pinus halepensis</i> | Aleppo Pine |
| → <i>Pinus pinaster</i> | Cluster Pine |
| → <i>Pinus radiata</i> | Monterey Pine |
| • View Slope | |
| → <i>Cercis occidentalis</i> | Western Redbud |
| → <i>Cupressus macrocarpa</i> | Monterey Cypress |
| → <i>Koelreuteria bipinnata</i> | Chinese Flame Tree |
| → <i>Melaleuca nesophila</i> | Pink Melaleuca |
| → <i>Melaleuca quinquenervia</i> | Cajeput Tree |
| → <i>Pinus halepensis</i> | Aleppo Pine |
| → <i>Pinus radiata</i> | Monterey Pine |
| → <i>Pittosporum undulatum</i> | Victorian Box |
| → <i>Schinus molle</i> | California Pepper |



- ① RESIDENTIAL EDGE
- ② VIEW FENCE OR PERIMETER WALL / FENCE
- ③ TREE SELECTION TO BLEND AND CONTRAST WITH EXISTING TREE FORMS, COLORS AND TEXTURES, IF PLANTED.
- ④ SHRUBS AND GROUND COVERS
- ⑤ POTENTIAL DRAINAGE SWALE

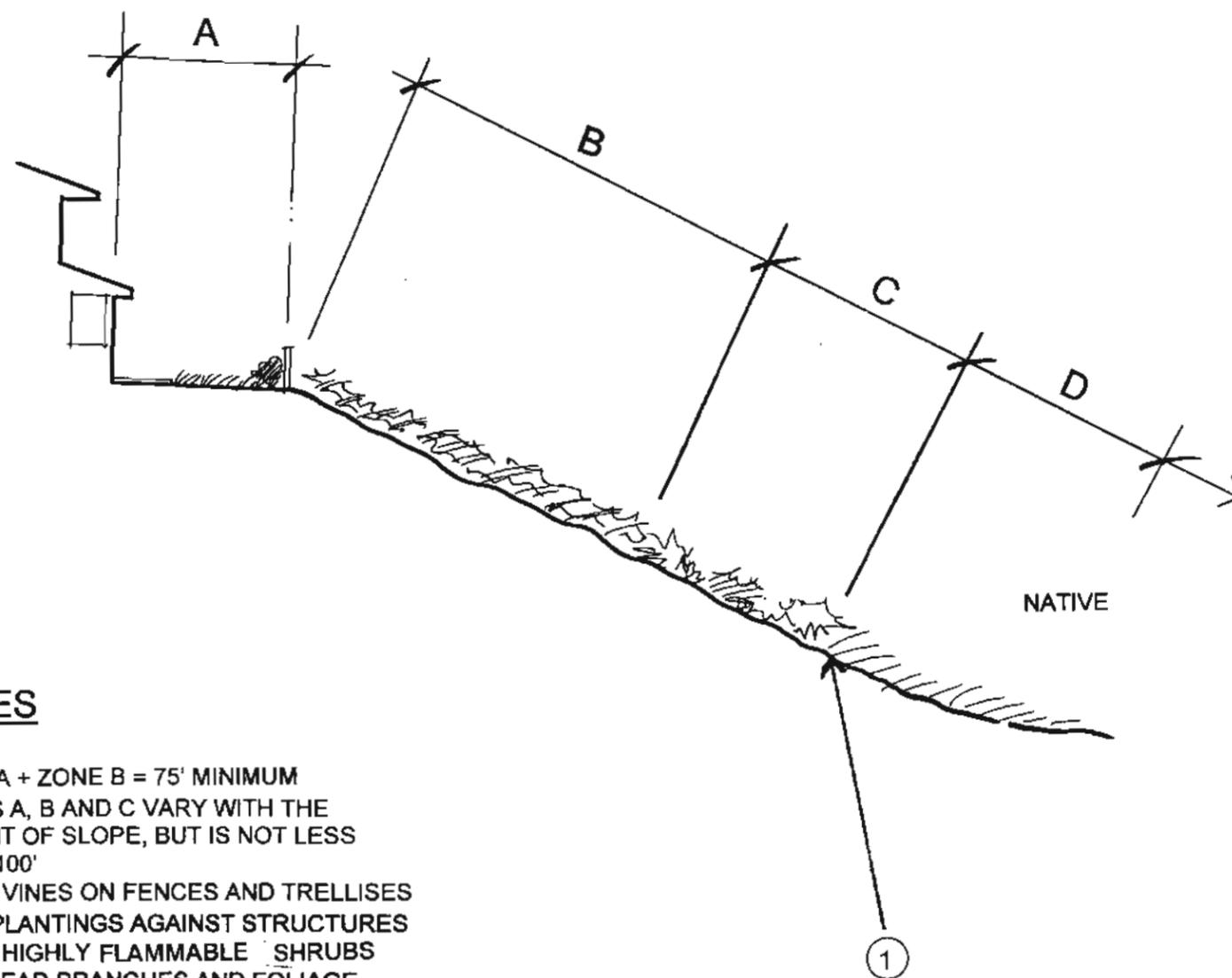
DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

BUFFER TREATMENT LANDSCAPE CROSS SECTION
NEW PACIFIC PROPERTIES SPECIFIC PLAN



- ① TREES AT BASE OF SLOPE
- ② VIEW FENCE
- ③ WALL OR FENCE
- ④ SHRUBS AND GROUND COVERS

DESIGN IS CONCEPTUAL IN NATURE
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ELEMENTS DEPICTED OR OTHERS
NOT SHOWN.



ZONE - A

- IRRIGATED
- RESTRICT HIGH FUEL SPECIES

ZONE - B

- IRRIGATED
- LOW FUEL
- NATURALIZING

ZONE - C

- IRRIGATED
- NATURALIZING

ZONE - D

- NATIVE THINNING ZONE

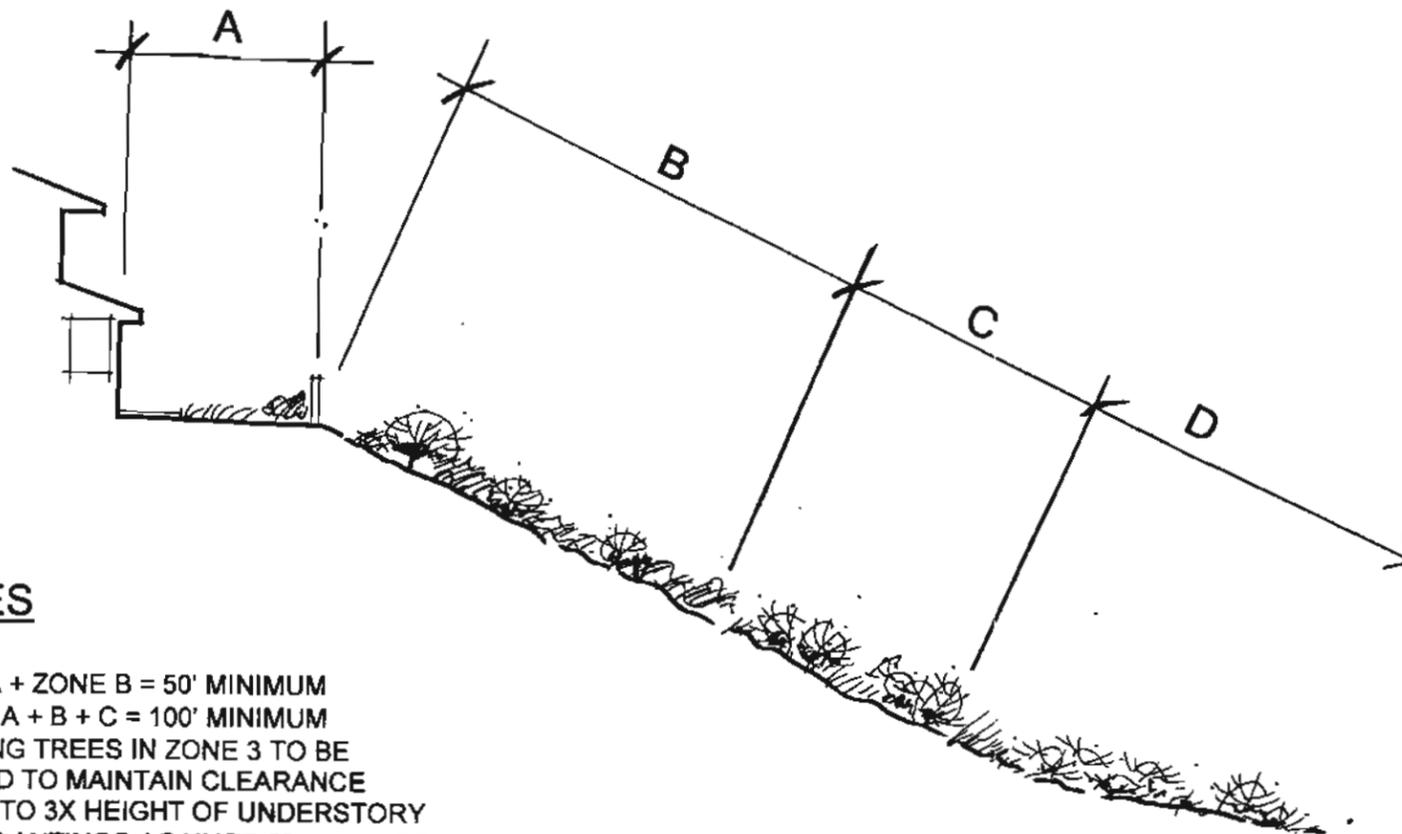
DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

NOTES

- ZONE A + ZONE B = 75' MINIMUM
- ZONES A, B AND C VARY WITH THE EXTENT OF SLOPE, BUT IS NOT LESS THAN 100'
- AVOID VINES ON FENCES AND TRELLISES
- LIMIT PLANTINGS AGAINST STRUCTURES
- AVOID HIGHLY FLAMMABLE SHRUBS
- THIN DEAD BRANCHES AND FOLIAGE

① POTENTIAL DRAINAGE SWALE

1



NOTES

- ZONE A + ZONE B = 50' MINIMUM
- ZONES A + B + C = 100' MINIMUM
- EXISTING TREES IN ZONE 3 TO BE PRUNED TO MAINTAIN CLEARANCE EQUAL TO 3X HEIGHT OF UNDERSTORY
- LIMIT PLANTINGS AGAINST STRUCTURES
- AVOID HIGHLY FLAMMABLE SHRUBS
- THIN DEAD BRANCHES AND FOLIAGE

ZONE – A

- IRRIGATED
- RESTRICT HIGH FUEL SPECIES

ZONE – B

- IRRIGATED (TEMPORARY)
- PLANTING OF LOW FUEL SPECIES
- SELECTIVE PRUNING OF 60% OF THE MODERATE FUEL SPECIES VOLUME
- 100% REMOVAL OF HIGH FUEL SPECIES

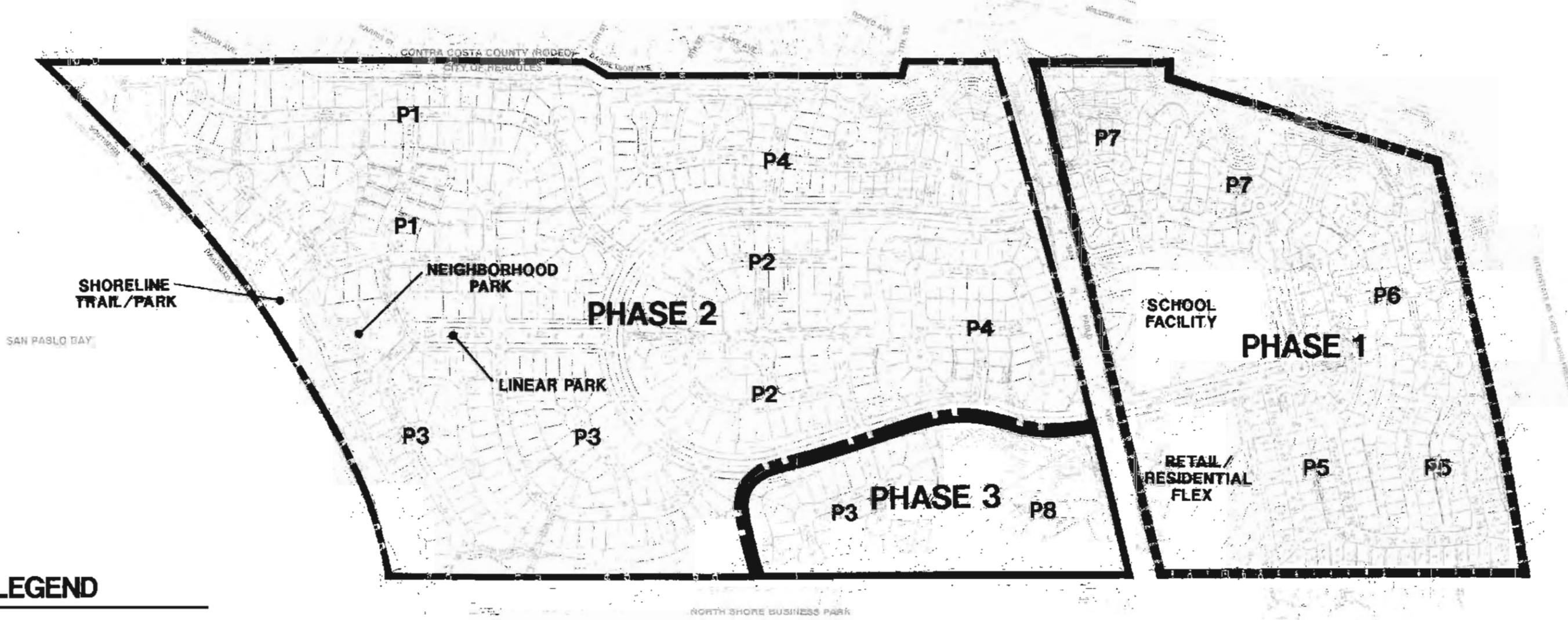
ZONE – C

- NON-IRRIGATED
- LIMITED OR NO PLANTING
- SELECTIVE PRUNING OF 40% OF THE MODERATE FUEL SPECIES VOLUME
- 100% REMOVAL OF HIGH FUEL SPECIES

ZONE – D

- NATIVE (UNDISTURBED)

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.



LEGEND

-  PHASE 1
-  PHASE 2
-  PHASE 3

| <u>Botanical Name</u> | <u>Common Name</u> |
|-------------------------------------|-----------------------------|
| <i>Myoporum Pacificum</i> ' " | NCN |
| <i>Oenothera berlandieri</i> | Mexican Evening Primrose |
| <i>Pelargonium spp.</i> | Geranium |
| <i>Pennisetum spp.</i> | Fountain Grass |
| <i>Phyla nodiflora</i> | Lippia |
| <i>Ribes viburnifolium</i> | Evergreen Currant |
| <i>Rosmarinus o. Prostratus</i> ' " | Prostrate Rosemary |
| <i>Sollya heterophylla</i> | Australian Bluebell Creeper |
| <i>Stipa pulchra</i> | Purple Needlegrass |
| <i>Trachelospermum jasminoides</i> | Star Jasmine |
| <i>Verbena spp.</i> | NCN |
| <i>Vinca spp.</i> | Periwinkle |

9. Lighting

Lighting throughout the New Pacific Properties project will be designed to differentiate between land use areas, emphasize community amenities, provide continuity along street corridors and ensure the safety of residents and users. In general, lighting will be designed to minimize light levels for any given application and to emphasize high use areas or objects to be illuminated. Low level, pedestrian scale fixtures will be utilized to the degree feasible.

Exterior lighting will be shielded or recessed to minimize direct glare and reflections. Lighting that represents movement, flashes, blinks or is of unusual high intensity or brightness is prohibited. See *Exhibit 48, Lighting Standards*, for typical light fixtures and light fixture selections for the Specific Plan.

a. Street Lighting

- City of Hercules Department of Public Works Improvement Standards should be the standards for New Pacific Properties, except as modified herein.
- Light standards along all roadways should be located at regular intervals along the sides of the roadway, or as double-headed fixtures in the median landscape strip, as appropriate.
- Light standards will be of a consistent scale, character and color along the complete length of San Pablo Avenue. Fixtures may be the same throughout the community, or may vary by street.
- Street lights and standards should be consistent within each neighborhood.
- Lighting may be concentrated at intersections, crosswalks and other pedestrian areas.
- Street lighting should be directionally shaded to reduce off-site fugitive light and glare. All streetlights should use high-pressure sodium, or other high efficiency lamps.

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| <u>Botanical Name</u> | <u>Common Name</u> |
|------------------------------------|------------------------|
| <i>Distictis buccinatoria</i> | Blood-Red Trumpet Vine |
| <i>Ficus pumila</i> | Creeping Fig |
| <i>Jasminum polyanthum</i> | Pink Jasmine |
| <i>Lonicera japonica</i> | Japanese Honeysuckle |
| <i>Magnolia grandiflora</i> | Southern Magnolia |
| <i>Parthenocissus tricuspidata</i> | Boston Ivy |
| <i>Passiflora spp.</i> | Passion Vine |
| <i>Podocarpus gracilior</i> | Fern Pine |
| <i>Pyracantha 'Santa Cruz'</i> | Firethorn |
| <i>Pyrus kawakamii</i> | Evergreen Pear |
| <i>Rosa banksiae</i> | Lady Banks' Rose |
| <i>Parthenocissus tricuspidata</i> | Boston Ivy |
| <i>Solanum jasminoides</i> | Potato Vine |
| <i>Wisteria sinensis</i> | Chinese Wisteria |

e. Community Groundcover List

| <u>Botanical Name</u> | <u>Common Name</u> |
|------------------------------------|-----------------------|
| <i>Allium peninsulare</i> | Wild Onion |
| <i>Arctostaphylos spp.</i> | Manzanita |
| <i>Avena fatua</i> | Wild Oat |
| <i>Baccharis pilularis</i> | Dwarf Coyote Bush |
| <i>Bromus spp.</i> | Brome Grass |
| <i>Carpobrotus edulis</i> | Iceplant |
| <i>Ceanothus spp.</i> | Wild Lilac |
| <i>Clarkia spp.</i> | Clarkia |
| <i>Cotoneaster spp.</i> | Cotoneaster |
| <i>Dichelostemma pulchellum</i> | Wild Hyacinth |
| <i>Elymus spp.</i> | Lyme Grass |
| <i>Eriogonum fasciculatum</i> | California Buckwheat |
| <i>Eschscholzia californica</i> | California Poppy |
| <i>Euonymus spp.</i> | Purple Winter Creeper |
| <i>Festuca spp.</i> | Fescue |
| <i>Hedera helix 'Hahnii'</i> | English Ivy |
| <i>Helictotrichon sempervirens</i> | Blue Oat Grass |
| <i>Heuchera sanguinea</i> | Coral Bells |
| <i>Hordeum californicum</i> | California Barley |
| <i>Iris douglasiana</i> | Douglas Iris |
| <i>Juniperus spp.</i> | Juniper |
| <i>Lonicera japonica</i> | Honeysuckle |
| <i>Lupinus nanus</i> | Sky Lupine |
| <i>Mahonia spp.</i> | Oregon Grape |
| <i>Mimulus spp.</i> | Monkeyflower |
| <i>Muhlenbergia rigens</i> | Deer Grass |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| <u>Botanical Name</u> | <u>Common Name</u> |
|------------------------------------|--------------------------------------|
| <i>Feijoa sellowiana</i> | Pineapple Guava |
| <i>Fuchsia t.</i> | |
| <i>Gartenmeister Bonstedt</i> ' | Fuchsia |
| <i>Hemerocallis spp.</i> | Daylily |
| <i>Ilex vomitoria</i> | Yaupon |
| <i>Kniphofia uvaria</i> | Red-Hot Poker |
| <i>Lavandula spp.</i> | Lavender |
| <i>Lavatera assurgentiflora</i> | Tree Mallow |
| <i>Leptospermum scoparium</i> | New Zealand Tea Tree |
| <i>Ligustrum j. Texanum</i> ' | Japanese Privet |
| <i>Liriope muscari gigantea</i> | Big Blue Lily Turf |
| <i>Myrica californica</i> | Pacific Wax Myrtle |
| <i>Osmanthus fragrans</i> | Sweet Olive |
| <i>Nephrolepis cordifolia</i> | Southern Sword Fern |
| <i>Pennisetum setaceum</i> | Fountain Grass |
| <i>Phormium tenax spp.</i> | New Zealand Flax |
| <i>Pittosporum tobira</i> | Tobira |
| <i>Plumbago auriculata</i> | Cape Plumbago |
| <i>Podocarpus spp.</i> | Fern Pine |
| <i>Prunus spp.</i> | Hollyleaf, Catalina, Carolina Cherry |
| <i>Punica granatum</i> | Pomegranate |
| <i>Pyracantha spp.</i> | Firethorn |
| <i>Raphiolepis spp.</i> | India Hawthorn |
| <i>Rhamnus californica</i> | Coffeeberry |
| <i>Rhus spp.</i> | Sumac |
| <i>Ribes spp.</i> | Gooseberry |
| <i>Rosa spp.</i> | Rose |
| <i>Rosmarinus officinalis</i> | Rosemary |
| <i>Rubus vitifolius</i> | Pacific blackberry |
| <i>Rudbeckia hirta</i> | Black-eyed Susan |
| <i>Salvia spp.</i> | Sage |
| <i>Sambucus mexicana</i> | Blue Elderberry |
| <i>Santolina spp.</i> | Lavender Cotton |
| <i>Scirpus cernus</i> | Fiber Optics Plant |
| <i>Stachys byzantina</i> | Lamb's Ears |
| <i>Taxus spp.</i> | Yew |
| <i>Trachelospermum jasminoides</i> | Star Jasmine |
| <i>Xylosma congestum</i> | Shiny Xylosma |

