NEW TOWN CENTER MARKET TOWN

Hercules, California

INITIAL PLANNED DEVELOPMENT PLAN

5 June 2008

NEW TOWN CENTER MARKET TOWN

Hercules, California

PREPARED FOR

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Owner

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Developer

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INTRODUCTION

PURPOSE AND INTENT

The City of Hercules' General Plan and zoning code contain a "New Town Center" district along Sycamore Avenue, San Pablo Avenue and Willow Avenue. The city's intent is for the area to become a compact, mixed-use, pedestrian-friendly, transit-oriented community gathering space with walkable streets and safe public spaces in the center of town. The New Town Center is intended to be a local and regional destination. The site's proximity to a major highway and future transit, in combination with the potential variety of retail opportunities will ensure the vitality of this New Town Center concept. In addition, the project will be visually attractive, contain architectural variety, and be constructed of high quality building materials.

The 35-acre New Town Center district is comprised of seven parcels, each with a different public or private owner. The City expects that development of the district will be a phased process as each parcel is developed.

The subject of this Initial Planned Development Plan (Initial PDP) is the development of the 6.62 acre parcel, called Market Town or PNR parcel (also referred to as the "Plan Area" throughout this document). It will be the first parcel developed in the New Town Center district. The site will be developed as one parcel, yet will be broken up into building areas for urban design purposes. In line with the city's intent for the New Town Center, the parcel will be redeveloped as a mixed-use project, with three major areas containing compact residential, retail, office, and parking uses. The plan program proposed in this document includes up to 400 new residential units, up to 60,000 square feet of retail space, and up to 80,000 square feet of office space.

The Hercules New Town Center Market Town

HOW TO USE THIS DOCUMENT

This Initial Planned Development Plan is designed for use by the developer, architects, designers, builders, planners, and regulators in their respective roles in developing buildings and places on the PNR parcel.

The Urban Design Plan (Section B) defines the basic parameters of the proposed program for developing the PNR parcel including: the site plan, land uses, and a planning example. Section B also defines the setback, parking and lot landscape requirements; circulation and access; building heights; environmental impacts; the anticipated implementation schedule; and the plan review process. It illustrates the relative scale of the residential and commercial uses and their relationship to one another in creating a human-scale pedestrian environment within the district. This section defines the basic parameters of the development that can be located on the site. Within these parameters, a wide variety of buildings can be designed, all of which meet the intent of this Initial PDP.

Section C defines conceptual architectural solutions for buildings designed for the Plan Area. The architectural parameters addressed in Section C include building massing, wall sections, the five architectural styles selected for the development (Bay Area Eclectic, Bay Area Art Deco, California Spanish Revival, Bay Area Traditional Industrial, and Bay Area Modern Industrial), and site signage. The description of the essential elements and character of each of the five architectural styles, their attendant details, and materials is intended to ensure that buildings in the Plan Area relate to well-respected architectural precedents in Hercules and the surrounding region, and impart a harmonious sense of place to this new district.

Development standards set forth in this Initial PDP are intentionally clear and direct. They are intended to guide the more detailed design of buildings and other site improvements that will be delineated in a Final Planned Development Plan (Final PDP) and will provide clear guidance to City of Hercules staff as they review the Final PDP for the site. The guidelines contained in this Initial PDP are meant to assure that these detailed designs will comply with the pertinent City of Hercules plans and zoning ordinances so that the necessary approvals and entitlements are granted. At the same time, as a design tool this Initial PDP offers a modicum of flexibility in designing buildings and a program that can respond to changing marketplace preferences and pressures over time. This flexibility is found throughout the document and defines the parameters of what is allowed and not allowed on the site. Unlike other Initial PDPs, this document does not define the specific location of each and every building on the site. Rather, it provides the rules and regulations for how and where development can occur on the site.

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CITY-WIDE REGULATORY CONTEXT

There are three regulatory documents relevant to the development of the Plan Area: the Hercules General Plan, the Hercules Zoning Ordinance, and the Central Hercules Plan Regulating Code (Chapter 28 of the Zoning Ordinance, hereafter, the CHP Regulating Code). These are discussed in turn below. The development outlined in this Initial PDP is designed to meet the guidelines and requirements of the New Town Center as it is identified in the amended General Plan and Zoning Ordinance.

CITY OF HERCULES GENERAL PLAN

The General Plan includes the New Town Center land use designation. This designation was added to the General Plan in 2008 to allow for the creation of a mixed-use town center at the center of the city. The New Town Center land use designation, which includes the PNR parcel, is intended to contain a diverse mix of residential, office and retail uses and follows the general intent laid out in the CHP Regulating Code.