d. Community Vine and Espalier List

| <u>Botanical Name</u> | <u>Common Name</u> |
|-----------------------------------|---------------------|
| <i>Clematis armandii</i> | Evergreen Clematis |
| <i>Clytostoma callistegioides</i> | Violet Trumpet Vine |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| <u>Botanical Name</u> | <u>Common Name</u> | <u>Evergreen (E)</u> <u>Deciduous (D)</u> |
|--|-----------------------------|--|
| <i>Platanus acerifolia</i> | London Plane Tree | D |
| <i>Platanus acerifolia 'Yarwood'</i> | "Yarwood" London Plane Tree | D |
| <i>Platanus racemosa</i> | California Sycamore | D |
| <i>Podocarpus gracilior</i> | Fern Pine | E |
| <i>Populus fremontii</i> | Western Cottonwood | D |
| <i>Populus nigra 'Italica'</i> | Lombardy Poplar | D |
| <i>Prunus caroliniana</i> | Carolina Laurel Cherry | E |
| <i>Prunus cerasifera</i> | Flowering Plum | D |
| <i>Prunus serrulata 'Kwanzan'</i> | Flowering Cherry | D |
| <i>Pyrus calleryana</i> | Ornamental Pear | D |
| <i>Quercus agrifolia</i> | Coast Live Oak | E |
| <i>Quercus chrysolepis</i> | Canyon Live Oak | E |
| <i>Quercus coccinea</i> | Scarlet Oak | D |
| <i>Quercus douglasii</i> | Blue Oak | D |
| <i>Quercus ilex</i> | Holly Oak | E |
| <i>Quercus lobata</i> | Valley oak | D |
| <i>Quercus suber</i> | Cork Oak | E |
| <i>Quercus virginiana</i> | Southern Live Oak | E |
| <i>Quercus wislizenii</i> | Interior Live Oak | E |
| <i>Rhus lancea</i> | African Sumac | E |
| <i>Salix babylonica</i> | Weeping Willow | D |
| <i>Schinus molle</i> | California Pepper | E |
| <i>Schinus terebinthefolius</i> | Brazilian Pepper | E |
| <i>Sequoia sempervirens</i> | Coast Redwood | E |
| <i>Tilia tomentosa</i> | Silver Linden | D |
| <i>Ulmus parvifolia</i> | Evergreen Elm | E |
| <i>Umbellularia californica</i> | California Bay Laurel | E |
| <i>Zelkova serrata 'Village Green'</i> | Sawleaf Zelkova | D |

c. Community Shrub List

| <u>Botanical Name</u> | <u>Common Name</u> |
|-------------------------------------|--------------------------|
| <i>Agapanthus</i> | Lily-of-the-Nile |
| <i>Aloe spp.</i> | Aloe |
| <i>Arctostaphylos spp.</i> | Manzanita |
| <i>Bergenia crassifolia</i> | Winter-Blooming Bergenia |
| <i>Buxus microphylla japonica</i> | Japanese Boxwood |
| <i>Camellia sasanqua 'Yuletide'</i> | Camelia |
| <i>Cistus purpureus</i> | Orchid Rockrose |
| <i>Coleonema pulchrum</i> | Pink Breath of Heaven |
| <i>Dietes bicolor</i> | Fortnight Lily |
| <i>Echium fastuosum</i> | Pride of Madeira |
| <i>Eriobotrya deflexa</i> | Bronze Loquat |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| <u>Botanical Name</u> | <u>Common Name</u> | <u>Evergreen (E) Deciduous (D)</u> |
|---------------------------------------|------------------------------|--|
| <i>Cedrus deodara</i> | Deodar Cedar | E |
| <i>Celtis occidentalis</i> | Common Hackberry | D |
| <i>Ceratonia siliqua</i> | Carob | E |
| <i>Cercis occidentalis</i> | Western Redbud | D |
| <i>Cercocarpus betuloides</i> | Mountain Ironwood | E |
| <i>Chamaerops humilis</i> | Mediterranean Fan Palm | E |
| <i>Cinnamomum camphora</i> | Camphor Tree | E |
| <i>Cornus stolonifera</i> | Redtwig Dogwood | D |
| <i>Crataegus spp.</i> | Hawthorn | D |
| <i>Cupressocyparis leylandii</i> | Leyland Cypress | E |
| <i>Cupressus arizonica glabra</i> | Smooth Arizona Cypress | E |
| <i>Cupressus macrocarpa</i> | Monterey Cypress | E |
| <i>Eucalyptus sideroxylon</i> | Red Iron Bark | E |
| <i>Eucalyptus ficifolia</i> | Red Flowering Gum | E |
| <i>Eucalyptus nicholi</i> | Willow-leaved Peppermint Gum | E |
| <i>Fraxinus oxycarpa 'Raywood'</i> | Raywood Ash | D |
| <i>Fraxinus velutina 'Modesto'</i> | Modesto Ash | D |
| <i>Fraxinus velutina 'Rio Grande'</i> | Fan-Tex Ash | D |
| <i>Geijera parviflora</i> | Australian Willow | E |
| <i>Ginkgo biloba 'Fairmount'</i> | Maidenhair Tree | D |
| <i>Gleditsia triacanthos</i> | Honey Locust | D |
| <i>Jacaranda mimosifolia</i> | Jacaranda | D |
| <i>Juglans hindsii</i> | California Black Walnut | D |
| <i>Koelreuteria bipinnata</i> | Chinese Flame Tree | D |
| <i>Leptospermum laevigatum</i> | Australian Tea Tree | E |
| <i>Ligustrum japonicum</i> | Japanese Privet | E |
| <i>Ligustrum lucidum</i> | Glossy Privet | E |
| <i>Liquidambar styraciflua</i> | American Sweet Gum | D |
| <i>Liriodendron tulipifera</i> | Tulip Tree | D |
| <i>Lithocarpus densiflora</i> | Tanbark Oak | E |
| <i>Maclura pomifera</i> | Osage Orange | D |
| <i>Magnolia St. Mary</i> | St. Mary Magnolia | E |
| <i>Magnolia soulangiana</i> | Saucer Magnolia | D |
| <i>Magnolia stellata</i> | Star Magnolia | D |
| <i>Malus spp.</i> | Flowering Crabapple | D |
| <i>Maytenus boaria</i> | | |
| <i>'Green Showers'</i> | Mayten Tree | E |
| <i>Melaleuca linarifolia</i> | Flaxleaf Paperbark | E |
| <i>Melaleuca quinquenervia</i> | Cajeput Tree | E |
| <i>Morus alba</i> | Mulberry | D |
| <i>Olea europaea</i> | Olive | E |
| <i>Phoenix canariensis</i> | Canary Island Date Palm | E |
| <i>Pinus halepensis</i> | Aleppo Pine | E |
| <i>Pistacia chinensis</i> | Chinese Pistache | D |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

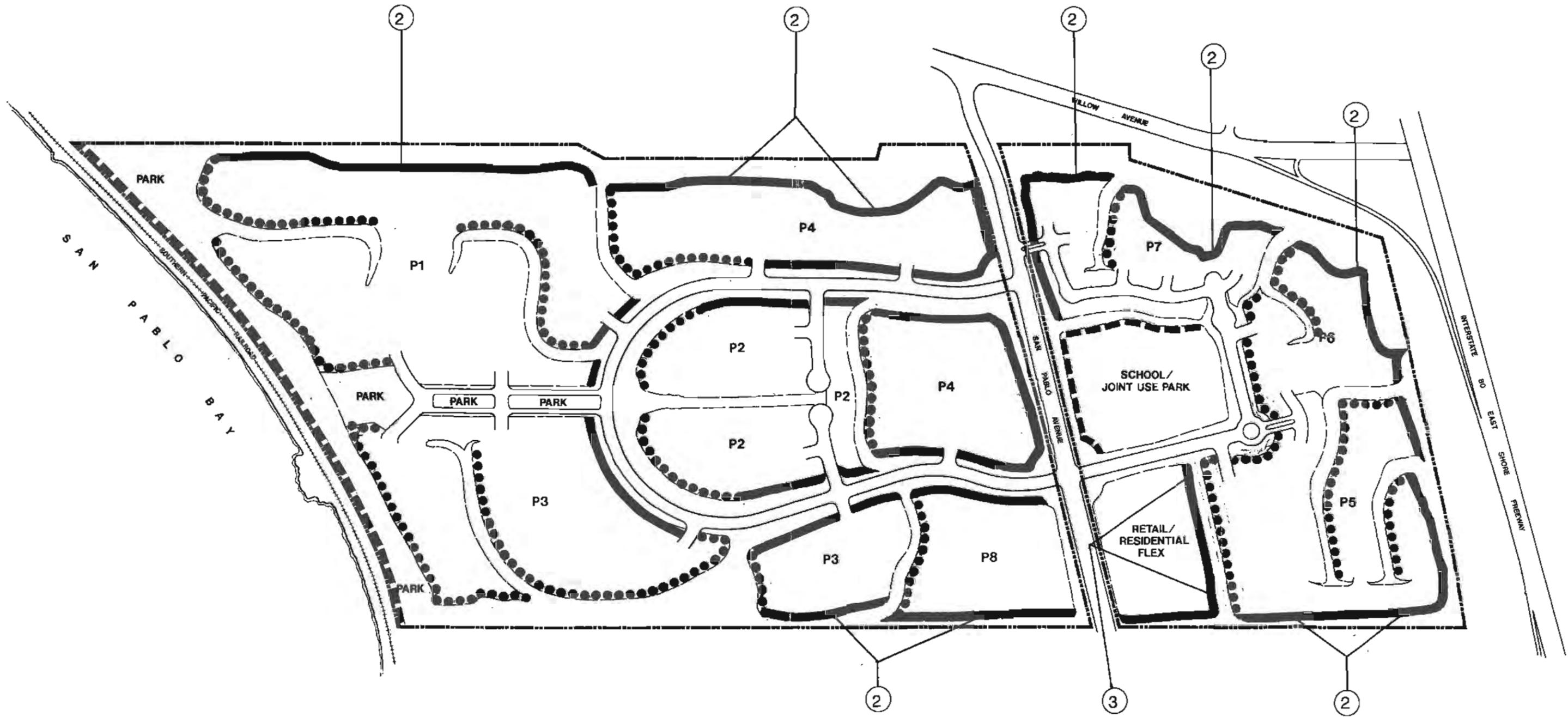
| <u>Botanical Name</u> | <u>Common Name</u> |
|------------------------------------|----------------------------|
| • Retail/Residential Flex | |
| → <i>Eucalyptus ficifolia</i> | Red-Flowering Gum |
| → <i>Liriodendron tulipifera</i> | Tulip Tree |
| → <i>Pinus halepensis</i> | Aleppo Pine |
| → <i>Platanus acerifolia</i> | London Plane Tree |
| → <i>Prunus caroliniana</i> | Carolina Laurel Cherry |
| → <i>Quercus ilex</i> | Holly Oak |
| • Product P1, P3, P5 and P6 | |
| → <i>Magnolia g. St. Mary'</i> | St. Mary Magnolia |
| → <i>Metrosideros excelsus</i> | New Zealand Christmas Tree |
| → <i>Tilia tomentosa</i> | Silver Linden |
| → <i>Ulmus parvifolia</i> | Evergreen Elm |
| • Product P2 | |
| → <i>Cinamomum camphora</i> | Camphor Tree |
| → <i>Magnolia St. Mary</i> | St. Mary Magnolia |
| → <i>Schinus terebinthifolius</i> | Brazilian Pepper |
| • Product P4 and P7 | |
| → <i>Koelreuteria bipinnata</i> | Chinese Flame Tree |
| → <i>Cupaniopsis anacardioides</i> | Carrotwood |
| → <i>Liquidambar styraciflua</i> | American Sweet Gum |
| → <i>Lithocarpus densiflora</i> | Tan Bark Oak |
| → <i>Quercus palustris</i> | Pin Oak |
| → <i>Schinus molle</i> | California Pepper |

b. Community Tree List (All Neighborhoods)

| <u>Botanical Name</u> | <u>Common Name</u> | <u>Evergreen (E)</u> <u>Deciduous (D)</u> |
|---------------------------------|----------------------|--|
| <i>Acacia melanoxylon</i> | Blackwood Acacia | D |
| <i>Acer palmatum</i> | Japanese Maple | D |
| <i>Aesculus californica</i> | California Buckeye | D |
| <i>Aesculus hippocastanum</i> | Horsechestnut | D |
| <i>Albizia julibrissin</i> | Silk Tree | D |
| <i>Alnus spp.</i> | Alder | D |
| <i>Arbutus menziesii</i> | Madrone | E |
| <i>Arbutus unedo</i> | Strawberry Tree | E |
| <i>Betula pendula</i> | European White Birch | D |
| <i>Broussonetia papyrifera</i> | Paper Mulberry | D |
| <i>Callistemon citrinus</i> | Lemon Bottlebrush | E |
| <i>Calocedrus decurrens</i> | Incense Cedar | E |
| <i>Casuarina cunninghamiana</i> | River She-Oak | E |
| <i>Catalpa spp.</i> | Common Catalpa | D |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| <u>Botanical Name</u> | <u>Common Name</u> |
|--|-----------------------------|
| • Perimeter Buffer Slope | |
| → <i>Alnus spp.</i> | Alder |
| → <i>Cupressus macrocarpa</i> | Monterey Cypress |
| → <i>Eucalyptus cladocalyx</i> | Sugar Gum |
| → <i>Pinus radiata</i> | Monterey Pine |
| → <i>Quercus agrifolia</i> | Coast Live Oak |
| → <i>Umbellularia californica</i> | California Bay |
| • Neighborhood Show Slope | |
| → <i>Betula pendula</i> | European White Birch |
| → <i>Eucalyptus ficifolia</i> | Red-Flowering Gum |
| → <i>Melaleuca quinquenervia</i> | Cajeput Tree |
| → <i>Pinus halepensis</i> | Aleppo Pine |
| • Community Show Slope/San Pablo Avenue | |
| → <i>Cupressus macrocarpa</i> | Monterey Cypress |
| → <i>Eucalyptus nicholi</i> | Willow-leafed peppermint |
| → <i>Eucalyptus sideroxylon</i> | Red Ironbark |
| → <i>Liquidambar styraciflua</i> | American Sweet Gum |
| → <i>Pinus halepensis</i> | Aleppo Pine |
| → <i>Pinus radiata</i> | Monterey Pine |
| → <i>Platanus acerifolia</i> 'Yarwood' | "Yarwood" London Plane Tree |
| → <i>Platanus racemosa</i> | California Sycamore |
| → <i>Sequoia sempervirens</i> | Coast Redwood |
| • Existing Perimeter Slope | |
| → <i>Arbutus unedo</i> | Strawberry Tree |
| → <i>Rhus lancea</i> | African Sumac |
| → <i>Schinus molle</i> | California Pepper |
| • Naturalized Area | |
| → <i>Cupressus macrocarpa</i> | Monterey Cypress |
| → <i>Arbutus menziesii</i> | Madrone |
| → <i>Juglans hindsii</i> | California Black Walnut |
| • Collector Loop/Enhanced Parkway Streetscape | |
| → <i>Cinnamomum camphora</i> | Camphor Tree |
| → <i>Koelreuteria bipinnata</i> | Chinese Flame Tree |
| → <i>Pinus halepensis</i> | Aleppo Pine |
| → <i>Platanus acerifolia</i> 'Yarwood' | "Yarwood" London Plane Tree |
| → <i>Prunus cerasifera</i> | Cherry Plum |
| → <i>Prunus serrulata</i> 'Kwanzan' | NCN |
| → <i>Quercus palustris</i> | Pin Oak |

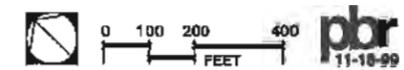


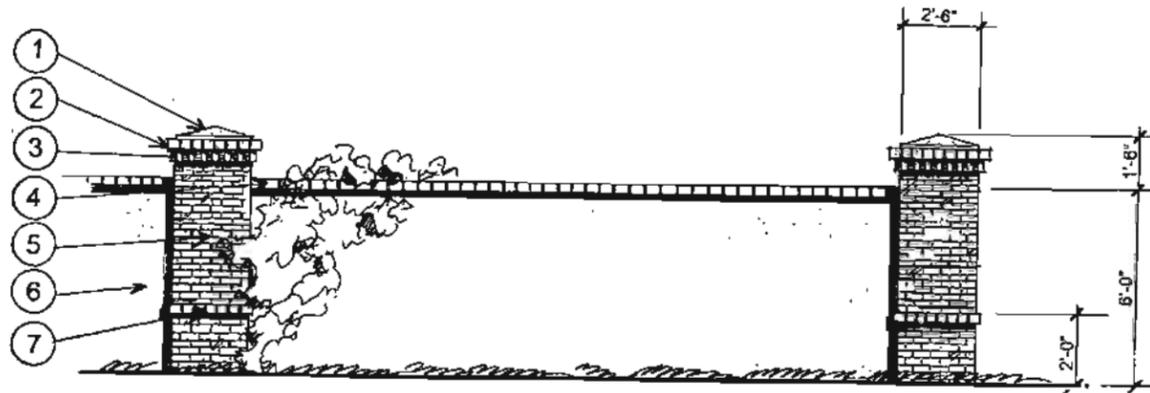
- ① WALL MASTER PLAN IS SUBJECT TO DETAIL PLANS
- ② WOOD PRIVACY FENCING PERMITTED
- ③ WOOD PRIVACY FENCING PERMITTED IF RESIDENTIAL DEVELOPMENT IS PURSUED IN RETAIL/RESIDENTIAL FLEX SITE

LEGEND

| | |
|--|----------------------|
| | SOLID WALL |
| | CHAIN LINK PERMITTED |
| | VIEW FENCE |

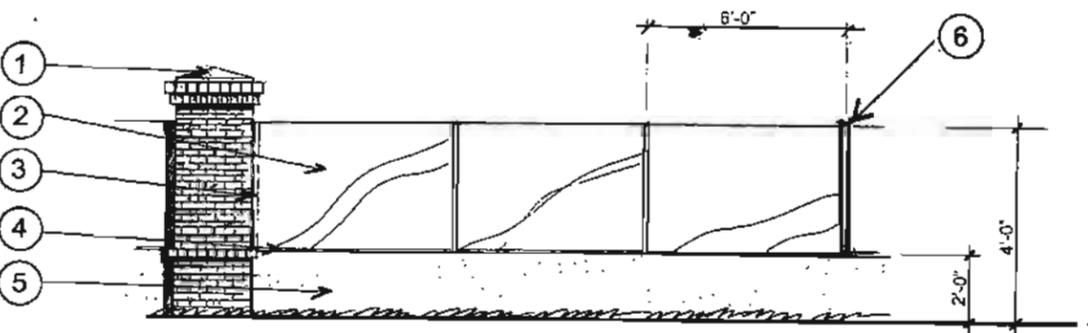
WALL MASTER PLAN
NEW PACIFIC PROPERTIES SPECIFIC PLAN





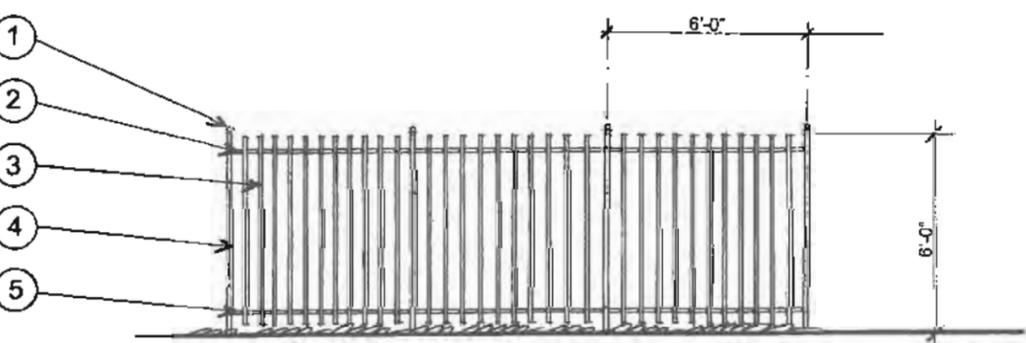
- ① STUCCO CAP
- ② BRICK ON END
- ③ BRICK ON 45 DEGREES
- ④ BRICK CAP
- ⑤ BRICK PILASTER
- ⑥ SLUMP BLOCK WALL, PAINTED SACK FINISH
- ⑦ BRICK ON END

PROJECT THEME WALL



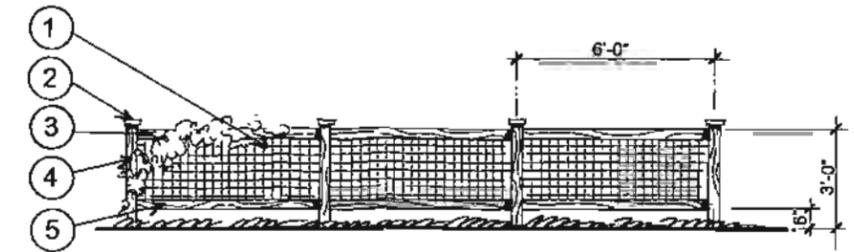
- ① BRICK PILASTER MAY OCCUR
- ② GLASS SCREENS
- ③ ALUMINUM POSTS
- ④ ALUMINUM 'V' CHANNEL
- ⑤ SLUMP BLOCK, PONY WALL SACK FINISH
- ⑥ ALTERNATIVE PILASTER

VIEW FENCE



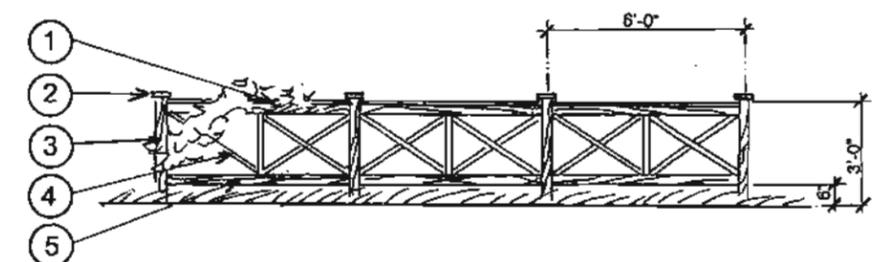
- ① POST CAP
- ② 1" TOP RAIL
- ③ PICKET
- ④ POST
- ⑤ BOTTOM RAIL

VIEW FENCE



*** WOOD RAIL WITH WIRE MESH**

- ① WIRE MESH
- ② WOOD CAP
- ③ 2X6 TOP RAIL
- ④ 4X4 POST
- ⑤ 2X6 BOTTOM RAIL



WOOD RAIL FENCE

- ① 2X6 TOP RAIL
- ② WOOD CAP
- ③ 4X4 POST
- ④ 2X4 RAIL
- ⑤ 2X6 BOTTOM RAIL

(A) WOOD RAIL WITH WIRE MESH MAY BE REPLACED WITH CHAIN LINK FENCE IN CERTAIN DISCREET AREAS ALONG THE SHORELINE TRAIL PARK, SUBJECT TO AGENCY APPROVAL.

(B) STANDARD PRIVACY FENCES TO BE NEIGHBOR FRIENDLY. (WOOD OR MASONRY)

- Community walls and/or view fencing are intended to be located along all uses abutting arterial streets and the Loop Roadway, except along development area P8, the school site and the Retail/Residential Flex site.
- Walls shall be constructed of masonry or other permanent, durable, low maintenance materials.
- Decorative walls should not exceed six feet in height, not including berming. Residential areas abutting the major arterial and collector streets shall avoid long continuous lengths of community wall/sound wall, and should be designed to incorporate the following:
 - Provide breaks in privacy walls to allow pedestrian and bicycle connections to the collector road, where appropriate.
 - Limit wall heights to six-feet to the degree possible.
 - Provide extensive landscaping along walls to visually "soften" or hide the wall, and integrate the wall into the streetscape.
- On the public exposed side, all community walls are intended to be planted with vines.
- When changes in elevation occur, the wall should be stepped in equal, vertical intervals. No step should exceed 12 inches, where feasible.
- The use of walls or fences should be minimized throughout the retail/residential flex and multi-family sites.
- Walls used for screening should be a minimum of six inches higher than the item/area being screened.
- Walls in the retail/residential flex site shall utilize materials and colors compatible with the building architecture.

b. Privacy Fences

- Privacy fences should be used to separate individual lots or between residential parcels.
- Fences are intended to be of durable construction, and shall not exceed six feet in height. See other permitted materials.
- Design of private fences may be consistent in terms of material, color and detail within each residential neighborhood or subdivision.
- No front yard fencing/walls shall be allowed unless it is incorporated in an overall design program or project that ensures architectural integrity and maintenance of the neighborhood.
- All side yard fencing/gates shall be set back three feet (3') minimum from the front building elevations, unless the fencing is integrated in the architectural design element.

c. Open Fences

- View fences are intended to allow views of scenic vistas and open space from private residences, while providing security.
- Fences may include decorative wrought iron, tubular steel, clear glass, plexiglass or similar quality materials and may be used in combination with masonry walls. View fencing may utilize a common design for each neighborhood.

- View fences may include landscaping with trees and shrubs to frame and focus views, and to screen views of private yards from adjacent properties where feasible.
- View fences should be designed to provide security for rear yard pools, or be easily modified for security purposes in the event pools are added after initial construction. Such additions must be compatible with the fence design for the development area or overall community fences as applicable.
- View fences may include a solid or 'open' base of approximately one and one-half feet or more in height, with view fencing above the solid portion.
- All pools and spas shall be fenced with minimum 5-foot high fencing.
- The Natural Open Space area and other sensitive habitat areas should be fenced or otherwise protected according to the standards set forth in the EIR.
- The school site should minimize the use of chain link fencing, and limit such fencing to areas required for the safety of younger children. In general, tubular steel fencing is encouraged. The school site is intended to function as an integral, central element within the project and should be easily accessible to adjacent residents. Where used, chain link should be black vinyl clad fencing.
- Screens, fences and accessory structures should be compatible in material, color and texture to the main buildings.

11. Site Furnishings

Site furnishings are an integral component in establishing a distinct character for neighborhoods and discrete areas of New Pacific Properties. Site furnishings include, but are not limited to bus shelters, benches, drinking fountains, trash receptacles, mailboxes or other elements.

- Site furnishings should be consistent within, but may vary between the following areas:
 - Each residential neighborhood
 - Retail/residential flex, school facility, and multi-family residential sites
 - Parks
 - Enhanced parkway
- All common elements should be constructed of durable, long lasting materials.
- Benches should be permanently secured, and be located near areas of outdoor public use and gathering. In most locations, benches with backs are preferred, though flat benches may be appropriate in some locations.
- Benches shall not obstruct stairs, ramps, building access, trails, walks or other public use areas. Benches should be constructed of metal, weather-resistant wood, concrete, stone or other appropriate material and be compatible with community theme.
- A standard enclosure should be provided for newspaper stands which accommodates multiple newspaper stands and is compatible with other community site furnishings.
- Trash receptacles should be placed along trails and primary community paths, throughout parks, schools, commercial areas and other areas frequented by the public.
- Gang mailboxes for multi-family neighborhoods shall be integrated into the building(s) or clustered, in conformance with Postal Service requirements and compatible with the building architecture.
- Double mailboxes serving two homes or gang mailboxes are suggested to reduce the number of separate elements along the street. Mailboxes should be of a common

decorative design for each neighborhood. Boxes and support structures should be simple and understated.

- Common recycling bins should be provided for all multi-family residential areas in accordance with City requirements.
- Electrical transformers serving one or more homes should be sited and landscaped in a manner compatible with the overall landscape treatment of the area. Pads should be located approximately at the grade of the street, so as not to be visually obtrusive. Low retaining walls may be required on sloped streets, where adjacent lots occur at different grades. Landscaping should be used to provide screening, while still providing access for maintenance.
- Bus shelters should incorporate the architectural design elements of the neighborhood within which it occurs.

E. PRELIMINARY SIGNAGE PROGRAM

Signage and its integration into the overall project is a critical element in the design of any well-planned community. Use of forms, materials, and colors will establish continuity throughout the New Pacific Properties Specific Plan area. All signage shall conform to the landscape and architectural guidelines and concepts established by the Specific Plan. Size and configuration are determined by the function of the sign and according to the hierarchy of information, direction and organization.

Signs are intended to be utilized only where necessary, and in an understated manner, emphasizing an image of permanence and quality. The purpose of permanent signage is to convey information, to aid in identifying neighborhoods or other special areas. The advertising function of signs should be minimized. No sign should have parts of a reflective material. All signs should conform to the City of Hercules Sign Regulations except as modified herein.

1. Materials and Colors

- All materials used in permanent signs and monuments shall be constructed of durable, high quality materials such as wood, materials with a wood appearance, stone, metal, masonry, rock, plaster, plastered masonry, architectural concrete or wrought iron.
- Back-lighted and neon signs are prohibited.
- Acceptable accent and lettering colors should suggest native and natural materials.
- No sign should have parts of a reflective material, except for directional signage.

2. Community Identification Features (along Arterial)

The function of these features and the associated landscape area is to help establish an identity for the entire community and to mark the most important points of entry. *Exhibits 26 and 31-33* show locations of these features.

- The function of these features and the associated landscape area is to help establish an identity for the entire community and to mark the most important points of entry.
- Community identification features may incorporate the project marketing name and/or logo, and include architectural monuments, walls, landscaping and/or art.

- Community identification features located at the Primary Entry should be larger in scale and incorporate more extensive treatments than other community entries.
- Community identification features may be illuminated, utilizing internal or ground-mounted lighting. Internal lighting should be restricted to "halo-lit" or other discrete method.
- Signs should be sized to indicate scale and importance as follows: major streets, minor streets, neighborhood entries, private entry, parks directional, and facility/identification sign.

3. Neighborhood Identification Features (Interior)

The function of these features and the associated landscape area is to help establish an identity for each neighborhood. *Exhibits 26, 34 and 35* show potential locations of these neighborhood entries and features.

- Neighborhood entries may vary in design and character. Each Neighborhood Plan should develop more detailed designs for the neighborhood entries within the Specific Plan area.
- Neighborhood identification features should be discrete and understated. Signs should be incorporated into the overall landscape and architectural treatment for the entry.
- Illumination of neighborhood signs is discouraged. If allowed, lighting may be by ground-mounted up-lights, or "halo-lit" letters.

4. Community Directional Signs

The function of these signs is to help orient and identify major community facilities and neighborhoods within New Pacific Properties.

- Community Directional Signs may be located along San Pablo Avenue and other primary entrances to aid in providing directional information.
- Community Directional Signs may utilize reflective, or other highly visible lettering, and may be illuminated.
- Community Directional Signs shall utilize the same materials as used in Community Identification Features.
- In addition to any Community Directional Signs, primary trails may include durable signage at points of intersection with streets.

5. Street Signs

- Street and traffic control signs shall be consistent with the standards adopted by the City of Hercules.

6. Commercial Signs

The function of these signs is to identify the sites of commercial activity within the community. The following standards shall apply to all commercial signage except where otherwise indicated in subsequent sections of this Chapter.

- Billboards are prohibited within the New Pacific Properties Specific Plan area.
- Aside from one primary commercial complex identification sign, only building wall or ground mounted identification signs are allowed.
- Commercial Identification signs shall incorporate materials which reinforce the architectural character or theme of the area.
- Commercial Identification signs typically serve multi-tenant establishments, and should not identify individual tenants other than anchor or sub-anchor tenants. Such identification shall be understated and secondary to the purpose of the sign.
- Individual letters or characters on wall mounted signs may not exceed two (2) feet in height, except by variance.
- Up to two monument signs (One sign per street frontage) are allowed per multi-tenant commercial site.
- Freestanding Commercial Identification signs shall be integrated into the landscape treatment of the roadway and landscape setback.
- Ground-mounted and internal illumination of Freestanding Commercial Identification is permitted. Internal illumination, if used, may consist of an opaque background with transparent type, or "halo-lit" letters.
- No outdoor signs or other contrivance shall be devised, constructed or placed so as to rotate, gyrate, blink, move in any animated fashion or give the impression of movement.
- Painted sign boards of any type are prohibited with the exception of temporary signs.
- Identification signs shall be fixture signs and consist only of the individual letters and/or pictorial symbol denoting the name and the site or the person, firm, company, corporation or major tenant, operating the use conducted on the site.
- Permanent ground identification signs may be located within the building setback area, provided that no portion of any sign is located within the first five (5) feet as measured from the property line.
- Sign area shall be measured by a rectangle around the outside of the lettering and/or the pictorial symbol together with any frame forming an integral part of any ground sign and calculating the area enclosed by such lines.
- One directory sign listing only the names of on-site firms or businesses will be allowed per street frontage. Said sign shall be located adjacent to the entrance driveway or within the parking area of the lot. Such sign may be installed in addition to a building wall sign, but not in addition to another ground sign along the same street frontage.
- The sign in any multi-tenancy building must be uniform in style and location and must be oriented towards the parking area for that building. They will be permitted in addition to the multi-tenant directory ground sign.
- Signs giving direction to traffic entering or exiting, or to give special instructions on the premises may be used in addition to the other signs specified herein.
- Entrance or exit signs shall not exceed thirty (30) inches in height and one (1) square foot in area.

a. Highway Oriented Sign

- One highway-oriented sign located on the eastern boundary of the project adjacent to the I-80 is permitted. The highway-oriented sign shall not exceed 40 feet in height, and be designed and located so as to not impact residential views from the project.

b. Commercial Center Identification Sign

- One freestanding Commercial Identification sign located on San Pablo Avenue is permitted. Size shall not exceed the following maximum dimensions:
 - Height: 20 feet.
 - Overall Area: 160 square feet.

c. Freestanding Commercial Monument Signs

The function of these signs is to identify single users of commercial or other non-residential parcels, or special uses within a multi-tenant parcel.

- Freestanding Commercial Multi-tenant Identification signs shall not exceed the following maximum dimensions:
 - Height: 8 feet with 20% area allocated to Center Identification
 - Overall Area: 64 square feet.
- Freestanding Commercial Single User Identification signs shall be smaller in size than multi-tenant signs and shall not exceed the following maximum dimensions:
 - Height: 5 feet
 - Overall Area: 30 square feet.
- Freestanding Commercial Tenant Identification signs shall be ground-mounted, and located either in the landscape setback along the roadway or in the landscape median of the driveway or street serving the site.
- One Freestanding Commercial Tenant Identification sign shall be allowed per parcel.
- Freestanding Commercial Tenant Identification signs shall utilize the same materials as Commercial Identification Signs or Neighborhood Identification Features, as appropriate.
- Freestanding Commercial Tenant Identification signs shall be integrated into the landscape treatment of the roadway and landscape setback.
- Internal illumination, if used, may consist of an opaque background with transparent type, or "halo-lit" letters.

d. Commercial Tenant Building Mounted Signs

The function of these signs is to display the tenant's name on the building fascia to identify the business' location to on-site traffic.

- Commercial Tenant Building Mounted signs shall be easily legible from pedestrian and parking areas, and generally located on the fascia or wall, above or near the tenant's entrance.
- Roof mounted Commercial Tenant Building signs are prohibited.
- Within multi-tenant commercial developments, the tenant identification signs shall be consistent in terms of scale, placement, typeface, color, mounting details and illumination. Details should be standard throughout the retail/flex site unless individual tenants have a previously defined corporate logotype, logo or color.
- Fascia mounted signs may not project beyond the building roof or fascia line.

- Each firm or business in a multi-tenancy building(s) may have one wall sign over the entrance to its premises to identify itself, except for outlying pad buildings which are allowed up to two. The individual tenant's sign shall state only the name of the company and shall be limited to six (6) inch high letters.
- If illuminated, signs shall be internally lit, with "halo-lit" letters. Sign cans, mounted to the fascia, are prohibited.
- Only one wall sign shall be permitted along each street frontage of a lot.
- Individual business signs shall not be painted directly onto the face of the building, but shall be designed as a separate element and part of the structure utilizing the architectural elements appropriate to the building style.
- Wall mounted identification sign letters must be affixed to the face of the principal building in proper proportion and relationship to the building size and design.
- Individual letters or characters on wall mounted signs may not exceed two (2) feet in height.
- Wall mounted signs shall not comprise more than five percent (5%) of the area of the elevation upon which the sign is located.

7. Temporary Signs

Temporary signs serve an important function during the construction and marketing of the community. Temporary signs identify and direct visitors to current projects, model complexes, future development areas and amenities.

- Temporary signs shall be consistent in theme and type throughout the New Pacific Properties Specific Plan area.
- Temporary signs shall be permitted for a period up to six (6) months, or until property is sold, rented, or construction is finished.
- The general appearance of temporary signs should be maintained while in use.
- Temporary signs shall be removed promptly when they are no longer needed.
- Flags or banners relating to the initial sale of land or housing units shall be permitted on a temporary basis until the project is sold out.

a. Community and Project Marketing Information Sign

The function of these signs is to identify the New Pacific Properties project and to display the community name and marketing information about the entire community (e.g., amenities and special features). These signs also identify individual development projects, display parcel name, marketing information, and construction information, and they direct on-site sales traffic to active sales and leasing areas. Community Marketing Information signs may be located throughout the community, and may be illuminated.

b. Future Development Parcel Identification Sign

The function of these signs is to identify future development within the community, including development parcels, recreation areas, parks, schools, roadways or other amenities.

- Future Development Parcel Identification signs shall be consistent in theme and type with the temporary sign program.

VI. IMPLEMENTATION PROGRAM

A. PHASING PROGRAM

Implementation of the New Pacific Properties Specific Plan will require improvements to traffic/circulation, drainage, utility infrastructure including water and sewer mitigation, signage, and parks/open space. Water, sewer, drainage and other utility improvements will be constructed to adequately meet the development needs identified within each phase. Road improvements associated with the Enhanced Parkway loop roadway and adjoining collector street will be built in concurrence with the demand impacts identified for each phase. This program will ensure that the project's circulation system is steadily provided as adjacent development occurs over an extended period of time. This phasing plan is not intended to dictate time sequential development. Concurrent development within each of the phases may occur depending upon site availability, construction logistics, and market conditions.

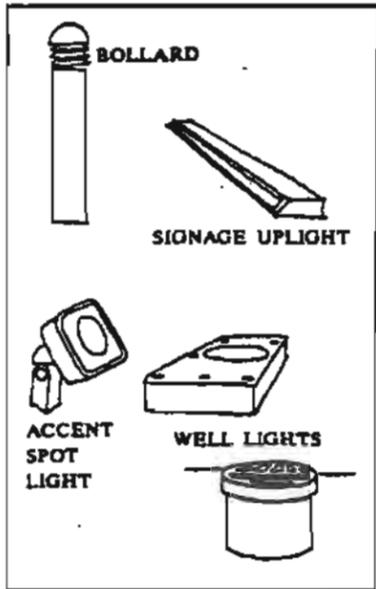
Numerous activities will take place to redevelop the New Pacific Properties Specific Plan area. The Pacific Refinery facility is currently in the process of being dismantled and removed. Environmental clean up of the site is also being conducted simultaneously. It is anticipated that the inland portion of the site will not require environmental clean-up since it did not contain structures or uses that held potential for contamination from hazardous materials. Any archeological data recovery program, including mitigation of potential impacts, will factor into the phasing of the project. Grading will occur in three levels to prepare the site for development, as described by the phasing program below.

The phasing program provides for the subdivision of project development into three general areas to be completed over the next 3-15± year period as illustrated in *Exhibit 51, Preliminary Phasing Plan*. Nearing the completion of each phase, landscape treatment and signage improvements will be implemented as indicated in subsequent sections. Because of the historical use of this area, the inland phase is likely to be developed first.