CITY OF HERCULES ZONING ORDINANCE

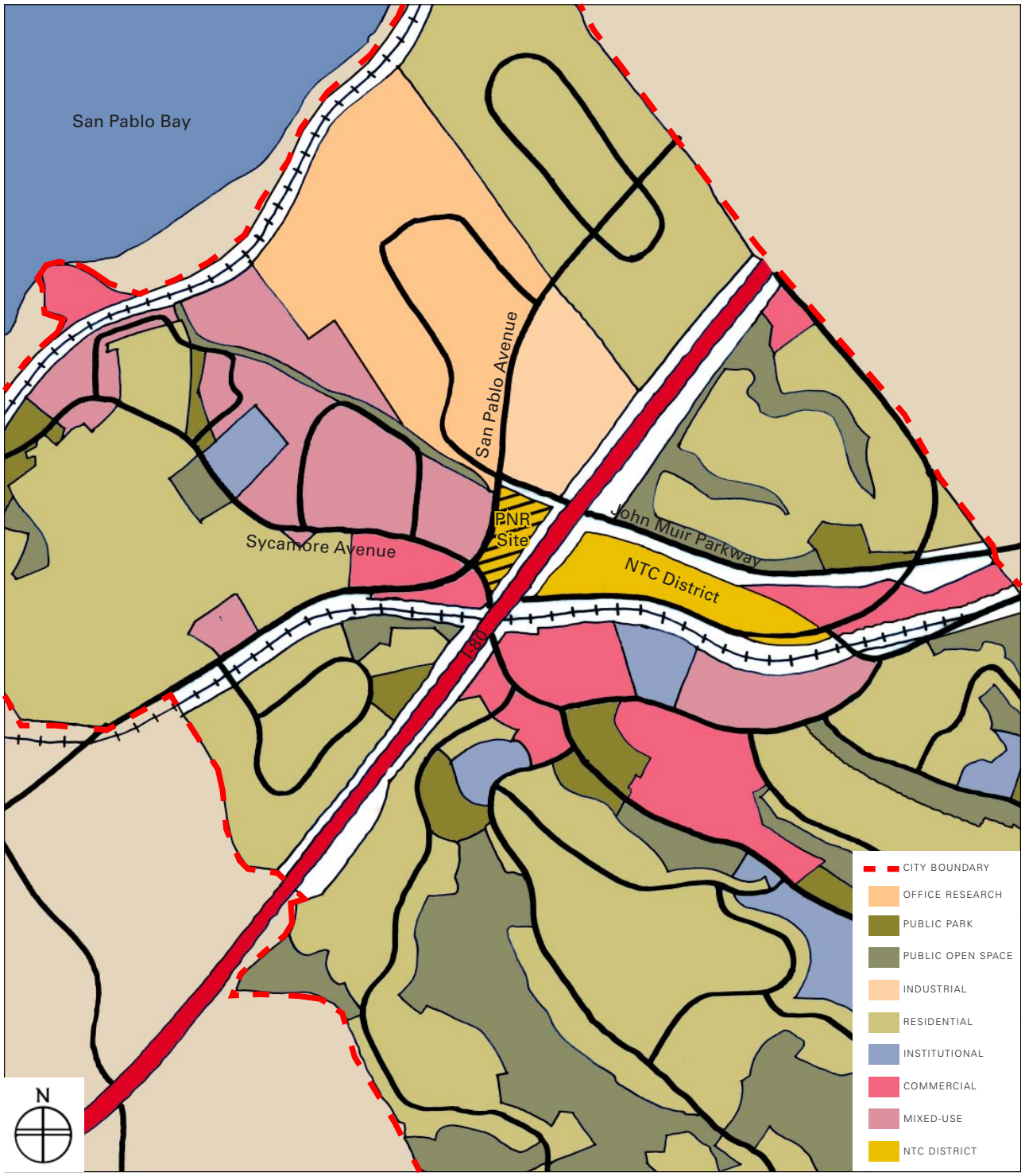
The PNR parcel is part of the New Town Center zoning district. This area is coterminous with the New Town Center land use designation. The New Town Center zoning district provides the development regulations for projects, including performance standards, land use regulations, allowable uses, and property development regulations.

The New Town Center zoning district requires that all projects proposed of five acres or more within the district prepare a Planned Development Plan, per Chapter 48 of the Zoning Ordinance. This Initial PDP, along with a Final PDP to be submitted in conformance with this Initial PDP, is in response to that requirement.

Affordable housing will be required based on a Development Agreement between the City/ Agency and the Developer or that the Developer will provide affordable housing per city codes at his or her discretion.

CENTRAL HERCULES PLAN

The Plan Area lies within the geographical area originally covered by the CHP Regulating Code. The New Town Center zoning district requires that the City Council make a finding, prior to approving this Initial PDP, that the proposed plan is consistent with the planning and design intent of the CHP Regulating Code and complements the character of the planning and design in the balance of the area covered by the CHP Regulating Code. It is the intent of this Initial PDP to provide the City Council with sufficient information to be able to clearly make such a finding with respect to the proposed development.



Existing land-use for the parcels around Hercules New Town Center (NTC) District

EXISTING ACCESS AND CIRCULATION

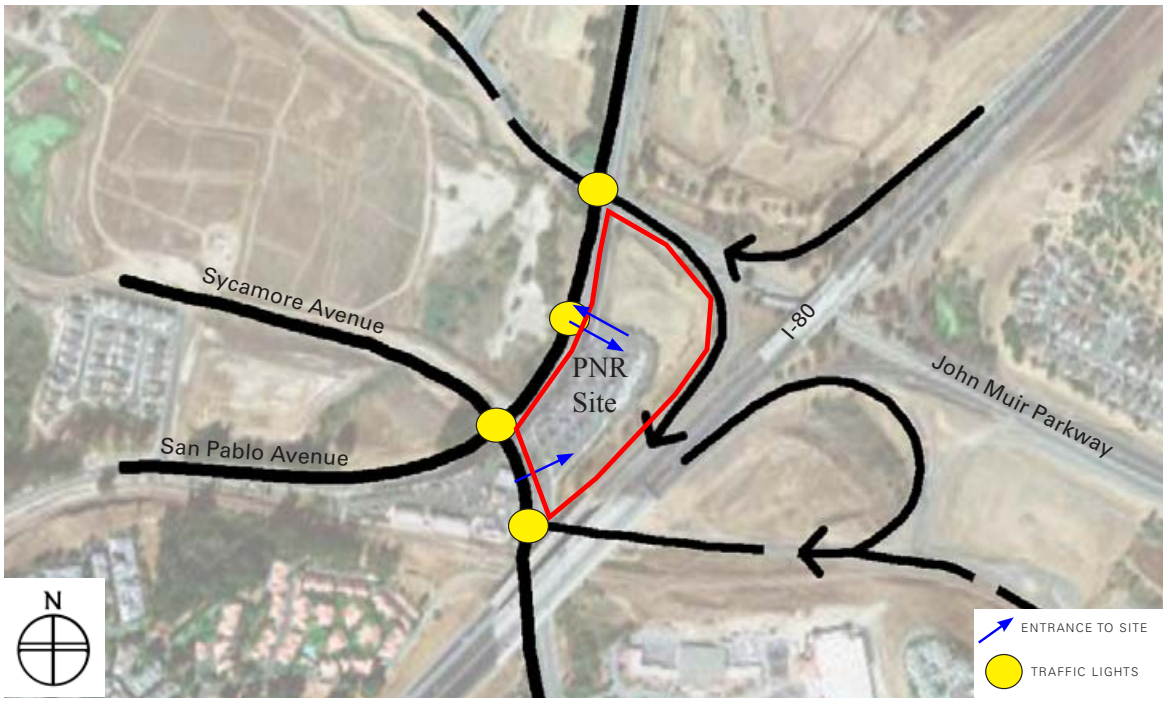
The PNR parcel of New Town Center is sited at an important crossroads within Hercules. The parcel is bound by major arterials on all sides, including San Pablo Avenue to the west, I-80 to the east, John Muir Parkway to the north and Sycamore Avenue to the south. This makes access and circulation around and through the site an important factor to resolve in order to ensure a positive experience for residents and visitors.