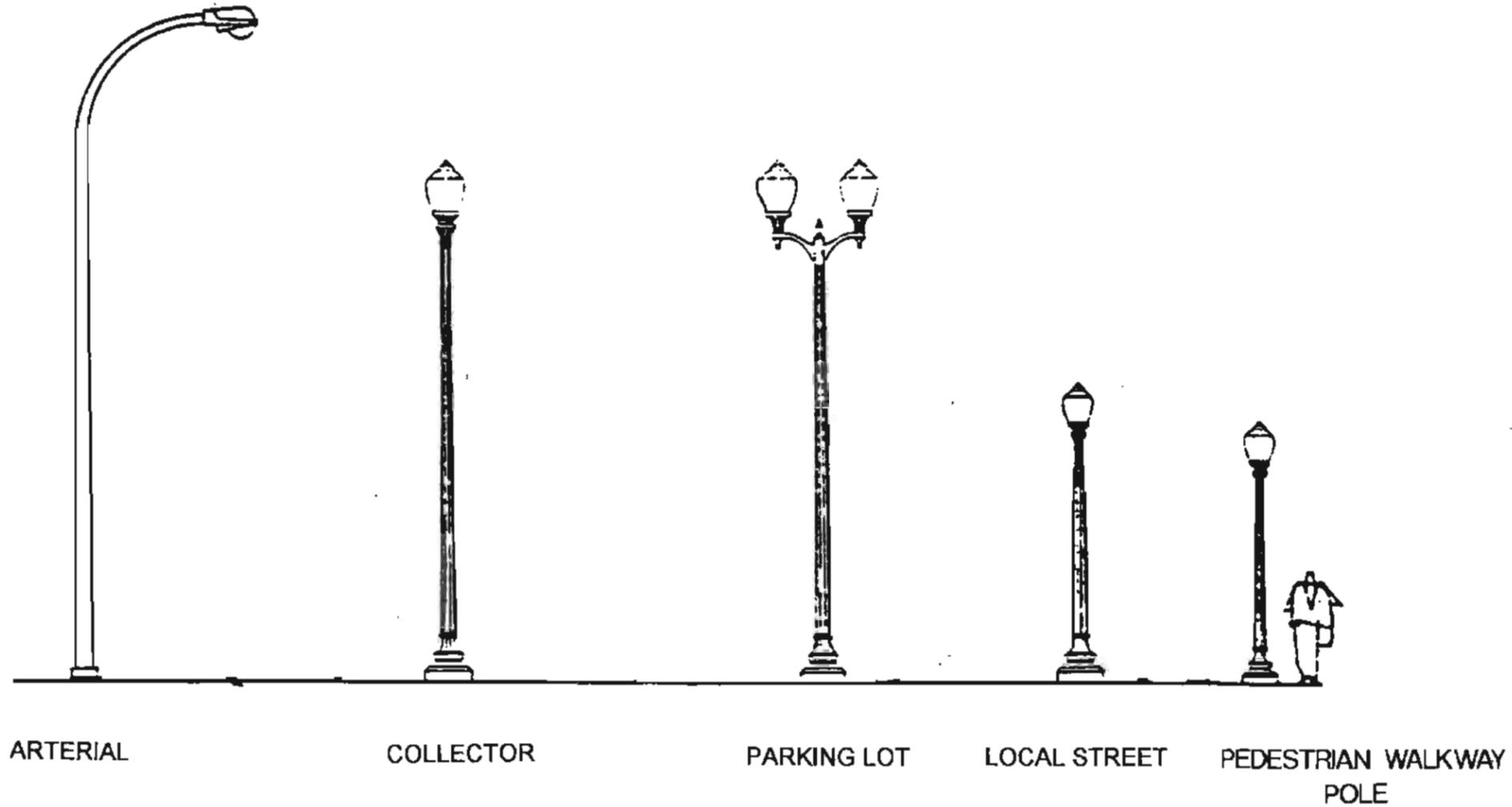
1. Phase One

Phase One consists of development of the inland section of the New Pacific Properties site. Because of the historical use of the coastal area, the inland phase is likely to be developed first. This section of the property, bordered on the west by San Pablo Avenue, will include sections of P5 and P7, P6, the School Facility site, and the Retail/Residential Flex site. The P5 area will contain single-family homes. The P7 development area abuts a sloped natural open space area, and will be carefully designed to support smaller pocket lots at a medium density, while maximizing the attributes of single-family residential zoning. P6 will utilize the unique Two-Pack Z-Lot design to compact lot sizes while more efficiently distributing yard space. The school site is made up of a playing field/park and an elementary school. The remaining 6.0-acre Retail/Residential flex site will be developed to accommodate commercial or residential uses.

Circulation improvements within this area include the southeastern section of the Enhanced Parkway collector loop roadway, minor and cul-de-sac roadways. These streets will be connected with a round-about at the point where the boundaries of the school site, P5 and P6



PEDESTRIAN/ ACCENT LIGHTING



NOTE:

SPECIFIC MANUFACTURER DESIGN TO BE SPECIFIED

DESIGN IS CONCEPTUAL IN NATURE AND MAY INCLUDE SOME OF THE ELEMENTS DEPICTED OR OTHERS NOT SHOWN.

b. Parking Lot Lighting

- Glare should not be visible from beyond the property line of development where feasible.
- To the degree feasible, parking lots may utilize a contrasting light fixture and lamp color to distinguish the parking area from adjacent streets.
- Fixture should be no taller than necessary to provide efficient lighting of the area.
- Fixture styles and colors should be compatible with the architectural elements of the site.

c. Pedestrian and Entry Area Lighting

- High use pedestrian areas, entrances to public facilities, bus or transit stops and other public areas should be illuminated with pedestrian scale lights.
- Warm light colors should be used for these areas.
- Light fixtures may be a combination of pole lighting, bollard lights or other, appropriately scaled fixtures.

d. Recreation Area and Athletic Facility Lighting

- No night lighting, except for security is permitted at the school's playing field.
- Park lighting should be kept to a minimum in the open space areas adjacent to the Natural Open Space area.

e. Landscape Lighting

- Light fixtures should be hidden from direct view unless the design intent calls for otherwise.
- Fixtures should be durable and easily maintained. For ease and efficiency of maintenance, all landscape light fixtures should be consistent throughout the community.

f. Service Areas and Security Lighting

- Night lighting should be installed which provides both security and an aesthetic draw for strolling.
- Lights in these areas shall avoid spillover onto adjacent use areas, and should shield the direct view of the light source, where feasible.

g. Sign Lighting

- In general, sign lighting should be understated and not call undue attention to the sign features.
- Ground mounted fixtures should be durable and integrated into the sign base or adjacent landscaping. Fixtures should be unobtrusive, and the light sources hidden from direct view, where feasible.

- Internally illuminated signs should utilize a dark, opaque background, with light colored type or logo. Additionally, halo lighting of letters or logos may be utilized.

10. Walls and Fences

Walls and fences will be an important component in the environment of New Pacific Properties Specific Plan area. They will provide privacy and security, shape important public spaces and help establish the character of the community. General guidelines are provided below, more specific wall type guidelines follow.

- Decorative walls and fences shall be limited to a maximum height of 72" (6 feet), except as noted below. Decorative walls may be used in conjunction with retaining walls.
- Retaining walls three feet (3') or greater in height shall be designed by an Engineer and approved by the City.
- Wood, wood looking, stone, stone veneer, stucco (including stucco covered block), masonry, brick, slump block, wrought iron, tubular steel, glass and clear plexiglass materials are permitted.
- Vinyl, cement other synthetic fencing/wall materials may be permitted provided it is designed and textured to appear as natural wood or stone.
- The following wall and fence materials and styles are prohibited:
 - Chain link fences or gates*
 - Barbed wire, chicken wire, or other wire fencing
 - Metal (aside from wrought iron or tubular steel), galvanized steel, corrugated metal, or aluminum fencing.
 - Unplastered, unfinished, unfaced standard precision block, including but not limited to tile block, C.M.U. or other similar materials.
 - Plywood or other flat surfaces which do not have relief.
 - Electrically charged fences.
- * Chain link fences shall be permitted within the School site, and within the Shoreline Trail Park.
- Walls or fences along San Pablo Avenue are intended to be uniform in terms of architectural material, texture and color.
- Jogging of the fence line or use of pilasters is encouraged to avoid monotony.
- View fences constructed of materials such as wrought iron, tubular steel, glass or plexiglass are encouraged along areas which abut or open to parks, open spaces and views. Consistency in design is encouraged within each neighborhood.

a. Community Walls

Community walls include all walls which adjoin major public streets, parks, public use area and community entrances (see *Exhibit 49, Wall Master Plan*, and *Exhibit 50, Typical Wall and Fence Designs*). In general, walls within the community should not become the dominant visual element, and care should be taken to blend the walls into the overall landscape. Community walls are intended to provide protection from roadway and other noise, privacy and security.

converge. Also included as part of this phase would be intersection improvements at San Pablo Avenue, comprised of storage lanes and signalization of the Primary intersection. The timing and level of signalization required will be determined by the City's traffic consultant and Engineer. Secondary access to San Pablo Avenue is also provided at P7. However, to minimize traffic impacts, vehicular movements are limited to right turn in and right turn out only. All appropriate resource agency permits will be attained prior to any development activity.

2. Phase Two

Phase Two falls within the Coastal portion of the property containing both sections of P1, P2, P4, two western sections of P3 and several park/open space public improvements. The residential zones will be developed with single-family housing on lots varying in size from 4,000 to 6,000 square feet. Park and open space improvements include the Neighborhood Park, Linear Park, Community Trail Park, Shoreline Trail Park, and enhancement of natural open space areas.

Roadway improvements associated with Phase Two include the Enhanced Parkway loop collector roadway, minor, one-way minor and cul-de-sac streets feeding from the Enhanced Parkway loop roadway and any remaining improvements to the Loop Roadway intersection at San Pablo Avenue. The progress of dismantling and remediation of the area and market conditions will mandate the development schedule of this phase. All appropriate resource agency permits will be attained prior to any development activity.

3. Phase Three

Phase Three is comprised of planning area P8 and a portion of planning area P3 as shown on *Exhibit 51*. These two sites will be graded together, but separately from the rest of the Coastal section of the property. P8 is anticipated to be graded to a super pad condition. Multi-family or single-family development is allowed. The preliminary Land Use Concept anticipates that P8 will realize multi-family units (townhouses, condominiums or apartments) at a density of 12-30 units per acre, however, should the market indicate that the need for another product type is stronger than the P8 multi-family residential product, the Specific Plan allows for other product types at a lower density to be designed in the P8 area, provided that the overall density for the entire New Pacific Properties Specific Plan area is not exceeded. P3 will consist of single-family residential development. The construction of minor collector and cul-de-sac streets within these two sites is also a part of this phase. Grading and development of Phase Three may follow or occur concurrently with development of Phase Two, depending on site availability and market conditions. All appropriate resource agency permits will be attained prior to any development activity.

B. DEVELOPMENT REVIEW PROCESS

Future development within the New Pacific Properties Specific Plan area will require individual development permits, including permits for the subdivision, construction, reconstruction, conversion, structural alteration, relocation, or enlargement of any structure or establishment of a

new use. Applications for these and other activities are subject to review by the City for consistency with the design guidelines and development standards in this Specific Plan.

Development within the Specific Plan area shall be in accordance with the criteria contained in the New Pacific Properties Specific Plan and related documents, including other City land use and zoning ordinances. In instances where this Specific Plan and City-wide development standards conflict, the guidelines set forth in this Specific Plan shall take precedence.

C. SUBSEQUENT DEVELOPMENT ENTITLEMENTS

Individual project approval requests and development applications for proposed development located within the New Pacific Properties Specific Plan area shall file an application with the Community Development Director for review and processing as outlined below.

1. Tentative and Final Subdivision Maps, Parcel Maps & Lot Line Adjustments

Applications for tentative and final subdivision maps, parcel maps, and lot line adjustments are to be filed with the Community Development Director for processing as outlined in Section 40.300 of the City of Hercules Municipal Code. The Community Development Director shall review the application for completeness, and either accept the application as final or mail a written notification of application incompleteness to the applicant within 30 days of submittal. Once an application has been deemed complete, it shall be reviewed and processed according to the provisions of California Government Code Section 65090 et seq.

2. Design Review

Prior to building permit issuance, conceptual architectural and landscape architecture plans shall be approved by the Planning Commission for each residential development area.

3. Use Permits

Use permits include Administrative, Conditional and Temporary use permits, all of which may be revocable or granted for a limited time period per conditions. After determining that an application for a use permit is complete, the Community Development Director shall review and take action on applications for Administrative use permits, and for Temporary use permits that have a duration of less than 30 days. Applications for Conditional use permits and Temporary use permits for a period of more than 30 days shall be forwarded to the Planning Commission with the Director's recommendation. Use Permits may be conditionally approved with respect to location, siting, construction, maintenance, operation, duration, and overall development as applicable to the protection of adjacent properties and the public interest. The Community Development Director and Planning Commission shall either approve, approve with conditions or deny applications for use permits.

4. Variances

An application for a Variance will be filed with the Community Development Director for review. Complete applications shall be forwarded to the Planning Commission with the Director's recommendation. The Planning Commission may grant a variance if these findings are made: 1) strict interpretation and enforcement of Zoning Ordinances would result in practical difficulty or unnecessary hardship inconsistent with the purposes of the General Plan, Zoning Ordinance or intent of this Specific Plan; 2) the property or the intended use of the property is subject to unique circumstances or conditions including size, shape, topography, location or is deprived of privileges enjoyed by other properties in the vicinity and in the same zoning district by the literal interpretation of Zoning Ordinances; 3) special privileges inconsistent with the limitations on other properties in the vicinity and in the same zoning district will not be granted; and 4) public health, safety or welfare and properties or improvements in the vicinity will not be negatively impacted.

5. Dwelling Unit Transfers

The Land Use Concept Plan of the New Pacific Properties Specific Plan is the product of current available predictions of market needs and development constraints over the lifetime of the project. As economic conditions evolve and development plans are refined, minor revisions and improvements to the Land Use Concept and Lotting Study statistical summary will be appropriate.

A transfer of residential units or zoning districts between residential land use categories subject to review and approval by the Community Development Director is permitted provided the following criteria are met:

- The maximum number of allowable dwelling units within the Specific Plan area is 880. This maximum shall not be exceeded.
- An overall variety of residential unit densities is provided. The project may not be developed under one residential unit density or product type.
- If the number of dwelling units assigned to a specific land use category is not achieved, the unused dwelling units may be transferred to another land use category/zone within the Specific Plan area.
- Transfers of multi-family units to land use categories currently designated for single-family use shall be allowed if it can be demonstrated that said transfer will not detrimentally impact existing residential areas outside of the Specific Plan area. For example, transfer of the P8 unit density to the P5 land use category location would be acceptable since that site abuts the I-80 freeway and a fuel storage facility, and would not detrimentally impact the existing and proposed abutting residential land uses. Specific design characteristics such as appropriate landscaping, setbacks, topographic conditions, etc. will be taken into consideration.
- The overall design, visual quality and integrity of Specific Plan community framework elements such as parks, open space, trails, etc. are not adversely affected.
- A Unit Allocation Table shall be submitted with each request for transfer of dwelling units indicating all land use categories/zoning districts, units allowed, units approved by

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tentative tract map, building permits granted and transfers in and out of each land use category.

- Additional or fewer units may be developed on the site so long as the maximum allowable 880 dwelling units is not exceeded.

D. SPECIFIC PLAN AMENDMENT PROCEDURES

As stated in Section 49.600 of the City of Hercules Municipal Code (October 1998), a "specific plan shall be amended in the same manner as a general plan, except that a specific plan amendment may be adopted by resolution or by ordinance consistent with the manner of adoption of the specific plan and may be amended as often as deemed necessary by the legislative body".

Specific Plan amendments shall be consistent with the General Plan. Once the proposed amendment has been submitted to the Planning Department, City Staff shall make a determination on the completeness of the amendment package, then issue public notice of the upcoming public hearings. In addition, City Staff will prepare a staff report with recommendation on the amendment and forward it to the Planning commission for review. The Planning Commission, in turn, will forward the amendment with their recommendation(s) to the City Council for a final decision. The City Council may elect to: 1) approve the amendment unconditionally; 2) approve the amendment with conditions; 3) deny the amendment; or 4) send the amendment back to City Staff for further refinement and/or modification.

E. FINANCING PLAN

As specified by Government Code Section 65451, *Table 5, Financing and Maintenance Plan* provided below identifies the financial responsibilities for the provision of the many public improvements proposed for the New Pacific Properties Specific Plan. The Financing and Maintenance Plan indicates the parties responsible for construction, financing and maintaining the public improvements proposed by the Specific Plan. The financing plan is intended to ensure the long-term maintenance of the many improvements within the Specific Plan. A variety of financing mechanisms including use of RDA funding/tax increment, Lighting and Landscaping Districts, Home Owners Associations, Developer fees, etc. will be utilized. The Specific Plan will be complemented by a Development Agreement between the Developer/Merchant Builders and the City of Hercules. Additionally, a detailed fiscal analysis for the proposed development will include a costs/revenue evaluation.

| TABLE 5 FINANCING AND MAINTENANCE PLAN | | | |
|---|-----------------------------------|-----------------------------------|--|
| Service or Facility | Party(ies) Executing Construction | Party(ies) Financing Construction | Party(ies) Responsible for Operation and |
| CIRCULATION AND TRANSPORTATION | | | |
| On-Site Streets/Roads | Developer Merchant Builders | Developer Merchant Builders | City of Hercules |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| TABLE 5 FINANCING AND MAINTENANCE PLAN | | | |
|--|--|--|---|
| Service or Facility | Party(ies) Executing Construction | Party(ies) Financing Construction | Party(ies) Responsible for Operation and |
| San Pablo Avenue: full ROW improvements and supporting grading | Developer City of Hercules | RDA Funding | City of Hercules |
| Off-Site Street/Intersection Improvements | City of Hercules | RDA Funding | City of Hercules |
| Regional Transportation Improvements (Outside City of Hercules, if applicable) | Designated Public Agency | Designated Public Agency Developer/Merchant Builders fee payment | Designated Public Agency |
| PARKS, TRAILS, OPEN SPACE, PARKWAYS | | | |
| Enhanced Parkway: Landscape/Lighting Loop Road | Developer | Land: Developer RDA Funding | City of Hercules, Lighting and Landscaping District Annexation |
| Community Trail Park | Developer Merchant Builders | Developer Merchant Builders, RDA | Home Owners Association |
| Private Open Space | Developer Merchant Builders | Developer Merchant Builders | Home Owners Association |
| Linear Park | Developer | Land: Developer RDA Funding | City of Hercules, Lighting and Landscaping District Annexation |
| Neighborhood Park | Developer | Land: Developer RDA Funding | City of Hercules, Lighting and Landscaping District Annexation |
| Shoreline Trail Park | Developer | Land: Developer RDA Funding | City of Hercules, Lighting and Landscaping District Annexation |
| Shoreline Trail/Park: Trail Basement | Developer | Land: Developer RDA Funding | East Bay Regional Parks District |
| Enhanced Project Entries: Landscaping and Monumentation | Developer | Land: Developer Developer Funding | Home Owners Association |
| Natural Open Space | Developer Merchant Builders | Land: Developer, Merchant Builders Developer, Merchant Builder Funding | City of Hercules, Lighting and Landscaping District Annexation |
| Wetlands | Developer Merchant Builders | Land: Developer, Merchant Builders Developer, Merchant Builder Funding, RDA | City of Hercules, Lighting and Landscaping District Annexation |

NEW PACIFIC PROPERTIES SPECIFIC PLAN

| TABLE 5 FINANCING AND MAINTENANCE PLAN | | | |
|---|---|--|--|
| Service or Facility | Party(ies) Executing Construction | Party(ies) Financing Construction | Party(ies) Responsible for Operation and |
| SCHOOL FACILITY | | | |
| Elementary School | West Contra Costa County Unified School District | RDA Funding, City of Hercules, Developer fee payment (subject to SB 50 requirements) | Facilities: WCCUSD Playfield: Agreement between WCCUSD and City of Hercules |
| UTILITIES | | | |
| On-Site Public Water Facilities | Developer Merchant Builders | Land: Developer, Merchant Builders Developer, Merchant Builder Funding, RDA | East Bay Municipal Utilities District |
| Aqueduct Relocation | Developer East Bay Municipal Utilities District | Land: Developer RDA Funding | East Bay Municipal Utilities District |
| Off-Site Public Water Facilities | East Bay Municipal Utilities District | Developer Contribution | East Bay Municipal Utilities District |
| On-Site Street Lighting | Developer Merchant Builders | Developer Merchant Builders, RDA | Lighting & Landscape District Annexation |
| On-Site Wastewater Collection System | Developer Merchant Builders | Developer Merchant Builders, RDA | City of Hercules, Utility Operator |
| On-Site Wastewater Pumping, Transmission Facilities | Developer | RDA Funding | City of Hercules, Utility Operator |
| Off-Site Wastewater Pumping, Transmission, Treatment Facilities | Governing Entity | Governing Entity RDA Funding | City of Hercules, Utility Operator |
| On-Site Wastewater Treatment Facilities (if applicable) | Developer | Developer Utility Operator, RDA | HOA, Utility Operator |
| On-Site Storm Drainage Facilities | Developer Merchant Builders | Developer Merchant Builders, RDA | East Bay Municipal Utilities District |
| Drainage at Bay, and specialty catch basins (if applicable) | Developer | RDA Funding | East Bay Municipal Utilities District |
| Electrical, Gas, Telephone | Developer | Utility Operator | Utility Operator |
| Cable TV | Developer | Utility Operator | Utility Operator |

VII. RELATIONSHIP TO THE HERCULES GENERAL PLAN

Land use development and resource conservation policies in Hercules are generally regulated by the City of Hercules General Plan. The City was incorporated in 1900 as a company town to allow the California Powder Works company to control the land around the plant facilities. The end of dynamite production left large parcels of surrounding land, which had originally served as a buffer zone, no longer needed. The City's first General Plan was adopted in the early 1960s. In 1993 a Community Panel studied the City's existing land use concept and recommended that a new one be incorporated into the revised Land Use Element of the General Plan, which was adopted on February 13, 1996. The New Pacific Properties Specific Plan is consistent with and further implements the Hercules General Plan. The New Pacific Properties Specific Plan refines the City's General Plan towards creating a vision for a new residential and possibly commercial community. This section lists relevant goals and policies of the City of Hercules General Plan and demonstrates how the New Pacific Properties Specific Plan is consistent with the Hercules General Plan.

The New Pacific Properties Specific Plan represents a vision for the redevelopment of a former oil refinery into a new residential and possibly commercial community which is expected to support over 2,700 people. The conversion of the previous industrial use is an opportunity to eliminate blight and the land use conflicts which existed between the nearby residential uses and the former refinery, but also to provide new housing opportunities. The Specific Plan policies provide for the development of a new community which not only responds to market demand, but also helps the City meet its fair share affordable housing obligations. The provision of an extensive system of parks, open space and a school facility ensures that needed public facilities will be available for future residents.

A. LAND USE

As stated previously in this Specific Plan, the City's current General Plan Land Use designation for the project site is "Industrial" west of San Pablo Avenue, and "Planned Commercial Industrial" east of San Pablo Avenue. A General Plan Amendment to "Specific Plan" will be initiated which would allow implementation of the New Pacific Properties Specific Plan. The property is zoned "Industrial" by the City. The New Pacific Properties project proposes that the designation be changed to "Specific Plan" and include Residential, Commercial/Residential, School and Open Space/Park zones. The City also has incorporated this property into a new multi-site Redevelopment Area.

The proposed General Plan land use designation would serve to meet the needs of the subregion and respond to the goals and objectives of the community.

Objective 1 **Achieve a level of population and employment which preserves and enhances the desired character of the community.**

Policy 1.1 *Encourage and only allow development that is consistent with the Land Use Diagram, Land Use Categories; and objectives, policies and programs of the Land Use Element.*

Project Consistency: A General Plan Amendment is proposing to change the current "Industrial" and "Planned Commercial Industrial" land use designations to "Specific Plan". The planning areas dictated in the Specific Plan include P1 Residential, P2 Residential, P3 Residential, P4 Residential, P5 Residential, P6 Residential, P7 Residential, P8 Residential, Retail/Residential Flex, School, Parks, and Open Space. Corresponding zoning will be established, including Residential Low Density (SP-R-L), Residential Low Medium Density (SP-R-LM), Residential Medium Density (SP-R-M), Residential Medium High Density (SP-R-MH), Residential Z-Lot Line (SP-R-Z), Residential Pocket Lots (SP-R-PL), Residential Multi-Family (SP-R-MF), Retail/Residential Flex (SP-R/RF), School (SP-S), Open Space/Parks (SP-OS). Utilizing high standards in architectural and landscape design, the New Pacific Properties development will enhance the character of the overall community, provide needed housing stock, and support Hercules' desired quality of life.

Objective 2 Develop a community that balances housing, jobs, and commercial opportunities.

Policy 2.1 Commercial and industrial development shall be consistent with gross intensity ranges in the Land Use diagram and Land Use Categories. Higher intensity may be considered if such development is consistent with the City's goals and policies. However, each project with a proposed higher density would be subject to site-specific environmental analysis to determine incremental impacts.

Policy 2.2 Develop non-residential Land Use Categories which reduce the need for residents to leave the community by providing a variety of shopping and service opportunities.

Policy 2.3 Emphasize employment-generating development, which is lagging behind residential development.

Project Consistency: The project allows for the opportunity to add retail as well as residential uses to the community of Hercules, encouraging the maintenance of balancing housing, jobs and commercial opportunities. The retail/residential flex planning area is designed to allow for employment generating uses as well as retail sales and service opportunities, provided market constraints do not impede successful commercial development opportunities for this site and other competitive areas within the City. The quality residential development envisioned for the overall Specific Plan area will enhance the existing business community in the City by attracting new residents who will patronize retail and business establishments. Tax increment funding generated by residential development as envisioned in the Redevelopment Plan will assist in promoting local economic development and job growth. An elementary school can generate approximately 40 full time employees and support additional local service employment positions.

Objective 3 Ensure the provision of public facilities and services needed to support growth that balances jobs, commercial, and housing opportunities, and also protects the quality of life in the community.

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- Policy 3.1 Develop transportation facilities to provide access to the region, particularly public transit systems (buses, ride sharing, rail transit, as well as potential over-water transit).*
- Policy 3.2 Land Use Element and Circulation Element need to be closely coordinated to insure that traffic from new development will not overwhelm the carrying capacity of the circulation system.*
- Policy 3.3 Reasonable traffic flow and direct access between neighborhoods should be provided or preserved, where feasible.*
- Policy 3.4 Create a strong and successful focus or center for business and activities that would provide services, shopping opportunities which would attract employees, clients, and patrons from a regional area, while not disturbing existing residential and community oriented areas.*

Project Consistency: The proposed development is in accordance with the various objectives of the Land Use Element and Circulation Element. A preliminary traffic analysis shows that the New Pacific Properties project is not anticipated to generate more traffic than exists for historical industrial land uses and is significantly less than would be generated under the current industrial General Plan and Zoning Designations. Public transportation is available through the Bay Area Rapid Transit (BART) Express, which runs to Linus Pauling Avenue less than a mile south of the site, as well as the Western Contra Costa County Transit Authority (WestCAT), WestCAT Dial A Ride Service, and the Alameda-Contra Costa Transit (AC Transit). The AC Transit line runs directly along San Pablo Avenue. Improvements will be made to the circulation system, including San Pablo Avenue, to assure that traffic from the New Pacific Properties project will not exceed the carrying capacity of the roadways. A street and pedestrian circulation scheme connecting the Specific Plan planning areas and San Pablo Avenue will allow smooth access throughout the site. The retail/residential flex area provides the opportunity to serve as a focus for business and activities, providing retail and service stores to benefit the immediate community. Tax increment funding not otherwise available can be used to enhance other focus areas more centrally located to the City core.

Objective 4 Develop sufficient employment and commercial tax generating uses to maintain a positive City government fiscal condition.

- Policy 4.1 Encourage local and regional commercial uses that can benefit from substantial regional traffic on I80, Highway 4 freeway, and San Pablo Avenue.*
- Policy 4.2 Encourage uses that bring additional revenues (retail sales, property tax) either directly or indirectly to the City.*

Project Consistency: The project will contribute property tax and retail sales directly to the City with up to 880 units of residential development and up to 6.0 acres of retail/residential flex development. Existing commercial development viability will be enhanced by increasing local quality housing stock.

Objective 5 **Develop and maintain a pattern of residential land uses which provide for a variety and balance of densities and opportunities for a mix of dwelling and residential type.**

Policy 5.1 *Residential development shall be consistent with gross density ranges in the Land Use Categories and with the Land Use Diagram.*

While a goal of the City is to limit total residential growth to that which would result from development of all parcels at mid-range density, architectural diversity and economic variety are also equal goals of the City. The density of residential development allowed on any parcel within the City should take into consideration these City-wide goals as well as site specific considerations including, but not limited to topography, economics, neighborhood compatibility, provision of affordable housing, market conditions; and the capability of a proposed development project to further other specific goals, policies and objectives of the General Plan.

Policy 5.2 *State law requires the City to allow development of new residential areas and units as part of meeting the regional need for housing.*

Policy 5.3 *Provide additional affordable and/or senior citizen housing.*

Project Consistency: The New Pacific Properties Specific Plan includes a variety of single-family detached homes as well as opportunity for multi-family attached units. The planned single-family detached homes range in allowable density from 2.0 units per acre to 17.0 units per acre, with an average lot size from 2,668 square feet to 6,000 square feet. Price ranges have not yet been determined, though they are expected to vary in relation to lot size and other characteristics.

The 7.8-acre parcel designated as multi-family attached homes will have a density of 12-30 units per acre and could produce a total of 117± units, though as an alternative, this site may be developed with the single-family development standards of planning area P7. The New Pacific Properties project will make a pro-rata contribution towards affordable housing in accordance with state redevelopment law. In addition, as part of a Redevelopment Area, tax increment revenues will be used, in part, to support affordable housing in accordance with the law.

Objective 6 **Provide residential neighborhoods with a variety of cost ranges disbursed throughout the City.**

Policy 6.1 *Larger scale residential development should, within its land use designation and density range, include a mix of dwelling types while preserving the existing natural topography, where feasible.*

Policy 6.2 *Multi-family residential land should be developed with a balance of open space, landscaping, and recreational amenities and should be accessible to commercial and recreational areas and public transportation.*

Policy 6.3 Wetlands mitigation, flood control improvements and riparian corridors should not be used in the calculation of required park space, parks, or recreational areas. However, the City may accept such areas in the calculation of required park space if they are accessible to the general public for use and enjoyment.

Project Consistency: The New Pacific Properties project will provide a mix of dwelling and lot sizes and price ranges, while preserving the general terraced and sloping existing and natural topography of the site. Prices are anticipated to meet the needs of a wide variety of buyers. Approximately 61.1± acres of recreation, parks and open space will be provided on site. Open space areas will be connected to commercial and residential areas, as well as public transportation corridors. Wetlands on site will be preserved to the greatest extent possible as a "Natural Open Space" area, and will be enhanced to mitigate for any loss due to development. The Natural Open Space area has not been included in the park calculations.

Objective 7 Achieve a pattern of development that is consistent with the City's desired image.

Policy 7.1 Establish a visual identity for the City that distinguishes it from the surrounding areas.

Project Consistency: The proposed residential, school and retail development is consistent with the high standards envisioned for education, housing, retail and City services. The Specific Plan provides siting, architectural, and landscape design guidelines that will establish an identity for the project's various components and a Traditional American architectural design theme. The landscape plan will enhance the section of San Pablo Avenue adjacent to the site, the Enhanced Parkway, and smaller residential streets to create a warm and welcoming environment in the various planning areas.

Objective 8 Preserve Hercules history while developing its future.

Policy 8.1 Preserve and enhance the historic district area.

Project Consistency: The New Pacific Properties site does not lie within the historic district area, or the Hercules Properties, Inc. parcels.

Objective 9 Promote attractively designed and economically feasible development.

Policy 9.1 New development shall be planned and developed in a manner that funds or mitigates costs for providing municipal services. New residential development shall not create new unfunded costs for providing municipal services.

Project Consistency: The project will be attractively designed through establishment of development standards and design guidelines, including architectural, landscape and signage design guidelines. A detailed discussion regarding design implementation is provided in Chapter V-Design Guidelines, and Chapter VI-Implementation Program of this Specific Plan. Municipal service costs are supported by taxes generated by new development. Mitigation is additionally

supplied by providing tax increment funding, projected to be in excess of 100 million dollars that would not otherwise be available for local control to support affordable housing and economic development.

Objective 10 **Provide recreational and cultural amenities within the community that meet the needs of the residents and workers.**

Policy 10.1 *Promote development of a regional commercial recreation center or complex along, or adjacent to, the major transportation routes (I-80, Highway 4 freeway, and San Pablo Avenue).*

Policy 10.2 *Create places for residents and workers in the community to meet and socialize.*

Project Consistency: Recreation facilities will be a part of the project's parks/open space program. Neighborhood and community parks will be equipped with improved trails and areas for the public to congregate. On-site trails will allow for pedestrian hiking or bicycle recreation, will connect to the neighborhood and community parks throughout the site, and link to the regional Bay shoreline trail system as well as the area wide bicycle system.

Objective 11 **Participate and cooperate in regional and sub-regional planning activities.**

Policy 11.1 *Cooperate with appropriate jurisdictions and/or agencies in preparation of State Mandated Regional Plans (e.g., Congestion Management and Source Reduction and Recycling element).*

Policy 11.2 *Participate in regional and sub-regional planning and traffic issues to better address the potential regional impacts upon the community.*

Project Consistency: The project will participate in applicable regional and sub-regional planning activities. For example, the proposed Shoreline Trail/Park will allow connection to, and be developed in consultation with, the East Bay Regional Parks District shoreline trail system and standards. In addition, the project embodies the reuse of Brownfields sites consistent with the objectives of the Contra Costa County Economic Partnership.

Objective 12 **Attain new development with residential and employment mixed uses.**

Policy 12.1 *Encourage mixed use development that provides for an integrated mixture of residential and employment generating uses within the same structure.*

Project Consistency: The Retail/Residential Flex parcel is designed specifically to allow for retail and/or residential uses. Depending on market demand, the 6.0-acre area could be developed into 100% commercial uses, a mixture of commercial and residential uses, or 100% residential development. Thus the New Pacific Properties Specific Plan could provide an integrated mixture of residential and employment generating uses within the same neighborhood.

Objective 13 Compatible land uses within existing and planned development areas.

Policy 13.1 Create a transition between residential neighborhoods and commercial/industrial areas, except where such mixed uses are desirable (e.g. live/work space and other designated areas). Land uses must minimize adverse impacts, and those that would not negatively impact adjoining properties should be encouraged.

Policy 13.2 Design of flood control improvements along Refugio Creek should be coordinated with appropriate resource agencies and done in a manner to function as a transition area between land uses.

Policy 13.3 Strongly encourage cooperation and joint planning by and among owners of large parcels during the land use planning and entitlement process; this effort would include such items as master hydrology and circulation plans, joint studies and cooperative infrastructure development. In particular, it is anticipated that development of parcel C and the "southern slope" portions of the HPI site will be the subject of a joint planned development process.

Project Consistency: The Specific Plan provides a transition between existing residential neighborhoods to the north in the Community of Rodeo by retaining the existing tree stands and buffers. The plan also calls for open space buffers along the exterior borders of the Specific Plan area. Where the existing abutting use could detrimentally impact the proposed Specific Plan use, buffer areas have been incorporated into the Land Use Concept. For example, additional setback has been designed into the portion of the project which will abut the PG&E temporary storage facility. This Specific Plan does not negatively impact the existing surrounding development.

The New Pacific Properties Specific Plan area does not fall under the flooding influence of Refugio Creek.

The project meets the goal of encouraging cooperation and planning of large parcels during the land use planning and entitlement process. Discussions have been initiated to consider sharing easements for water line infrastructure improvements being undertaken in conjunction with the abutting elderly housing project and the New Pacific Properties Specific Plan project.

Objective 14 Protect and enhance significant and desirable environmental attributes and features.

Policy 14.1 Develop trail systems, open space, and other amenities that benefit the quality of life in the community.

Policy 14.2 Preserve the existing natural topography, ridgelines and valleys where feasible and desirable.

Project Consistency: The Specific Plan provides 61.1 acres of open space, comprised of a 1.6-acre Neighborhood park; a 0.9-acre Linear park; 2.3 acres of Community Trail Park; a 9.5-acre Shoreline Trail/Park; 3.6 acres of Natural Open Space; and 9.0 acres of Enhanced Parkway; 4.1

acres of land occupied by roads and 25.6 acres of associated open space, as well as a proposed School Facility site. The Shoreline Trail/Park will be dedicated to the East Bay Regional Park District and connect to the Districts San Francisco Bay Trail to the north and eventually south of the site. Community trails will connect the shoreline trail to the various park and open space areas distributed throughout the site. Grading of the site will follow generally the existing natural topography, forming a terraced slope towards the bay shore and creating enhanced views of the bay which were formerly degraded by refinery structures and operations. No significant ridgelines are impacted.

Objective 15 Provide for public, semi-public and non-profit uses and activities throughout the community.

Policy 15.1 Public, semi-public and non-profit uses may be allowed in commercial and industrial land use categories, if the type of use and level of activity is compatible with uses and activities allowed in that land use category. As an example corporation yards and utility substations may be allowed where industrial uses are allowed.

Project Consistency: A public elementary school site is being planned within the Specific Plan area. Both the Retail/Residential Flex and Open Space zoning districts of the New Pacific Properties Specific Plan will accommodate public and semi-public uses. Additionally, extensive public park areas are incorporated throughout the entire project area.

Objective 16 Work closely with developers and the West Contra Costa Unified School District and John Swett Unified School District to mitigate potential adverse impacts of future development on school facilities.

Policy 16.1 Refer applicants of new developments to the appropriate School Districts in order to pay the District's required developer impact fees prior to the building permit issuance for individual projects, as needed and justified, to maintain school performance standards.

Policy 16.2 Future elementary school sites will be combined with a park to maximize joint use possibilities for each facility.

Project Consistency: The proposed residential development of the New Pacific Properties project could generate approximately 685 students. The project falls within the West Contra County Unified School Districts (WCCUSD). This Specific Plan provides a 7.5-acre school site. Developer school impact fees, land acquisition and other school related requirements will be coordinated per state law and Senate Bill 50 (SB50) to mitigate adverse impacts.

The recent passage of Proposition 1A and the enacting law, Senate Bill 50 (SB 50) provides \$9.2 million in bonds to fund K-12 and higher education facilities. The funding for K-12 facilities will provide funding for new construction, modernization of older schools, funding for districts in hardship situations and funding for class size reduction. The provisions of SB 50 prohibit local agencies from denying either legislative or adjudicative land use approvals on the basis that school facilities are inadequate, and reinstate the school facility fee cap for legislative actions

(e.g., general plan amendments, specific plan adoption, zoning plan amendments). These provisions are in effect until 2006 and will remain in place as long as subsequent State bonds are approved and available.

SB 50 establishes three levels of developer fees:

- Level One fees are the base statutory fees of \$1.93 per square foot of residential development and \$0.31 per square foot of commercial/industrial development.
- Level Two fees allow the District to impose development fees above the statutory levels, up to 50% of certain costs under designated circumstances. The State would match the 50% funding if funds are available.
- Level Three fees apply if the State runs out of bond funds after 2006, allowing the District to impose 100% of the cost of the school facility or mitigation minus any local dedicated school moneys.

In order to levy the alternate (Level Two) fee and qualify for 50% State-matching funds, the District must prepare and adopt a school facilities needs analysis, apply and be eligible for State funding, and satisfy one of the four specified criteria by January 1, 2000 (or two of the four criteria after January 1, 2000): 1) 40% of pupils are enrolled on multi-track year round schedule; 2) a general obligation bond to finance new school facilities has been placed on the ballot in the past four years and passed with 50% + 1 vote; 3) at least 20% of teaching stations are portable classrooms; or 4) the District has issued debt or incurred obligations for capital outlay in an amount equal to 15% of the District's local bonding capacity (or 30% if post-November 1998 landowner - approved Mello-Roos bonds are counted). The ability of a city or county to impose fees is limited to the statutory and potential additional charges allowed by the Act, as described above. The West Contra Costa County Unified School District endorsed the passage of Proposition 1A. Development of the project will conform with the law.

B. CIRCULATION/SCENIC HIGHWAY

The basic circulation system for the area is discussed in the Hercules General Plan. The New Pacific Properties Specific Plan improves upon the existing infrastructure (San Pablo Avenue) and adds additional local facilities for efficient and safe vehicular circulation and complementary facilities for pedestrians and bicyclists. The circulation system also proposes the construction of the Shoreline Trail/Park, which will provide linkages to the project's pedestrian/bicycle trail systems and park facilities to the park system on-site, as well as allow for future connections to the Bay Trail, a regional trail facility.

Objective 1 **Establish a long-term program for the construction of streets and preservation of future rights-of-way based on traffic projections.**

Policy 1.1 *The City shall participate in and/or encourage the following planned capital improvements, as applicable: (Transportation 3a)*

- *Installation of ramp-metering hardware at all on-ramp locations on I-80;*
- *Reconstruction of I-80/Highway 4 freeway interchange'*
- *Construction of Highway 4 freeway;*

- *Widening of I-80 to include HOV lanes, Atlas Road to Carquinez Bridge.*

Project Consistency: Preliminary traffic projections show that traffic generated by the proposed project would be well below the acceptable levels of the existing requirements, as discussed further in this section and section "I" Growth Management. The New Pacific Properties project will comply with project-specific mitigation identified in the EIR for this Specific Plan and will contribute its fair share toward traffic impact mitigation.

Objective 2 Coordinate the street system with land use and other elements of the General Plan.

Policy 2.1 Proposed elements within view of designated scenic routes in the City should be reviewed in terms of their visual impact.

Policy 2.2 Sewer, water and stormwater facilities performance standards shall be maintained as required by the Growth Management Element.

Policy 2.3 Additional transportation policies are included within the Growth Management Element.

Project Consistency: San Pablo Avenue has been designated as a scenic route between its intersections at Pinole Valley Road and Interstate 80 at Crockett. Currently, the view towards the bay shore from I-80 fronting the New Pacific Properties Specific Plan area is blocked by existing topography and abandoned and deteriorating refinery improvements. The implementation of this Specific Plan would improve the view along San Pablo Avenue since the existing industrial refinery would be replaced by well designed, master planned residential development using a high level of aesthetic composition, architecture and landscaping standards. San Pablo Avenue itself will be enhanced with landscaping and circulation improvements. Improvements to the existing sewer, water and storm drainage system will be undertaken in conjunction with development of the New Pacific Properties Specific Plan, so that provision of these services will meet the performance standards of the Growth Management Element of the General Plan.

Objective 3 Unify the City with a functional internal street system of arterial, collectors, and local streets.

Project Consistency: The proposed roadways of the project are designed according to City standards and guidelines for construction of streets. These will connect to Hercules' existing street system and will join the project site to the core of the City.

Objective 4 Provide adequate access from the freeways to the surface street system.

Project Consistency: The streets of the Specific Plan area can be accessed from the freeway which serves the City of Hercules by existing facilities. The site can be reached from the south, off Interstate 80 (I-80) at Sycamore (the westerly extension of Highway 4), west to San Pablo

Avenue and north to the site. Alternate access can be gained from the north, off I-80 at Willow Avenue, west to San Pablo Avenue and south to the site.

Objective 5 **Coordinate the City's street system with adjoining City, county and state facilities.**

Project Consistency: All on-site roadways will be contained entirely within the site in conformance with Objective 3, and will connect to the Loop Roadway, which will access San Pablo Avenue. These connection points were chosen in coordination with the City's existing adjoining street system, and the adjacent system in Rodeo, with minimum spacing requirements between roadway intersections. The southern project entrance (the Primary Entrance) observes the spacing requirements between it and Linus Pauling Avenue, the northern entrance to the project and the distance to Willow Avenue.