Regarding traffic, the General Plan addresses traffic circulation, scenic routes, and public transit. Within these broad categories there are two conditions affecting the development of this parcel.

The first factor is adjacency to the two freeways, I-80 and Highway 4, which intersect in a complex of connector and entrance ramps at the northeast corner of the PNR parcel. These freeways, which are the main motor routes connecting Hercules to the Bay Area and elsewhere in California, constrain the northern and eastern boundaries of the site but offer a unique opportunity for the site to be a prominent, attractive gateway.

The second factor is San Pablo Avenue, which is the major, historical artery connecting Hercules with towns to the north and south. San Pablo Avenue also provides bypass capacity for I-80 freeway traffic during periods of freeway congestion. San Pablo Avenue is designated as a Route of Regional Significance in the Congestion Management Plan adopted by the Contra Costa Transportation Authority. San Pablo Avenue is heavily traveled in peak periods, mainly by vehicles entering or leaving the freeway system. Multiple traffic lanes and relatively high traffic volumes make the site's western and southern edges unpleasant for pedestrians and cyclists, but also establish the site as one of the best, obvious commercial locations in Hercules.

Currently there are two driveway access points into the PNR parcel - one on Sycamore Avenue and one on San Pablo Avenue. The PNR site's layout constrains these two access points to their current locations.



Existing conditions diagram indicating current traffic patterns and ramp configuration adjacent to the PNR parcel. Also shown are the existing traffic signals and access points.

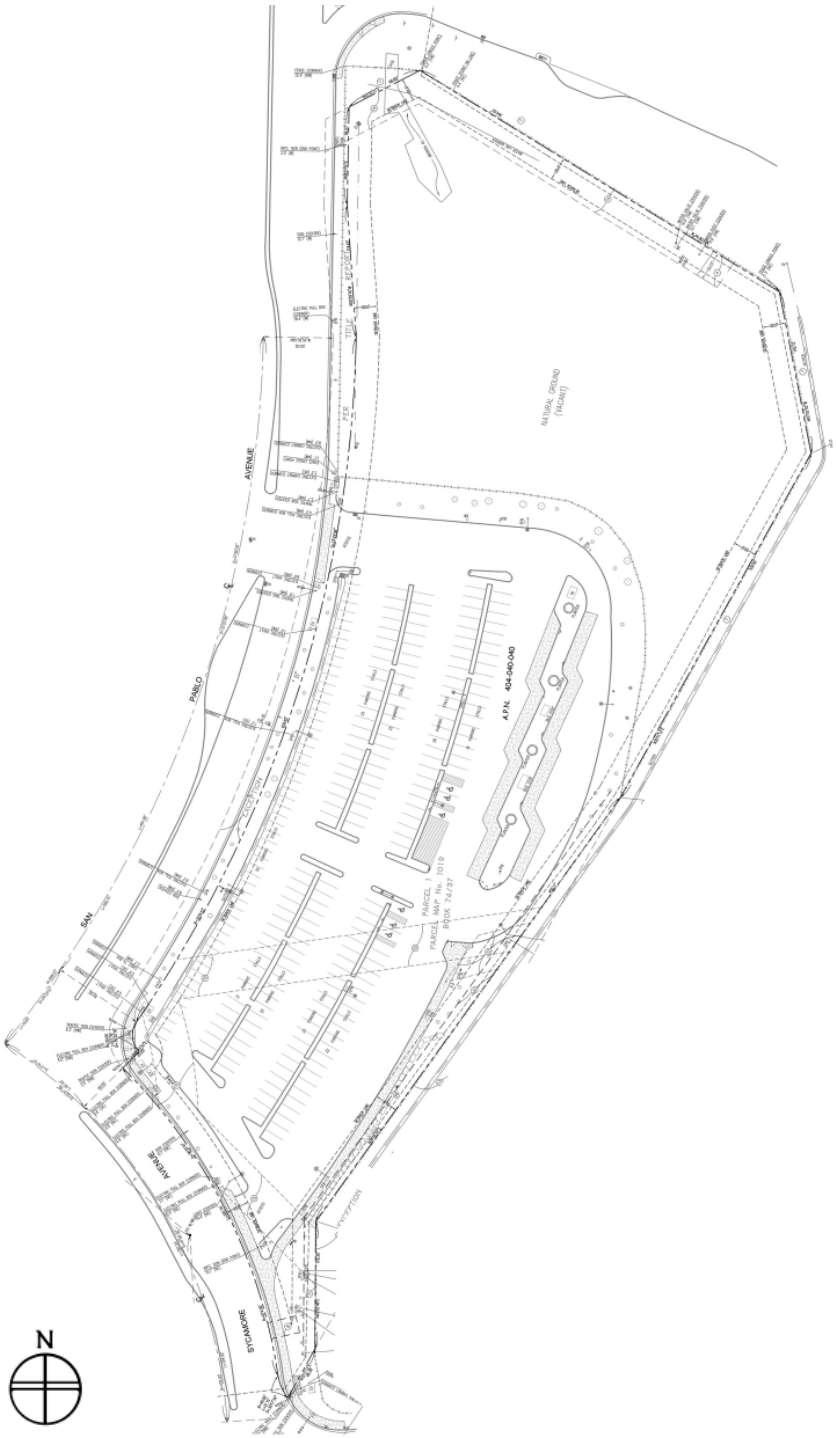
PNR PARCEL

EXISTING CONDITIONS

The PNR parcel of Hercules New Town Center is currently being utilized as a surface parking lot for transit users. Access into the site is limited, but available at strategic locations off of San Pablo and Sycamore Avenues.

To the north, the PNR parcel is bounded by John Muir Parkway (Highway 4) which connects nearby cities in the east to the local neighborhoods within Hercules. Similarly, along the south boundary, Sycamore Avenue provides a connection between the existing commercial and residential opportunities in the eastern portion of Hercules with new residential developments in the western portion. Sycamore Avenue also provides access from the PNR site to the Waterfront District and San Pablo Bay.

The eastern boundary of the PNR parcel is Interstate 80 which runs from the San Francisco Bay Area through Hercules and into Sacramento. The western boundary is San Pablo Avenue, a major arterial that connects Hercules to towns north and south. San Pablo Avenue and John Muir Parkway often experience heavy congestion around the site due to traffic stacking for the I-80 entrance ramps.



Engineer's survey of the existing conditions of the PNR parcel (not to scale)

Site boundary on aerial

URBAN DESIGN PLAN

AREA STRUCTURE

One of the goals of the New Town Center zoning district is to create a more dynamic shopping and living experience that also allows for a diverse mix of uses within the site. Rather than designating specific uses for each area within the PNR parcel, the New Town Center zoning category allows and encourages multiple combinations of uses and building types on each individual parcel. Prior to development, a specific alternative must be approved by the City of Hercules as part of a Final PDP.

This parcel will be divided into building areas for urban design purposes. The area structure of the PNR parcel for the Hercules New Town Center is shown as divided into two areas: A and B. To allow for additional flexibility at the corner, Area B is divided into two subareas: B1 and B2. The area structure on the site is shown on the following page. The area structure was created to provide a more refined direction and regulation for the uses and other site requirements (such as parking, height, landscape, etc.) for sub-areas of the site. Thus, throughout this Initial PDP, specific guidance is provided for both areas of the site.

Additionally, development of each area may proceed separately from one another. The Planning Commission may also allow separate Final PDP's to be created for Area A and B separately.