Objective 6 **Maintain acceptable local circulation operating conditions on arterial streets/intersections and on local collector streets.**

Policy 6.1 *For health, safety and general welfare, it is the City's policy to provide adequate levels of traffic service throughout the City. Level of Service D or better is the City wide standard for traffic operating conditions during peak hours on arterial streets and intersections. Level of service D for the commercial/industrial development is acceptable under the following conditions:*

- 1) striving for off-peak uses*
- 2) producing Living Wage jobs*
- 3) generating City Revenue and/or*
- 4) proposing development that is otherwise highly desirable community-wide.*

New development shall be required to pay its fair share of the cost of improving regional routes so that compliance with the service standard specified in the Action Plan is maintained. (Policy 6.1 was summarized)

Project Consistency: Preliminary traffic studies performed by DKS Associates showed that the project's proposed land uses would generate 12,833 Average Daily Trips less than the commercial/industrial use permitted under current zoning and general plan designations. Impacts to the surrounding collector and local streets would be reduced significantly by the change in land uses on the site. Caltrans plans to widen the two lane segment of John Muir Parkway to four lanes, where traffic volumes during peak hours are already operating above capacity. The New Pacific Properties project will conform to the mitigation measures identified in the project environmental review to minimize impacts and pay its fair share of the cost for improvements along regional routes.

Objective 7 **Minimize through traffic in residential neighborhoods.**

Policy 7.1 *Neighborhood design should discourage through traffic on local streets.*

Policy 7.2 Residential streets will be designed in relation to the needed capacity and the adjoining housing patterns.

Project Consistency: The various residential designations of the New Pacific Properties project were arranged on either side of the arterial street, San Pablo Avenue, so that no through traffic enters a residential area. The Enhanced Parkway is the main access to the individual residential zones. Each residential zoning district has its own contained roadway network of minor and cul-de-sac streets, which connects to the Enhanced Parkway.

Objective 8 Promote public transit service within the City and area.

Policy 8.1 The City shall actively participate in cooperative efforts to provide effective public transit to the City and adjacent communities, including promoting a commuter rail extension of BART in the City and a train station along San Pablo Bay within the Lower Refugio Valley serving the Capitol Corridor to intercept through travelers on In-80.

Project Consistency: Public transportation for the City of Hercules and surrounding community is provided by several companies. The Western Contra Costa County Transit Authority (WestCAT) and WestCAT Dial A Ride Service, Alameda-Contra Costa Transit (AC Transit), and Bay Area Regional Transit (BART) Express. Currently, the AC Transit line runs along San Pablo Avenue, and the BART Express line runs to Linus Pauling Avenue less than a mile south of the site.

Objective 9 Provide a comprehensive system of riding and hiking trails.

Policy 9.1 The City should promote the establishment of riding and hiking trails throughout the community and coordinate with other agencies planning trail systems in the area and region.

Project Consistency: The easement section of Shoreline/Trail Park to be granted to the East Bay Regional Park District will be a pedestrian hiking/riding trail. It will allow the future connection at the north edge of the New Pacific Properties project site to north and south along the western edge of the site, and connect through the project's on-site trail system, to the Neighborhood Park, Community Trail Park, Linear Park, and school site areas of the project. Since the East Bay Regional Park District has not acquired the bay shore land to the south of the project site, there is no connection currently available. However, the trail could be easily connected in the future when such is incorporated into the regional trail system.

Objective 10 Provide for needed transmission facilities in a manner compatible with other elements of the General Plan.

Policy 10.1 Major transmission and fuel lines should be reviewed to ensure compatibility with affected General Plan elements.

Project Consistency: Implementation of the New Pacific Properties Specific Plan will entail extension of water and sewer transmission facilities, and relocation of existing water, fuel, and electrical transmission lines running through the inland portion of the site. The existing 24" and 36" aqueducts in the easterly portion of the project, as well as a TOSCO pipeline running along side, will be relocated. The P.G.&E. electrical lines running along eastern and northern borders of the project which serviced the refinery are slated for abandonment.

C. HOUSING

The goals and policies contained in the Housing Element are intended to provide a wide range of housing opportunities within the City. The New Pacific Properties Specific Plan will help the City increase its stock of affordable housing and help reach the City's fair share affordable housing goals as defined by the Association of Bay Area Governments.

Objective 1 **Provide a sufficient number of affordable housing units to meet the needs of current Hercules residents and provide a fair share of market area housing needs, as outlined by the Association of Bay Governments.**

Policy 1.1 *On a City-wide basis, increase the number of housing units for persons within the various household income levels to meet the need for additional housing during the 1990-1995 period. This objective is to add the following number and percent of housing units during the five-year period:*

| | <i>Very low Income</i> | <i>Low Income</i> | <i>Moderate Income</i> | <i>Above Moderate Income</i> | <i>TOTAL</i> |
|-------------------|----------------------------|-----------------------|----------------------------|--------------------------------------|--------------|
| <i># of Units</i> | 202 | 151 | 240 | 669 | 1,262 |
| <i>Percent</i> | 16% | 12% | 19% | 53% | 100% |

Policy 1.2 *New housing development should include a variety of home designs, at various densities and price levels, both lower and higher than presently available.*

Policy 1.3 *Seek all available resources, including tax-increment revenues, in-lieu fees, and State and Federal monies, to help meet its objectives for the provision of housing at the various income levels outlined above.*

Policy 1.4 *Actively support and work with non-profit housing development corporations and other housing providers to promote development of affordable housing*

Policy 1.5 *Use redevelopment tax increment funds for housing in cooperative ventures with the private sector to preserve and increase the supply of affordable housing.*

Policy 1.6 *Preserve affordability for a minimum of 15 years for projects developed with City assistance, and for a longer period for the one City-owned site.*

Project Consistency: The New Pacific Properties Specific Plan includes a variety of single-family detached homes as well as the potential for multi-family attached units in a variety of designs, at various densities and price levels, the majority of which are anticipated to be higher than presently available. The Redevelopment Plan includes the New Pacific Properties Specific Plan area. As such, the project will contribute substantial tax increment housing set aside funds, projected to be in excess of 45 million dollars. These funds will be used to fully mitigate the affordable housing requirements of the New Pacific Properties project. Affordable targets may be achieved through mortgage subsidies, low cost loan assistance, silent second mortgages, infrastructure assistance for affordable housing projects, or any other methods allowed by law. In addition to on-site affordable housing assistance, the tax increment housing set aside funds may be used off-site, in accordance with Redevelopment Law, so long as the funds spent are a direct benefit to the Redevelopment Area. It is the intent of this Specific Plan that tax increment housing set aside funds generated by the New Pacific Properties project are to be spent first to meet the affordable housing requirements of the New Pacific Properties project. As the tax increment housing set aside is projected to exceed the project's affordable housing requirements, additional tax increment housing set aside funds should be available to provide affordable housing in excess of project specific requirements, thereby increasing affordable housing supplies in the City.

Objective 2 Provide a selection of housing by type, tenure and price.

- Policy 2.1 Provide well-designed, well-built housing units for low- and moderate-income households in mixed-density developments, including Planned Unit Development (PUD), avoiding a concentration in any limited area.*
- Policy 2.2 Expand the number of rental units for those who cannot afford to purchase or who choose to rent.*
- Policy 2.3 Permit secondary housing units in all single-family residential areas, where their design and site features are compatible with surrounding areas.*
- Policy 2.4 In accordance with state legislation, manufactured housing and mobile home, built according to current federal standards, should be permitted on permanent foundations in residential neighborhoods, providing their exterior design is compatible with the surrounding homes.*

Project Consistency: This project establishes a variety of housing opportunities in scale and price for the new residents. Multi-family attached homes may be available as rental units.

Objective 3 Provide a sufficient number of housing units to meet the special needs of senior citizens, physically disabled, homeless, large families, and female-headed households.

- Policy 3.1 Encourage accessible units in all projects.*
- Policy 3.2 Encourage provision of senior housing by considering density bonuses up to general plan maximum where senior projects are desirable and compatible with*

special senior needs. Such projects shall only be allowed where the size of the structure and style of architecture are compatible with the surrounding neighborhood.

Policy 3.3 Encourage provision of low-income housing units, especially for single persons, single parents, elderly and young families.

Policy 3.4 Assist the county and social service groups to serve the City's homeless population.

Project Consistency: The project will contribute tax increment revenues to support affordable housing and will conform to the provision of affordable housing per state redevelopment law. The City's Redevelopment Plan includes the New Pacific Properties site. A variety of house designs will be offered to accommodate all sizes of households.

Objective 4 Maintain and improve the quality of the existing housing stock, to assure its continued livability and safety.

Policy 4.1 Minimize the deterioration of the existing housing stock and need for future rehabilitation or replacement by ensuring that at least the current level of quality is maintained, both for owner-occupied and rental units.

Policy 4.2 Restore and maintain residential structures of architectural or historic significance.

Project Consistency: The project consists entirely of new development and has no existing historical housing stock. Tax increment housing set aside funds can be used to improve existing housing stock under certain circumstances.

Objective 5 Maintain the quality of existing neighborhoods and encourage the development of attractive, viable new neighborhoods.

Policy 5.1 Where appropriate, encourage residential uses in commercial areas, and limited commercial uses in residential areas to promote access to services.

Policy 5.2 Design new residential areas to avoid conflict with major streets or thoroughfares, to have access to transit facilities, and to encourage safe and convenient alternatives to the private auto.

Policy 5.3 Provide and maintain municipal services and facilities at adequate levels in all residential neighborhoods, to meet current standards.

Project Consistency: The New Pacific Properties project will contain a flex site which could accommodate limited retail/commercial uses in combination with residential areas to provide convenient access to community goods and services. The development will utilize design guidelines as discussed in this Specific Plan, Chapter V, to maintain a high aesthetic standard

and quality. As conceptually designed, the project will not conflict with major streets or thoroughfares. Access is readily available to transit facilities running along San Pablo Avenue, and to the park and ride lots located near the site. All services and facilities will be designed and sized to accommodate service to the Specific Plan area in accordance with the project environmental review and City requirements.

Objective 6 **Promote energy conservation in new and existing residential units and neighborhoods with energy efficient design and placement of new housing as a priority.**

Policy 6.1 *Require timely and full compliance by the building industry with the California residential energy conservation standards and with the Solar Rights Act.*

Policy 6.2 *Encourage the use of solar systems, both active and passive designs, in new residential development.*

Policy 6.3 *Promote efficient patterns of development, such as clustering townhouses, duplexes, multi-family construction, and mixed uses.*

Policy 6.4 *Encourage the retrofitting of existing homes with energy conservation measures and solar systems where feasible and cost-effective.*

Project Consistency: Residential energy conservation is encouraged in the New Pacific Properties Specific Plan area through compliance with the California Energy Conservation standards and by allowing solar heating units in accordance with design guidelines. The project may include multi-family construction, innovative Z-lot line and "two-pack" model development. In Z-lot line areas, the property line zigzags around the dwelling to meet minimal setbacks, yet expands the yard space available to each home. The two-pack arrangement consists of setting back entrances and garages to vary the street elevation and allowing the placement of two adjacent homes to occur on their shared lot line. This design complies with Uniform Building Codes for health and safety while efficiently reorganizing open space.

Objective 7 **Promote equal housing opportunity and access for all persons and families regardless of age, race, ethnicity, religion, sex, family composition, or disability.**

Policy 7.1 *Eliminate discrimination in housing which is based on arbitrary and illegal criteria such as race, ethnicity, religion, sex, family composition, or disability.*

Policy 7.2 *Establish a means to facilitate resolutions of problems and conflicts which may occur in tenant-landlord disputes.*

Project Consistency: The project will comply fully with the City of Hercules to insure that the residences built on the New Pacific Properties will be equally accessible to potential tenants and owners regardless of age, race, ethnicity, religion, sex, family composition, or disability in accordance with the law.

Objective 8 **Ensure citizen understanding of Hercules' housing needs and program options, and promote broad participation in the implementation of the housing element.**

Policy 8.1 *Encourage public input to the review and update of Hercules' housing element.*

Policy 8.2 *Provide information on the City's housing needs and programs for meeting those needs to interested organizations and individuals.*

Project Consistency: Throughout the planning process of this project, public meetings and hearings have been held with the Community, the Planning Commission, the City Council and other governing agencies to discuss the project and its impacts and opportunities within the community. Public input has been accepted and carefully considered in the finalization of this Specific Plan.

D. OPEN SPACE/CONSERVATION

The Hercules General Plan provides a system of public parks, trails and open space networks throughout the City. The New Pacific Properties Specific Plan calls for an extensive system of pedestrian trails, a shoreline park, neighborhood park and other green space buffers. Overall, the Specific Plan will include approximately 23% of the project area as open space (excluding roads). Public access is provided to the open space areas including the related parks and Bay Trail. Portions of the wetland areas are also proposed to be preserved and enhanced as part of the overall land development scheme.

Objective 1 **Provide adequate recreation, park and open space resources as the community expands.**

Policy 1.1 *Expand the community's park, trail and open space system to meet the demands of future growth. The comprehensive park, trail and open space system shall provide linkages between developed and developing areas.*

Policy 1.2 *The City shall continue to work closely with the East Bay Regional Park District (EBRPD) in implementing the Shoreline Trail through Hercules*

Policy 1.3 *The City shall ensure that new development funds its share of costs associated with the provision of park facilities by attaching project-specific mitigation as conditions of approval.*

Policy 1.4 *The City may consider development agreements that will provide additional community parks and recreation facilities, such as ballfields and other areas for organized recreation, in exchange for allowing development at greater than the "typical" FAR, as specified in the proposed Land Use Element Update.*

Policy 1.5 *The City shall continue to honor the Briones Hills Agricultural Preservation Agreement that was adopted jointly with Lafayette, Martinez, Orinda, Pinole, Pleasant Hill, Richmond and Contra Costa County. The agreement protects a 64*

square mile area from development through a joint policy of not annexing any land within the preservation area to urban service districts or cities.

Project Consistency: The New Pacific Properties project proposes an extensive park, trail and open space system, all interlinked with one another, and connecting to the local and regional trail systems. The Specific Plan proposes to develop a 1.6-acre Neighborhood Park; a 0.9-acre Linear park running from the western neighborhood park, east to the Community Trail Park; a 2.3 acre Community Trail Park connecting to the projects internal parks and open space; 9.0 acres of Enhanced Parkway; 9.5 acres of Shoreline Trail/Park with a trail easement to be granted to the East Bay Regional Park District for connection to the San Francisco Bay Trail along the entire bay shore; 3.6 acres of natural open space left undeveloped; 4.1 acres of roads; and 25.6 acres of associated open space. Further discussion on these items is provided in Chapter III-Specific Plan Elements. The trails allow for linkages between both developed areas to the north, and developing areas to the south which would not have been available without development of this property. Additional recreational facilities will be available at the School site.

Objective 2 Preserve seasonal freshwater wetlands.

Policy 2.1 The City shall require project proponents to design construction footprints to avoid any wetlands and buffer zones around wetlands. If avoidance is not possible projects shall be redesigned so as to impact the least amount of wetlands. Any areas that are classified as wetlands and will be affected by project development shall be recreated either on or off site in accordance with CDFG and COE.

Project Consistency: Wetlands have been delineated, and determined to collectively cover approximately 0.97 acre on the southwestern portion of the site. The Land Use Concept plan has been developed to preserve the majority of the wetland area as Natural Open Space. Since development of the project could not avoid impacting a small portion of the jurisdictional wetlands or waters, the New Pacific Properties Specific Plan proposes to enhance existing wetlands on site, in accordance with the CDFG and COE and appropriate mitigation requirements. All appropriate resource agency permits will be acquired prior to any development activity in the affected areas.

Objective 3 Protect the Refugio Creek riparian corridor from encroaching development.

Policy 3.1 Design of building footprints along any riparian corridors shall be outside the CDFG-and/or COE-pre-approved buffer zone. Sensitive riparian habitats shall be marked by a qualified biologist to deter any destruction by equipment during construction.

Project Consistency: The proposed project does not lie within the vicinity of Refugio Creek and will not affect the riparian plant community found there. Mitigation measures from the project environmental review will be incorporated in the development.

Objective 4 **Protect riparian and wetland communities from degradation through introduction of urban pollutants in storm water runoff.**

Policy 4.1 *The City shall require project proponents to design facilities to prevent degradation of riparian and wetland communities from urban pollutants in storm runoff.*

Project Consistency: The New Pacific Properties project will adopt the City's program to install sedimentation and grease basins in the storm drain systems in parking lots. The project will comply with current federal National Pollutant Discharge Elimination Standards (NPDES) to maintain the quality of riparian and wetland communities in the Specific Plan area. Removal of the refinery from the site has removed significant potential impacts to the wetland from that facility, thus further enhancing protection of the wetlands.

Objective 5 **Preserve salt marsh zones along San Pablo Bay.**

Policy 5.1 *The City shall review development proposals for consistency with minimizing impacts to salt marsh zones. Buildings shall be located on existing developed or graded areas, where practicable.*

Project Consistency: The Specific Plan area does not incorporate the 12.68-acre salt marsh zone located to the west of the Union Pacific Railroad line and berm. This section of shoreline is part of San Pablo Bay, one of 13 Regional Shorelines defined by the East Bay Regional Park District (EBRPD) as having significant recreation, interpretive, or scenic values on land, water, and tidal areas. No development is proposed in this area.

Objective 6 **Protect native plant communities and habitats for special status plant and animal species.**

Policy 6.1 *The City shall continue to utilize environmental review under CEQA to review development projects that are not exempt from the California Environmental Quality Act for impacts on sensitive species and their habitat.*

Policy 6.2 *The City shall require that development within the General Plan area incorporate features to preserve habitat for sensitive species.*

Policy 6.3 *As much open space as possible within sites proposed for development shall be retained as informal open space for wildlife habitat, rather than as formal, landscaped parks or grounds. The City shall require that native plants from local area be used in landscaping, and in areas with a lower water table, native drought tolerant species shall be used in landscaping.*

Project Consistency: Native plant communities were identified along the western margin of the site resembling the coastal prairie plant community. Preliminary biological survey work recommended that a rare plant survey be conducted prior to any development to determine the

presence of several rare, threatened and endangered plant species commonly associated with coastal prairie vegetation in the San Pablo Bay region.

The land surrounding the wetlands in the southern portion of the site may also contain marginal habitat for red-legged frogs (*Rana aurora draytonii*), which has been listed as endangered by the U.S. Fish and Wildlife Service. It was also found that the wetlands may have the potential to provide breeding habitat for the California Tiger Salamander (*Ambystoma californiense*). Further site studies were also recommended prior to development of these areas to evaluate the potential for occurrence of these species. The mitigation measures identified in project environmental evaluation will be incorporated in the development.

Objective 7 Ensure an adequate water supply for the community.

Policy 7.1 The City shall cooperate with East Bay Municipal Utility District planning efforts to help ensure an adequate water system for existing and future residents and to maintain adequate water reserves.

Policy 7.2 The City shall implement programs of water conservation to make efficient use of its water supply.

Policy 7.3 Encourage effective water conservation practices by residents and businesses including household conservation and use of drought-resistant landscaping and reclaimed wastewater.

Project Consistency: The East Bay Municipal Utility District (EBMUD) is the chief supplier of water to the City of Hercules. Hydraulic analysis to be conducted by EBMUD will determine the appropriate fire flows necessary for the proposed land uses, and will indicate the mitigation measures required to install and provide adequate water service to the proposed project. It is anticipated that the developer will be required to extend main transmission lines to the site, and provide on-site service lines. This Specific Plan also recommends that landscaping incorporate drought-resistant plants. New Pacific Properties will secure a AWill-Serve@ letter from EBMUD as part of the development process.

Objective 8 Adequate wastewater treatment capacity to serve the existing and future demands.

Policy 8.1 The City shall initiate studies and programs to identify wastewater treatment requirements to meet existing and future demands, and implement effective funding mechanisms.

Project Consistency: Several options are available regarding sewer service for the New Pacific Properties Specific Plan area. The most desirable involves connection to the City=s existing wastewater treatment facility located in the City of Hercules. Other options include creation of a new treatment plant to be located elsewhere on the property, connection to the Hercules/Pinole wastewater treatment facility, connection to a new Joint Power=s Authority facility contemplated in the unincorporated community of Rodeo to the north, or connection to the Rodeo Sanitation District.

Objective 9 **Improve surface run-off water quality.**

Policy 9.1 *Develop a Master Water Quality Control Plan for the City, including measures to clean up existing sources of pollution, and to develop ways of preventing further pollution such as specific water treatment policies for industries and retention basins for surface runoff suspected of carrying roadway pollutants. (See policies and standards in the Growth Management Element)*

Project Consistency: The former Pacific Refinery facility located on the site is currently in the process of being dismantled and demolished. Environmental cleanup is being conducted simultaneously. Elimination of the Pacific Refinery and replacement with a primarily residential community will considerably improve the quality of surface run-off water.