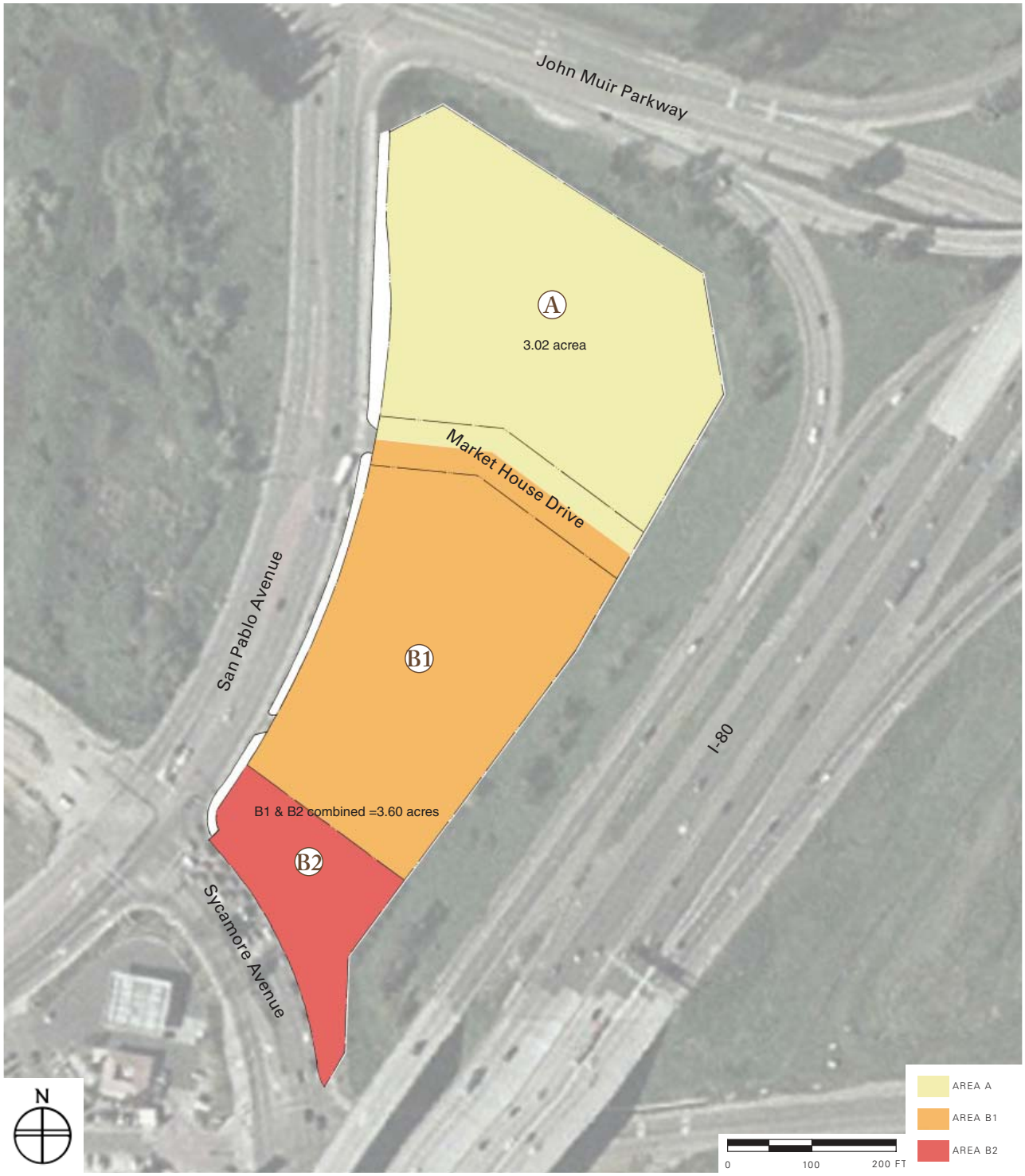


Diagram showing the area divisions in the PNR parcel

PROGRAM AND BUILDING USES

The PNR parcel has the possibility for multiple site plan alternatives with a gross acreage of 6.62 acres. The NTC District zoning and two area structure allows for a flexibility of uses and a program that is mixed-use in nature. As defined in the New Town Center General Plan land use designation, the PNR site shall have an overall residential density of between 30 and 65 units per acre and the FAR shall be between 0.1 and 2.0 for the entire parcel. The maximum amount of development allowed on the PNR parcel based on this Initial PDP and its associated Environmental Impact Report is as follows: 400 residential housing units (in 360,000 square feet); 80,000 square feet of office space; 60,000 square feet of retail space; 600 residential parking spaces and 560 non-residential parking spaces. Development shall not exceed this program without revising the Initial PDP. The maximum amount of development on the site and in each Area (A and B) is presented in the table on page 17. The program may increase by 20% without amending the IPDP so long as the total for the site is not exceeded.

Areas A, B1, and B2 allow residential use as well as non-residential uses, including retail and office space. However, Area A will generally remain residential while Areas B1 and B2 may be a combination of destination retail, office spaces and residential uses. The Initial PDP also allows for flexibility of uses on any level. More specific information on each Area is presented below.

AREA A

Area A will contain the majority of the parcel's residential units. Residences may be in the form of multi-family and or live/work units and may be distributed throughout the area. A small amount of non-residential uses may be allowed on Area A. The non-residential uses shall be associated with the live/work units or small-scale retail. The non-residential uses shall be no more than 15 percent of the total building square footage of Area A. Non-residential uses are limited to the first floor of Area A.

AREA B

Area B will contain the majority of the commercial activities for the parcel. Area B1 may be mixed-use by providing residences above ground-floor retail. Live/work units and/or lofts may also be incorporated into area B1. Area B1 will be the center for retail activity within this new district with some potential retail in Area B2. Retail uses should focus on those uses that provide neighborhood-serving amenities such as grocery stores, cafes, dry cleaners, and restaurants. Temporary or seasonal retail uses, such as cart vendors and farmers markets, may also be located in the public spaces of this area. The temporary uses shall not be counted toward the square footage of the total development program.

Area B2 shall be primary non-residential with a focus on office uses. The ground-floor space of Area B2 may be used for retail uses. These uses should be complementary to both the office uses and the retail uses on Area B1. Acceptable uses include, but are not limited to, banks and other financial service institutions and professional service retail such as travel agencies.

MAXIMUM PROGRAM BY AREA

	ACREAGE	TOTAL BUILDING (SQ. FT.)	RESIDENTIAL (SQ. FT.)	RESIDENTIAL (UNITS)	GROUND FLOOR RETAIL (SQ. FT.)	OFFICE (SQ. FT.)
AREA A	3.02	300,000	300,000	250	0-20,000	0-20,000
AREA B	3.60	200,000	150,000	150	20-60,000	60-80,000
MAX ALLOWED PNR PARCEL	6.62	500,000	360,000	400	60,000	80,000

**Any combination of uses should not exceed the Total PNR Parcel amount*

	ACREAGE	MAX. DENSITY/ ACRE	MAX COMMERCIAL FAR	MAX. FAR (ALL USES)
GROSS	6.62	60 units/acre	0.21	1.75
NET	5.44	73 units/acre	0.25	2.11

**All internal drives are private drives and there are no public parks or public open spaces included in the PNR Parcel. The acreage above, therefore, represents the entirety of the parcel*
**All densities/acre and FARs in the chart are maximums that will not be exceeded*
**Maximum gross density/acre was calculated by taking the total PNR Parcel area, including the acreage of open space, setbacks, easements and road right-of-ways of the Parcel, and dividing it by the maximum number of residential units allowed on the Parcel.*
**Maximum Net Density/Acre was calculated by taking the total PNR Parcel area, minus gross setbacks, and dividing it by the maximum number of residential units allowed on the Parcel.*

SETBACK REQUIREMENTS AND BUILDING PLACEMENT

The Initial PDP identifies a series of lines that define the placement of buildings in relationship to property lines, new internal access drives, and public spaces. Setback requirements and Build-to Lines are set for each area of the plan to ensure that building placement promotes and encourages a high-quality, pedestrian-oriented district. Building footprints are allowed in all areas outside of these setbacks and Market House Drive. Building Setbacks and Build-to Lines are established for each plan area as follows:

AREA A

The Facade of buildings facing San Pablo Avenue in Area A shall be placed at a Build-to Line set at 7 feet from the back of an improved sidewalk and planting strip along San Pablo. A minimum of 30% of the building frontage along San Pablo shall be built to this Build-to Line.