Objective 10 **Reduce flooding potential within flood prone areas.**

Policy 10.1 *Ensure that adequate drainage facilities and pollution prevention and control infrastructure are built to accommodate the increase in runoff from newly developed areas.*

Project Consistency: The site is not subject to flooding and does not include flood prone areas. Nevertheless, setbacks of over 100' from the railway berm which abuts the bay shore have been incorporated into the project design. Drainage courses along the site have been designed to transport runoff water efficiently from the site to collection facilities and outfall sites.

Objective 11 **Improve air quality within the community.**

Policy 11.1 *Development within the City shall be conditioned to reduce air quality impacts during construction and subsequent operation.*

Project Consistency: Air quality within the community will be improved with closure and dismantling of the Pacific Refinery, which will eliminate historically detrimental air quality impacts which were generated by that heavy industry facility. During the construction phase, the project will comply with the City's dust abatement program, which includes the following measures. All construction areas will be misted with water, when possible, at least twice a day during excavation and ground preparing operations to reduce dust emissions from the ground. Stockpiles of sand, soil and earthen materials will be covered or equipped with windbreaks to substantially reduce wind erosion and excessive dust concentrations. Trucks carrying dirt and debris offsite will also be covered during transport. Vehicle speeds will be reduced to 10 miles per hour on unpaved roads and over disturbed soils on site. Dirt or debris spilled onto paved surfaces will be swept or washed immediately to reduce the potential for vehicular movement to create dust emissions into the air. The contractor will maintain and operate construction equipment to minimize exhaust emissions and run equipment only when necessary. The Contractor will also be required to monitor the implementation of an approved dust control program and increase watering of soils, when necessary.

Objective 12 Protect and preserve important historic and prehistoric resources.

Policy 12.1 Prehistoric resources shall be identified and preserved to the extent feasible. If previously unknown subsurface cultural resources are discovered during excavation activities on the identified parcels or elsewhere in the study area, excavation would be temporarily halted and an archaeologist consulted as to the importance of the resources. Should the archaeologist determine that the resources are important, the project sponsor would follow the procedure described in Program 12a.2.

Project Consistency: Both pre-historic archeological and paleontological resources were found on the New Pacific Properties Specific Plan area. In compliance with the General Plan objectives, and CEQA and City guidelines for the preservation of historic and prehistoric resources, the mitigation measures provided in the project environmental review will be incorporated in the project.

Objective 13 Preserve and enhance scenic views within the community.

Policy 13.1 Development proposals shall be reviewed in terms of natural objects in the vicinity that have aesthetic significance. This may include open space, eucalyptus groves, or vegetation that serves as a view corridor or has important visual attributes. Development proposals shall be sited to ensure that these features are retained or replaced to the extent feasible, resulting in minimal view impairment.

Policy 13.2 Plantings that serve to screen views of residential development, or that help maintain a natural-appearing landscape, shall be retained to the extent feasible. Such plants could be thinned selectively if thinning would improve view corridors. If specific trees are to be removed, such as eucalyptus trees, preferably native species, that will provide suitable screening while retaining important view corridors, especially along San Pablo Avenue which is a designated scenic corridor.

Policy 13.3 Preserve the wooded tree-lined character of the proposed hiking/biking trail along Refugio Creek west of I-80 through retention of existing vegetation and/or planting replacement trees and other vegetation.

Policy 13.4 Development shall preserve important view corridors, where feasible, by identifying and preserving the attributes of the view corridors that characterize its significance (e.g. framing elements, surface water reflections, presence or absence of impinging details) as seen from roadways, pedestrian paths or other public vantage points to avoid view obstruction. Buildings shall be sited so as to minimize view obstruction from sensitive viewpoints.

Policy 13.5 New development shall be designed to minimize light and glare impacts.

Project Consistency: Although San Pablo Avenue has been designated as a scenic route, topography and the existing Pacific Refinery facility effectively block views to the bay from the avenue. The New Pacific Properties Specific Plan proposes to replace the existing Pacific Refinery with a well designed residential community using a high level of aesthetic composition, architecture and landscaping standards. This proposal will enhance the views along San Pablo Avenue through roadway and landscaping improvements. The existing tree buffer along the northern border of the site will be retained as a buffer and to further enhance the view over the site. New dramatic views towards San Pablo Bay and the surrounding environment will be created and afforded to new residents of the New Pacific Properties project. Views of the San Pablo Bay shoreline will be opened to the public along and within the Shoreline Trail/Park.

E. SAFETY/SEISMIC SAFETY

Objective 1 **Consider potential seismic, geologic, flood and fire hazards and introduce adequate safety measures in development plans and proposals.**

Police 1.1 *Seismic, geologic, flood and fire safety policies will be integrated into other mandatory elements of the General Plan. Administration and enforcement of municipal regulations provide positive measures for implementing safety policies.*

Project Consistency: The site is not subject to site specific significant seismic, geological or flood hazards. It is situated in a high fire hazard area, and as such, the Specific Plan incorporates architectural and landscape design feature recommendations to mitigate the fire hazard. Removal of the refinery significantly reduces fire hazards in the community. The use of fire resistant landscaping plant materials is encouraged. For information on specific programs implemented by this Specific Plan, please refer to Chapter V, *Design Guidelines*.

Objective 2 **Minimize exposure of public facilities and development to seismic hazards.**

Policy 2.1 *No critical facility or school shall be permitted in areas subject to very strong ground shaking or ground failure until an evaluation of alternative sites with reduced seismic hazards are completed.*

Policy 2.2 *Projects proposed for all critical facilities including schools, high-population facilities (such as shopping malls) and industries using or generating significant amounts of hazardous materials within areas subject to very strong earthquake ground shaking or ground failure shall conduct geotechnical studies and structural design evaluations.*

Policy 2.3 *The City will update the Earthquake Preparedness and Emergency Response Plan as necessary to establish emergency access points to evaluate the comprehensiveness of the City's evacuation routes in relation to the specific effects of seismic-induced ground shaking, liquefaction, and lurching within the community.*

Policy 2.4 The administration of subdivision and grading ordinances should allow for flexibility in the review and approval of construction plans to permit sound engineering design in the solution of specific geological problems. Site specific geotechnical investigations shall be required for every new development.

Project Consistency: The Specific Plan area, including the proposed school site, does not lie in a significant geologic hazard area, and no major faults traverse the project area. A preliminary soils analysis of the Specific Plan area indicated that the site provides excellent foundation conditions from the standard seismic hazards. All grading will occur in compliance with City standards, which will assure proper compaction of fill and hill stability.

Objective 3 **Ensure that adequate fire protection is provided throughout the City and that all new structures conform to current fire safety standards.**

Policy 3.1 The City should continually evaluate the alternatives for providing adequate fire service to meet the changing needs of the City in the most efficient manner.

Policy 3.2 New development shall be designed to minimize exposure to fire hazards.

Project Consistency: The Fire Station on Refugio Valley Road and the Rodeo Station on Third Street in Rodeo are the responding fire service stations for the site. The Refugio Valley Road Station has a response time of under five minutes to all portions of the Specific Plan area, and the Rodeo Station is located in even closer proximity to the project, thus a new fire station location is not required on site. The New Pacific Properties project will be designed to minimize exposure to fire hazards in many ways. Trails will be developed to accommodate fire vehicles and apparatus where warranted. Fire resistant plant material will be used in open spaces and a buffer of irrigated landscaping will be maintained between open spaces and developed areas to minimize fire hazards. Architectural design guidelines include recommendations for features which minimize exposure to fire hazards, such as concrete, cement or composition shingles or similar fire resistant roofing materials. Lastly, subdivision design will incorporate significant setbacks from possible off-site fire sources, such as the PG&E fuel temporary storage facility located adjacent and to the south of the Specific Plan area on the east side of San Pablo Avenue.

Objective 4 **Reduce flood hazards through flood channel improvements and development standards.**

Policy 4.1 New development shall be sited and designed to minimize generation of and exposure to flood hazards.

Project Consistency: The New Pacific Properties site is not in a designated flood zone. Drainage will be designed to prevent flooding downstream.

F. NOISE

Objective 1 **Ensure that all new development is compatible with the existing and future noise environment.**

Policy 1.1 Protect existing noise-sensitive land uses from long-term noise impacts generated by new projects. The City shall use the following criteria to judge the significance of long-term noise impacts on existing noise-sensitive land uses:

- Noise level increases resulting from traffic associated with new projects will be considered significant if: (1) the noise level increase is 5 dBA Ldn or greater and the future noise level is less than 60 dBA Ldn; or (2) the noise level increase is 3 dBA Ldn or greater and the future noise level is 60 dBA Ldn or greater.*
- Noise levels produced by stationary sources associated with new projects will be considered significant if they exceed the noise level standards set forth in Table 7 as measured at any affected noise-sensitive land use.*
- Noise levels produced by other noise sources (such as ballfields, etc.) will be considered significant if an acoustical study demonstrates that a significant adverse community response would occur. The criteria to judge the significance of the community response would be based on acceptable analysis techniques such as the International Standards Organization's "Assessment of Noise with Respect to Community Response, ISOR-1996-1971".*

Project Consistency: The New Pacific Properties project is not expected to generate noise levels above those historically associated with the industrial use of the site. Noise levels generated from the site are anticipated to reduce with implementation of the project.

The City's General Plan preliminary noise analysis was consulted during initial design of the Land Use concept. In addition, a detailed noise analysis was performed for the site, which confirmed that noise levels over the Specific Plan area range from 60 dBAs to just over 70 dBAs. Where sensitive areas such as residential or school uses are located adjacent to noise sources, such as main thoroughfares, sufficient land, landscape or wall sound buffers will be provided for the safety and comfort of inhabitants. The Land Use Concept has sited the school so that it is surrounded primarily by residential uses. The Specific Plan further buffers the school site from possible noise generated by San Pablo Avenue by a slope and a landscape buffer. The western residential area is set apart from the Union Pacific Railroad by a slope and the Shoreline Trail/Park, which includes a meandering strip approximately 150 feet wide of landscaped park and open space. Home sites in close proximity to the railroad may incorporate berms, sound walls and other building techniques to minimize noise impacts. Site planning will allow for certain homes to be placed so as to use non-noise sensitive structures such as garages to shield noise-sensitive areas, or orient buildings to shield outdoor spaces from a noise source.

Since the Enhanced Roadway on the project will be used primarily by residents and their guests, heavy traffic is not expected and will not produce noise beyond that of a residential street. Nevertheless, it is anticipated that landscape walls will be incorporated into rear lot design for those parcels which abut the enhanced parkway. San Pablo Avenue is an arterial roadway, and residential parcels adjacent to this street will receive treatment to reduce noise to an acceptable level. The New Pacific Properties Specific Plan provides buffers and incorporated siting design elements to minimize noise levels. Such mitigation may include the implementation of walls,

berms, fences, landscape buffers, added insulation, or the minimal use of windows on street facades.

Objective 2 **Prevent all new noise sources from increasing the existing noise level above acceptable standards.**

Policy 2.1 *New residential development projects shall meet acceptable exterior noise level standards. The noise contour map on file at City Hall shall be used to screen projects to determine if acoustical studies will be required. The "normally acceptable" noise standards for new land uses established in Land Use Compatibility for Community Exterior Noise Environments shown in Table 6 shall be modified by the following:*

- *The maximum acceptable noise levels in residential areas is an Ldn of 60 dBA. This level shall guide the design and location of future development, and is a goal for the reduction of noise in existing development. A 60 dBA Ldn goal will be applied where outdoor use is a major consideration (e.g., backyards in single-family housing developments and recreation areas in multi-family housing projects). The outdoor standard will not normally be applied to small decks associated with apartments and condominiums, but these will be evaluated on a case-by-case basis. Where the City determines that providing an Ldn of 60 dBA or lower cannot be achieved after the application of feasible mitigation, an Ldn of 65 dBA may be permitted at the small decks at the discretion of the City council.*
- *Indoor noise level shall not exceed an Ldn of 45 dBA in new housing units.*
- *If the noise source is a railroad, then the outdoor noise exposure criterion should be 70 dBA Ldn for future development.*
- *Noise levels in new residential development exposed to an exterior Ldn of 60 dBA or greater shall be limited to a maximum instantaneous noise level in bedrooms of 50 dBA. Maximum instantaneous noise levels in all other habitable rooms should not exceed 55 dBA. The typical repetitive maximum instantaneous noise level at each site would be determined by noise monitoring. Examples would include truck passbys on busy streets, train passbys and train warning whistles.*
- *Appropriate interior noise levels in commercial, industrial, and office buildings are a function of the use of space and shall be evaluated on a case-by-case basis. Interior noise levels in offices generally should be maintained at 45 dBA Leq (hourly average) or less.*
- *These guidelines are not intended to be applied reciprocally. In other words, if an area currently is below the desired noise standards, an increase in noise up to the maximum should not necessarily be allowed. The impact of a proposed project on an existing land use should be evaluated in terms of the*

potential for adverse community response based on the significant increase in existing noise levels, regardless of the compatibility guidelines.

For non-transportation related noise sources, outdoor noise levels within a residential property should not exceed the limits in Table 7. Interior noise levels shall be 15 decibels lower than those shown in Table 7.

Policy 2.2 New non-residential land development projects shall meet acceptable exterior noise level standards set forth in Table 6. The noise contour map on file at City Hall shall be used to screen projects to determine if acoustical studies will be required.

Policy 2.3 Noise created by commercial or industrial sources associated with new projects or developments shall be controlled so as not to exceed the noise level standards set forth in Table 7 as measured at any affected residential land use.

Project Consistency: The City's General Plan preliminary noise analyses projected that the Day-Night Noise Levels (Ldn) for the site would fall between 60 to just over 70 decibels. A detailed noise analysis conducted for the site confirmed this range. Decibels ranging from 60-70 dBA are located in three narrow bands along Interstate 80 Freeway at the east end of the Specific Plan area; along the Union Pacific Railroad which runs along the western boundary of the site; and along San Pablo Avenue. Residential community noise exposure of 60 dBA would be normally acceptable, and those areas with ranges of 60-75 decibels would be conditionally acceptable, requiring noise insulation and mitigation features. A rating between 70-80 dBA would also require noise reduction/insulation features and shielding of outdoor areas. A special provision is also contained in the Noise element of the City's General Plan, which raises the residential exterior noise standard to 70 dBA for areas located near a railroad.

Although the East I-80 runs along the eastern boundary of the New Pacific Properties site, considerable differences in elevation between the highway and the site, the freeway being as much as 112 feet lower in elevation, significant noise impacts will be limited to the northeastern portion of the property. The areas that are to be impacted the most by noise generated from the railroad has been included in open space as a Shoreline Trail/Park providing landscaped buffers, slopes and berms between the tracks and the neighboring residential uses. The potential for noise levels as high as 85 dBA exists within the retail/residential flex site, as generated by large commercial trucks. Mitigation measures including on-site landscape sound berms and buffers, sound reducing walls, sound mitigating building design and materials, and site placement and setbacks will be used to insure that sound levels from I-80, San Pablo Avenue and the Union Pacific Railroad track do not exceed residential standards.

Objective 3 Eliminate or reduce noise from existing or objectionable noise sources.

Policy 3.1 Adopt/update a noise ordinance or nuisance ordinance to control noise generating activities, such as barking dogs, loudspeakers, parties, power tools, etc.

Policy 3.2 Control the level of noise at noise-sensitive land uses generated by construction activities through implementation of the following measures:

- *For construction near noise-sensitive areas, as determined by the Community and Business Development Department, require that noisy construction activities (including truck traffic) be scheduled for periods, according to construction permit to limit impact on adjacent residents or other sensitive receptors.*
- *Develop a construction schedule that minimizes potential cumulative construction noise impacts and accommodates particularly noise-sensitive periods for nearby land uses (e.g., for schools, churches, etc.)*
- *Where feasible, require that holes for driven piles be pre-drilled to reduce the level and duration of noise impacts.*
- *Where feasible, construct temporary solid noise barriers between source and sensitive receptor(s) to reduce offsite propagation of construction noise. This measure could reduce construction noise by up to 5 decibels.*
- *Require internal combustion engines used for construction purposes to be equipped with a properly operating muffler of a type recommended by the manufacturer. Also, require impact tools to be shielded per manufacturer's specifications.*

Policy 3.3 Reduce the level of truck-generated noise in residential areas through implementation of the following restrictions:

- *The City shall restrict truck traffic in residential area except for non-regular deliveries within the area or on designated truck routes. The City shall review and update the noise ordinance to limit truck traffic noise impacts to sensitive receptors.*
- *The City shall post signs prohibiting trucks from using the proposed Claeys Road extension, except for local deliveries. All other trucks shall be required to use Sycamore Avenue to reach the Claeys Road/SR4 interchange.*

Project Consistency: As identified earlier, existing noise generated from the New Pacific Properties Specific Plan area is anticipated to be reduced significantly as the use of the property changes from heavy industrial to primarily residential and retail. The new proposed school and retail uses will generate some new noise levels on the inland side of the property, but they will be significantly lower than the heavy industrial uses which previously influenced the area. To maintain acceptable noise levels throughout construction, all activities will follow the scheduled work periods determined by the CEQA process and City standards. A construction schedule will be implemented by the Developer to minimize cumulative noise impacts through the various phases of construction.

A land use pattern has also been developed which limits traffic and deliveries to the retail site through access off San Pablo Avenue, at the south end of the project, avoiding passage through residential areas and the remainder of the Specific Plan area. Trucks that need to travel on San Pablo Avenue directly through the project will not need to enter any residential areas.

G. HAZARDOUS WASTE MANAGEMENT

Objective 1 **Accept responsibility and develop appropriate planning for the safe and responsible treatment and transfer or disposal of wastes within the City jurisdiction or in coordination with other jurisdictions.**

Policy 1.1 *To adopt appropriate administrative authority to monitor and enforce City ordinances for businesses and other hazardous waste users within the City limits of Hercules.*

Policy 1.2 *To train City employees who handle or otherwise come in contact with hazardous materials or hazardous waste in the proper safety procedures for management and handling of these substances.*

Project Consistency: In conjunction with closure of the Pacific Refinery, and requests for removal of refinery improvements, a decommissioning, dismantling and remediation plan was developed and approved by the City, dismantling of the refinery improvements is underway, and mitigation measures are being implemented. Adoption of the New Pacific Properties Specific Plan will significantly reduce the quantity of hazardous wastes to be generated in comparison with the former Pacific Refinery land use, or any other industrial land use. The land uses proposed within the Specific Plan are not anticipated to generate significant amounts of hazardous waste.

Objective 2 **Designate prevention of deterioration of public health or the environment caused by hazardous waste as a primary goal of the City government.**

Policy 2.1 *To bring all City facilities into compliance with proposed hazardous waste management policies of this plan, and with other existing statutes and ordinances at the county, state and federal level.*

Policy 2.2 *To train current inspectors to recognize hazardous materials and hazardous waste management problems and to coordinate with fire department inspectors in order to ensure compliance with the Hercules Municipal Code and other applicable regulations.*

Project Consistency: Again, closure, decommissioning and dismantling of the Pacific Refinery and site remediation will significantly help the City to bring its facilities into compliance with proposed hazardous waste management policies. Land uses proposed within the Specific Plan are not anticipated to generate significant amounts of hazardous waste.

Objective 3 **Adopt policies and targets which restrict further increases in and seek reductions in the volume and toxicity of hazardous waste committed to land disposal.**

Policy 3.1 *To establish an enhanced hazardous waste and materials data base, oriented toward land use issues and zoning ordinance enforcement, to be implemented by the Planning Department, which will incorporate information from businesses which hold City of Hercules business licenses.*

Project Consistency: Again, conversion of the site from a heavy industrial uses (which generated significant volumes of hazardous waste amounting to 57% of the City's hazardous waste shipped off site), to a residential and retail development will reduce the volume and toxicity of hazardous waste committed to land disposal.

Objective 4 **Oppose increases of hazardous waste treatment, storage or disposal within the City limits unless such activities are consistent with this Plan, and laws and ordinances of the City of Hercules.**

Policy 4.1 *To direct the City Attorney to enforce all applicable ordinances pertaining to hazardous waste generation, transport, storage and handling, and to adequately deter accidental or purposeful violations of these ordinances.*

Project Consistency: The project does not propose to develop any hazardous waste generators which are not in accordance with the City of Hercules General Plan or any laws and ordinances of the City. This Specific Plan proposes residential, retail, school and open space development which are anticipated to generate small amounts of hazardous waste in conformance with City requirements. Commercial development requires consistency with the General Plan, laws and ordinances of the City of Hercules.

Objective 5 **Encourage as a first priority, waste minimization and source reduction of existing waste generation facilities.**

Policy 5.1 *To encourage waste minimization by local businesses through source reduction, product substitution, development or alternative technologies, recycling or other effective means.*

Policy 5.2 *To encourage community participation in municipal household hazardous waste collection through educational and technical programs. Such programs might include distribution of educational materials, sponsoring educational events, arranging community hazardous waste collection days and investigation of potential for a permanent household hazardous waste collection facility, in accordance with the solid waste JPA.*

Project Consistency: The New Pacific Properties development will encourage waste minimization of its inhabitants through the means mentioned above and participate in technical

or educational programs run by the City or other agencies. It is anticipated that the project will participate in and be serviced by the City's recycling waste collection program.

Objective 6 Encourage recycling, reuse and on-site treatment as second priorities for hazardous waste management techniques.