The Facade facing John Muir Parkway in Area A shall be set back a minimum of 26 feet from the property line along the Parkway.

The Market House Drive Facade of buildings in Area A shall be set back a minimum of 0 feet from the back of an improved sidewalk along any new internal access drives. There shall be a minimum of 40 feet between buildings along this drive.

The Rear Facade of buildings in Area A shall be set back a minimum of 10 feet from the rear property line adjacent to I-80.

Upper floor terraces and balconies may extend up to 6 feet across the front Build-to Line or side street setbacks. Upper floor bay windows may extend up to 3 feet across the front Build-to Line or side street building setbacks. Porches and stoops shall not encroach across any Build-to Line or setback.

The buildings on Areas A and B1 may be attached on the upper floors only in the area identified in the figure on the following page.

AREA B

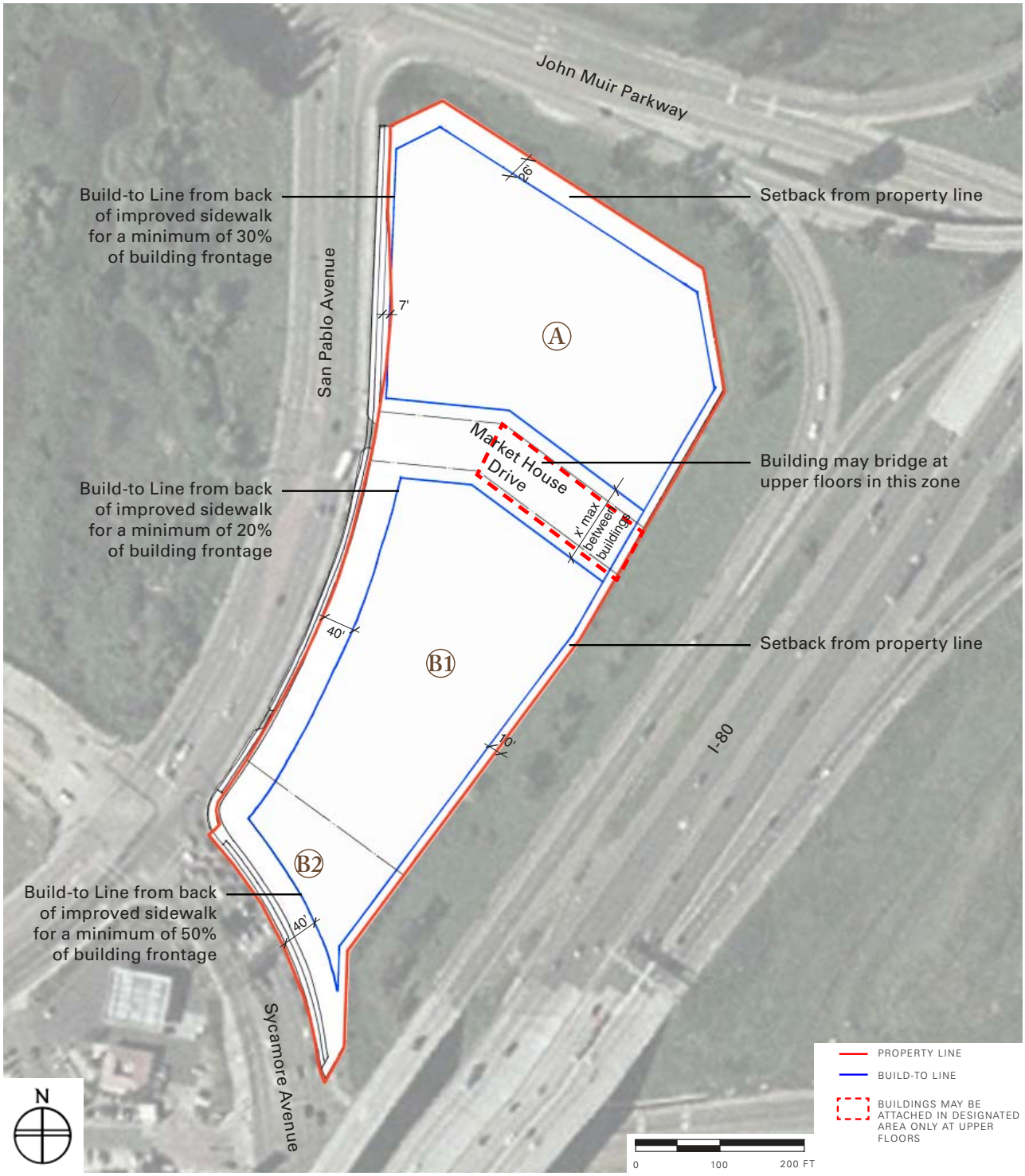
The San Pablo Avenue Facade of buildings in Area B shall be placed at a Build-to-Line set at 40 feet from the property line along San Pablo. A minimum of 20% of the building frontage along San Pablo shall be built to this Build-to Line. A maximum of 28 surface parking spaces will be allowed in front of the San Pablo Avenue Facade in parcel B.

The Sycamore Avenue Facade of buildings in Area B shall be placed in a setback zone partially determined by an existing easement. The Sycamore Avenue Facade of buildings must be placed a minimum of 25 feet from the property line.

The Market House Drive Facade of buildings in Area B shall be set back a minimum of 0 feet from the back of an improved sidewalk along any new internal access drives.