Project Consistency: On site treatment resulting from new development will not be feasible considering the minimal amount of hazardous waste potentially produced by the new development. The project will, however, carry out appropriate mitigation measures to actively encourage recycling and reuse of its generated hazardous waste.

Objective 7 Provide strong direction and support to actively enforce laws, regulations and ordinances concerning issuance of permits, inspection, compliance and data availability concerning the generation, storage, transportation, treatment and disposal of hazardous waste or the generation, storage and transportation of hazardous materials.

Policy 7.1 To enhance information submitted by local businesses regarding the generation storage, transport and disposal of hazardous waste within the City limits of Hercules upon application or renewal of City business licenses.

Policy 7.2 To assist with coordination of county or regionally sponsored technical assistance programs to small quantity generators and other industry regarding hazardous material handling and hazardous waste management.

Project Consistency: The New Pacific Properties project will conform with all applicable Hercules laws, regulations and ordinances concerning the generation, storage, transportation, treatment and disposal of hazardous waste and materials.

Objective 8 Develop effective programs for waste management within the appropriate City agencies to achieve a coordinated strategy to deal with Citywide waste management issues.

Policy 8.1 To designate the City Manager as the lead coordinator with each department responsible for the achievement of specific hazardous waste management goals for the City of Hercules. The Department shall accomplish these management goals by working with designated county agencies including the Rodeo-Hercules Fire Protection District, County Planning Department and County Health Department.

Policy 8.2 To coordinate City actions with the Rodeo-Hercules Fire Protection District and other emergency response agencies. To train City personnel in the emergency response procedures in interagency agreements through establishment of an interagency emergency response task force for this purpose.

Project Consistency: The proposed development will comply with waste management requirements of the City of Hercules.

Objective 9 **Work in coordination with other applicable jurisdictions to formulate workable agreements for inter-jurisdictional policies regarding mutual hazardous waste management concerns.**

Policy 9.1 *To appoint a person or committee charged with maintaining communication with other (neighboring) jurisdictions and arrange a mechanism for developing inter-jurisdictional waste management strategies for common problems involving the City and neighboring jurisdictions.*

Project Consistency: The New Pacific Properties development will cooperate with City agencies and neighboring jurisdictions to participate in the formation of workable agreements regarding hazardous waste management, to the extent applicable.

Objective 10 **Investigate the feasibility of a Joint Powers Authority (JPA) or other regional agreement for the management of hazardous waste.**

Project Consistency: The proposed development will cooperate in the feasibility investigation of regional or Joint Powers Authority agreements which might be developed regarding management of hazardous waste, to the extent applicable.

H. ECONOMIC DEVELOPMENT

Objective 1 **Reduce sales tax leakage, target specific retail businesses for development, provide on-going support to retail businesses, and provide the goods and services needed by residents.**

Policy 1.1 *The City of Hercules will continue to work with other cities in the County to site one or more new county-wide landfills, and to implement the requirements of Assembly Bill 939.*

Project Consistency: The Hercules General Plan encourages new retail development within the City. Since sales tax comprises a vital part of the funding for municipal operations, it is important that successful shopping centers and retail/commercial opportunities occur within the community. This Specific Plan proposes to allow for up to 6.0 acres of retail/residential flex development to support the Specific Plan residential neighborhood. Additionally, new residents will significantly support existing under performing businesses within the community. Solid waste generated from the New Pacific Properties project will be transported to the Richmond sanitary landfill. According to the Richmond Sanitary Service, that landfill has sufficient capacity to accommodate projected waste from the project. Furthermore, once that landfill is full, solid waste will be transported to the Portrero Hills landfill, located in the community of Fairfield to the east.

Objective 2 **Maximize the collection of existing revenue and adopt new on-going revenues, as needed, to continue providing quality City services; and, maintain and promote efficiency in City operations.**

Policy 2.1 *The City of Hercules recognizes the need to provide sewage treatment capacity that is sufficient to treat wastewater from existing and foreseeable development in the City, without causing violations of water quality standards.*

Project Consistency: The City operations have historically been financed by development service fees, interest earnings and pre-Proposition 13 property taxes. The decline in active development as the City approaches build-out has created a revenue shortfall, hindering the City's ability to provide adequate services to its residents. The New Pacific Properties development will add up to 880 residential units on 136.2 acres of land and up to 6.0 acres of retail /residential flex property. These will be subject to property and sales taxes, respectively, contributing to Hercules' tax base revenue. Also, since the Specific Plan area falls within the City's Redevelopment Expansion Area, new tax increment revenue will be generated to help support City infrastructure.

Sewage treatment for the City of Hercules is provided by the City of Pinole/Hercules Plant and the Hercules Plant. The New Pacific Properties project will contribute it's pro rata share towards facilities needed to support proposed development, if connection and capacity are made available.

Objective 3 **Create a positive environment for economic development by maximizing human resource and community attribute potential. This objective includes promoting citizen involvement in the community and the educational system, and continuing to insure provision of adequate health, safety, recreation and social programs in the community.**

Policy 3.1 *The City of Hercules will continue to provide and encourage the community programs which support a positive business environment and will work toward greater visibility of the community to promote business activities.*

Project Consistency: This Specific Plan seeks to promote the high standards in housing, infrastructure, facilities, maintenance, and community life which make Hercules attractive to businesses that induce economic development. Proposed infrastructure improvements will enhance City services and promote adequate safety in the community. Project development will be in accordance with Design Guidelines as discussed in Chapter V, assuring a quality environment, instill a sense of pride in the neighborhood, and will encourage citizen involvement in the community. The Specific Plan provides areas to recreate in its various parks and open spaces and promotes education by incorporating a school facility that will maintain the high quality education of the local school district for the project's generation of students. These factors will maintain Hercules' desired image as a well-maintained, aesthetically pleasing community, that is inviting to new business activities.

Objective 4 **Target specific industrial businesses and actively promote industrial sector development. Clarify City standards for industrial development to insure that environmental quality standards are maintained and the overall quality of life is not degraded, without effectively, putting the City out of the market for new development.**

Policy 4.1 *The City will assist in the improvement of air quality as one part of "quality of life".*

Project Consistency: The proposed project does not contain industrial development, nor does it contain any development which would potentially create or emit air pollutants; or reduce the quality of air. An amendment from an Industrial General Plan land use designation to Specific Plan, which would include residential, retail, school, parks and open space development, would assure the City that air quality would improve on the site, since the proposed Specific Plan land uses would generate significantly less air pollutants than the previous industrial Pacific Refinery operations.

Objective 5 **Represent and promote local interest in the regional operating environment in order to support economic development.**

Policy 5.1 *The City will pursue annexation of the unincorporated areas within its Sphere of Influence to control development of these areas so the City service capability is not adversely impacted.*

Project Consistency: The project site is completely within City limits of the City of Hercules, and not a part of the City's Sphere of Influence. No annexation is required to implement the New Pacific Properties Specific Plan. The New Pacific Properties Specific Plan will comply, whenever applicable, with the City's goal of promoting "local interest in the regional operating environment" and supporting economic development. The site falls within the City's Redevelopment Expansion Area, which will make funding available to help redevelopment of the Property and others in the City. The resulting improvements will in turn generate revenues contributing towards the economic health of the City.

Objective 6 **Encourage local businesses to employ Hercules residents and target businesses for development which can offer jobs for Hercules residents.**

Project Consistency: The proximity of the 6-acre retail/residential flex parcel to the residential areas within the project has the potential to encourage living-working relationships for the new residents. Additionally, new residents will be available to work in and patronize existing businesses. The proposed elementary school could employ up to 40 full time positions and support a variety of ancillary local businesses.

Objective 7 Use the Redevelopment Agency to promote economic development.

Policy 7.1 The City of Hercules recognizes that redevelopment may offer opportunities for attracting desired business development and that the project scope of the Redevelopment Agency's activities may be modified to enhance economic development.

Project Consistency: The City of Hercules has adopted a Redevelopment Plan to include the New Pacific Properties Specific Plan site. RDA funding will be available to help finance public improvements outside of the New Pacific Properties project site as well as redevelopment of the site, including infrastructure to accommodate the proposed Specific Plan uses. Inclusion in the Redevelopment Expansion Area will also result in increment tax financing for the Agency to use to enhance economic development. This Specific Plan proposes to replace a heavy industrial use with a quality, aesthetic residential and retail project. Again, the site is currently undergoing redevelopment with the closure, dismantling and remediation of the former Pacific Refinery site.

Objective 8 Attract and support new commercial business development to achieve community balance and create jobs.

Policy 8.1 The City of Hercules will identify business types with potential for development in Hercules.

Policy 8.2 The City will actively seek new business development opportunities and will actively promote the community as a place to do business.

Policy 8.3 The City of Hercules will carefully evaluate business development proposals in order to completely assess the proposed project's net impact to the overall fiscal condition of the City.

Policy 8.4 The City shall clearly state its intent, expectations, and evaluation criteria for new development.

Project Consistency: The 6.0-acre Retail/Residential Flex parcel has the potential to allow new businesses to serve the residential development of the New Pacific Properties site, and the communities of Hercules and Rodeo.

Objective 9 Retain and support existing businesses through creation of a positive business environment and through programs to strengthen and promote development of existing businesses.

Policy 9.1 The City of Hercules will develop an on-going business retention and development program to make business assistance resources available to existing businesses in the community and to strengthen the Chamber of Commerce.

Policy 9.2 The City of Hercules recognizes the role and value of development agreements in encouraging desired business development. The City also recognizes the potential benefit to business interests in that a development agreement can provide opportunities to mitigate the risk derived from uncertainty in development process. The City will consider use of a development agreement in situations where it can clearly be demonstrated that a strongly positive benefit will be obtained by the City.

Policy 9.3 The City shall establish and publicize a development review process that includes analysis of all relevant issues in a timely manner, so that development applications can be presented for final action without undue delay.

Project Consistency: It is anticipated that a development agreement will be negotiated with the City for implementation of the New Pacific Properties Specific Plan. Utilization of the Specific Plan entitlement route, rather than a combination of Planned Development Plan, Use and Variance permits, will allow for more flexibility in design of the project, assure comprehensive review of the site and proposed project, and simplify and expedite the City's review process.

I. GROWTH MANAGEMENT

1. Traffic Service Standards

Objective 1 **The City shall maintain traffic operations on streets that are designated as Basic Routes[®] at the standard described below.**

Objective 2 **The City shall participate in the West Contra Costa Transportation Advisory Committee regarding traffic operations on Routes of Regional Significance, and shall help meet the goals and service standards for these routes by implementing the Action Plans for those routes, as adopted by the City and Contra Costa Transportation Authority.**

Objective 3 **New development shall be required to pay its fair share of the cost of improving existing City streets so that compliance with the designated LOS is maintained.**

Objective 4 **New development shall be required to pay its fair share of the cost of improving regional routes so that compliance with the service specified in the Action Plan (when adopted) is maintained.**

Policy 1.1 The City of Hercules will continue to monitor traffic improvement need and will expand the capacity of local streets as required, in order to meet the traffic service standards in the Circulation Element and Growth Management element (when adopted). (From Economic Development Element of General Plan)

- Policy 1.2 Through the Circulation Element, the City will conduct and periodically update computer-based modeling of traffic operations on local streets at full build-out of the community. (From Economic Development Element of General Plan)*
- Policy 1.3 The City will work cooperatively with other agencies to reduce congestion on regional traffic routes without impairing the long term ability of the City to provide services. (From Economic Development Element of General Plan)*
- Policy 1.4 The LOS standards in this Element will be used to evaluate the traffic impacts of new developments, and no application shall be approved which may cause a violation of these standards unless either:*
- a. Improvements that will mitigate the projected LOS impact are programmed in the City's or Redevelopment Agency's Capital Improvement Program; or,*
 - b. A Finding of Special Circumstances has been made for the intersection; or,*
 - c. Improvements will be made by a project sponsor as part of a project.*
- Policy 1.5 The City shall develop and implement a mitigation program to insure that new development pays its fair share of the cost of maintaining adequate operations on the Basic Routes and the Routes of Regional Significance.*
- Policy 1.6 As mentioned above, a traffic study shall be performed for any proposed project that may generate 100 or more vehicle trips during the morning or afternoon peak hour. This traffic study shall be prepared in compliance with the technical guidelines issued by the Contra Costa Transportation Authority, and shall be funded by the project sponsor under the direction of City staff.*
- Policy 1.7 Mitigation measures and conditions of project approval may include payment of fees to fund improvements on Basic Routes or Routes of Regional Significance. Fees for improvements to Basic Routes shall be deposited in a separate City Traffic Mitigation Fund. Fees for improvements to a regional route shall be handled in accordance with the Action Plan for the affected regional route.*
- Policy 1.8 Improvements to Basic Routes shall be programmed through the City's Capital Improvement Program.*
- Policy 1.9 Improvements to a Route of Regional Significance which are sponsored by the City of Hercules shall also be programmed through the City's Capital Improvement Program.*
- Policy 1.10 Improvements to a Route of Regional Significance that are not sponsored by the City of Hercules shall be programmed by the sponsoring agency, and may be acknowledged in the City's Capital Improvements program.*
- Policy 1.11 The City shall continue to participate actively at both the staff and the policy level in the West Contra Costa Transportation Advisory Committee and the Contra Costa Transportation Authority. Participation in these agencies shall include, but may not be limited, to full implementation of adopted Action Plans*

for Routes of Regional Significance in order to promote acceptable traffic movement on these routes. In the event that problems/issues cannot be resolved through discussion among the affected parties, the City will participate in CCTA's conflict resolution process.

Policy 1.12 The City shall report annually regarding compliance with the Measure C Growth Management Program, or as may be requested by the Contra Costa Transportation Authority.

Policy 1.13 The City shall adopt and implement a Transportation Demand Management Program, including the required policies adopted by the Contra Costa Transportation Authority. Such a program maybe jointly implemented in western Contra Costa County by the five cities and the County through the West contra Costa Transportation Advisory Committee.

Project Consistency: Residential roads within the project will be designed to accommodate flows at full buildout of the project. Preliminary traffic reports show that the total estimated amount of trips generated by the proposed Specific Plan Land Use Plan would be significantly less than build out of the existing General Plan allowed Commercial/Industrial density for the site. As well, the study shows that average daily trips generated by the project would be significantly below the current acceptable level of use. These trips would be distributed in the peak hours primarily on the section of San Pablo Avenue within the site, Sycamore Avenue, and I-80. Traffic would also affect Sycamore, and the portions of San Pablo Avenue that are south of the site to Hercules Avenue, and north of the school to Willow Avenue. San Pablo Avenue serves as an alternate route to I-80 and is a Route of Regional Significance.

The New Pacific Properties project also proposes transportation improvements directly related to some of the General Plan's recommendations, including improvements along San Pablo Avenue to increase efficiency and reduce congestion. Nevertheless, since the preliminary traffic report shows that development of the proposed project would generate more than 100 trips during peak hours, a further traffic impact study was required to determine the exact impact on existing and proposed roadways and intersections, and to provide mitigative measures such as signalization, per CEQA requirements.

2. Performance Standards for Capital Facilities

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| Objective 1 | Provide adequate service capacity in public facilities to support the provision of public services to existing and new development. |
| Objective 2 | Develop facilities that meet specific identified needs, while placing strong emphasis on facilities that can adapt to the needs of changing activities and programs. |
| Objective 3 | Coordinate City projects with plans of other agencies who could contribute to the success of projects in Hercules and surrounding areas. |

Policy 1.1 The City shall review all new development plans for conformance with the performance standards in this Element. The City will approve a development application only after making findings that one or more of the following conditions are met:

- a. Assuming participation in adopted mitigation programs, performance standards will be maintained following the project occupancy; or,*
- b. Because of the characteristic of the development project, project specific mitigation measures are needed in order to insure maintenance of standards, and such measures will be required of the project sponsor; or,*
- c. Capital projects in the Capital Improvements Program (or planned by service-providing special districts) will result in maintenance of performance standards.*

Policy 1.2 The City will establish and implement a development mitigation program to insure that new growth is paying its share of the cost associated with the maintenance of these standards.

Policy 1.3 The City shall require all new development to contribute to or participate in the improvement of the above-mentioned facilities and systems in proportion to the demand generated by project occupants and users.

Policy 1.4 Fees collected pursuant to these policies shall be deposited in special funds, and shall be used to support construction or improvements to the above mentioned facilities improvements, as programmed through the City's Capital Improvement Program and budget.

Project Consistency: Significant improvements are proposed to the existing infrastructure to service the New Pacific Properties Specific Plan proposed land uses, and to mitigate impacts from the project. The project sponsor will either privately fund and construct such improvements, such as extension and utility lines; share in the cost of design and construction of infrastructure improvements, such as for extension of water lines; or contribute a pro rata fair share towards needed improvements. Some improvements will be funded from redevelopment district funds, capital improvement projects, and other innovative financing programs.

Policy 1.5 Fire District Service - Fire Station(s) shall be located in the City so that five minutes emergency response time may be achieved by first response units for 90% of all emergency calls. Fire Stations shall be sized to accommodate a minimum of two (2) engines/trucks and three-person, 24-hour crews.

Project Consistency: Existing source facilities are sufficient to service the Specific Plan area and provide necessary fire flows. New distribution lines will be constructed to service the site specific needs. The site falls within the Rodeo-Hercules Fire District and within five minutes from Station 75, located in Rodeo, and Station 76, located in Hercules. Existing facilities will be sufficient to serve the proposed development. Service requirements are reviewed under the project environmental review.

Policy 1.6 Police - Office and supporting facilities shall be provided in a central, headquarters facility. Communication equipment (including repeater stations) shall be provided to allow communication between dispatch and police units throughout the service area.

Project Consistency: The Hercules Police Department operates out of City Hall at 111 Civic Drive. Currently, the department includes investigative and administrative offices, evidence storage, detention area, and related facilities. Dispatch of units is provided in conjunction with the City of Pinole Police Department. Response time to the Specific Plan area is estimated to be 3-4 minutes for an emergency, and within 15 minutes for non-emergencies, which is considered acceptable by the Police Department. In addition, some private security services may be contracted by individual commercial businesses which may chose to located within the Retail/Residential Flex zone of the Specific Plan area. The Police Department does not anticipate problems with providing public police protection to the Specific Plan area.

Policy 1.7 Parks

- Neighborhood Parks: A minimum of 1.75 acres of neighborhood parks shall be provided for each 1,000 residents.*
- Community Parks: A minimum of 3.25 acres of community park space shall be provided for each 1,000 residents.*
- Open Space: A minimum of 34 acres of open space (public and private combined) shall be provided for each 1,000 residents.*

Project Consistency: The New Pacific Properties Specific Plan estimates a build-out of up to 880 dwelling units on site, generating a population of approximately 2,746 (using a population multiplier of 3.12). As discussed in detail in Chapter II, Section B. 21, *Parks, Open Space and Conservation*, the project will provide 14.3 acres of park, 3.6 acres of natural open space, 4.1 acres of roads and 25.6 acres of associated open space, and 9.0 acres of enhanced parkway, which exceeds the minimum City park requirements.

Policy 1.8 Sanitary Facilities (Sewer) - Capability to transmit and treat waste water from all residential and non-residential developments to standards set by the Regional Water Quality Control Board.

Project Consistency: Several options are available regarding sewer service for the New Pacific Properties Specific Plan area. The most desirable involves connection to the City=s existing wastewater treatment facility located in the City of Hercules. Other options include creation of a new treatment plant to be located elsewhere on the property, connection to the Hercules/Pinole wastewater treatment facility, connection to a new Joint Power=s Authority facility contemplated in the unincorporated community of Rodeo to the north, or connection to the Rodeo Sanitation District.

Policy 1.9 Storm water Drainage - Meet Federal NPDES requirements for storm water run-off.

Project Consistency: The City has adopted a Master Plan of storm drains, in accordance with National Pollution Discharge Elimination Systems (NPDES) requirements. The project is

designed to incorporate adequate storm drainage facilities. The overall general existing drainage pattern will be retained in conjunction with development of the New Pacific Properties Specific Plan. Storm water will be primarily directed into a closed drainage system, and discharged into San Pablo Bay at the existing outfalls, or through additional engineered structures.

Policy 1.10 Water - Capability to provide potable water supply to all residential and non-residential developments.

Project Consistency: Water to the Specific Plan area will be supplied by the East Bay Municipal Utility District. Extension of the water system will be needed to service the New Pacific Properties Specific Plan area.

Policy 1.11 Flood Control - All new structures shall be located outside the Flood Zones A & B as designated by the Flood Insurance Rate Map (prepared by the Federal Emergency Management Agency, FEMA); or, insure that the finished floor elevation is at least 1 foot above the flood elevation as determined by FEMA.

Development of any property shall not significantly increase the flooding potential at downstream areas, or otherwise significantly impact or aggravate a flooding problem at downstream properties.

Project Consistency: The New Pacific Properties Specific Plan area falls outside of Flood Zone A & B, as designated by the Flood Insurance Rate Maps. The site falls within Flood Zone C, defined as an area of minimal flooding, as delineated on the Federal Flood Insurance Rate Map Community Panel Number 060434 0008 B. No mitigative measures are required for construction or development within Zone C. Furthermore, the project will not result in any increase in flooding downstream nor aggravate a flooding problem downstream.