The I-80 Facade of buildings in Area B shall be set back a minimum of 10 feet from the rear property line adjacent to I-80.

Pavilions, gazebos, or other free-standing, open-air structures may be placed beyond the Build-to Line within Area B if pedestrian access to such structures is provided. Canopies, awnings, and galleries may encroach up to 10 feet across any Build-to Line at the ground floor. Upper floor terraces and balconies may extend up to 6 feet across any Build-to Line or side street building setbacks. Upper floor bay windows may extend up to 3 feet across any Build-to Line or side street building setbacks. Porches and stoops shall not encroach across any Build-to Line.



Setback diagram for the PNR parcel

PARKING AND LOADING REQUIREMENTS

Parking shall be distributed throughout the PNR parcel. The quantity provided for each use shall comply with Chapter 32.3 of the City of Hercules Zoning Ordinance. As seen in the diagram to the right, parking shall only be allowed in the shaded portions of the site. Underground structured parking is also allowed anywhere within the site but shall be a minimum of a half-story below grade.

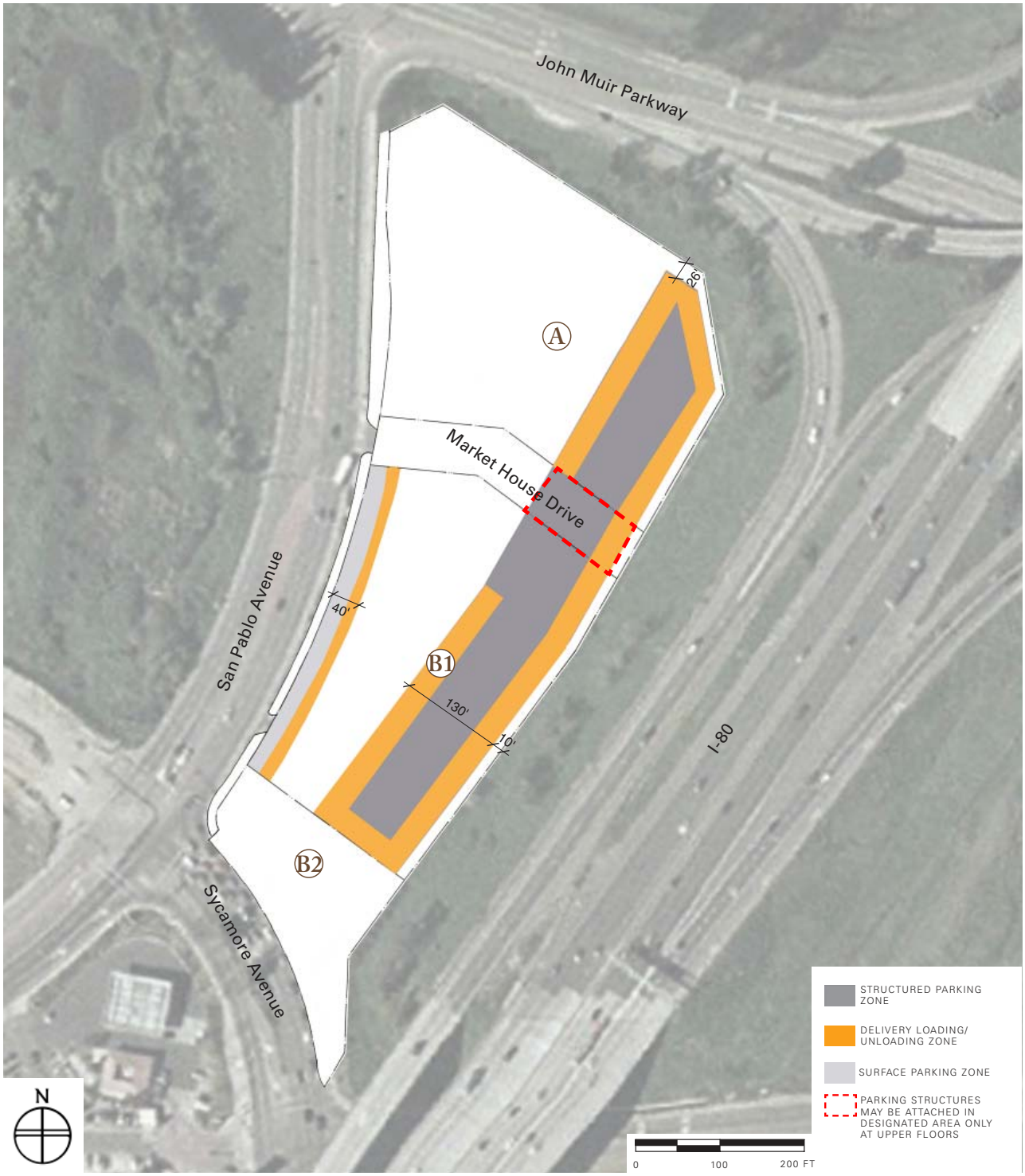
Above-ground structured parking shall be allowed in Areas A and B1. Above-ground structured parking shall not be allowed in Area B2. Parking structures in Area A shall have a 10-foot setback from the rear property line and a 26-foot setback from John Muir Parkway. The parking structure for Area B1 shall have a 10-foot setback from the rear property line.

Parking garages shall be visually attractive and screened from views from San Pablo and Sycamore Avenues. Elsewhere on the site, parking structures shall also be screened from public view wherever possible. Access to the structured parking facilities shall be from driving lanes within the parcel. There is no required setback between the structured parking and the driving lane. The parking garages on Area A and Area B1 may connect above grade as shown on the opposite page.

No parking is allowed along Market House Drive. Within Area B1, all parking must remain within a 40-foot setback from the front property line.

Parking within a single structure may be shared by numerous uses. In other words, each use does not need its own parking structure. Further, shared parking to reduce the total number of parking spaces is encouraged on the site and shared parking agreements may be created between the uses on the PNR parcel and uses on parcels, as approved by the City's Parking Authority.

A loading zone shall be located within the structured parking facilities or as part of the surface parking lot in Area B1.



Parking diagram for the PNR parcel

CIRCULATION AND ACCESS

There shall be a maximum of three external access points to the site. One entrance to the site shall be located on Sycamore Avenue. This entrance shall remain in its current location and shall allow for right-in and right-out turns into and out of the site. Two access points are allowed from San Pablo Avenue on to the site. The primary access point shall remain at the current location. This access point shall allow left-and right-hand turns into and out of the site. A secondary access point along San Pablo Avenue may be located between the primary access point and Sycamore Avenue. This access point shall allow for right-out turns only. Left-hand turns into or out of this access point are not allowed. While the exact width of each access point will be described in the Final PDP, all utilities and drainage must be accommodated within these access ways. All internal drives are private drives, not public streets.

In order to provide a high-quality urban experience, the pedestrian environment shall provide safe and direct pedestrian access to, along and within the site. Sidewalks that are a minimum of 7 feet wide are required along San Pablo Avenue and a minimum of 5 feet wide along Sycamore Avenue, where buildings are directly adjacent to the street. Within the site, sidewalks of at least 5 feet wide on both sides shall be located along the internal driveway. A minimum of 2 crosswalks shall be provided across the driveway separating Area A and Area B1. One crosswalk shall be located along San Pablo Avenue and the other shall provide a direct and safe connection between the parking garage on Area A and the town square/retail area on Area B1. Additionally, the final project must provide direct and safe pedestrian access from the parking structures to all the uses on the site.

The street sections below illustrate the three conditions described:

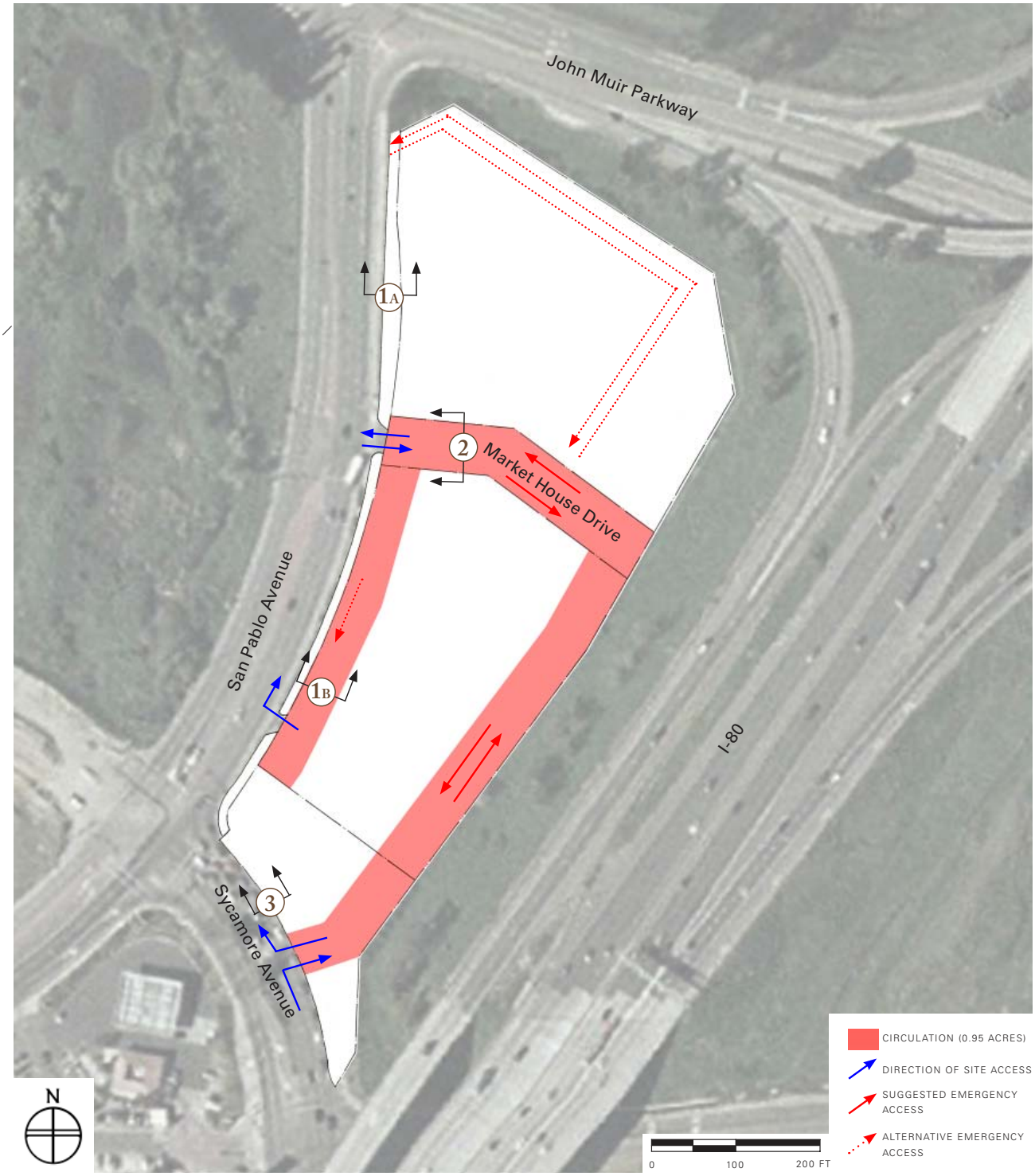


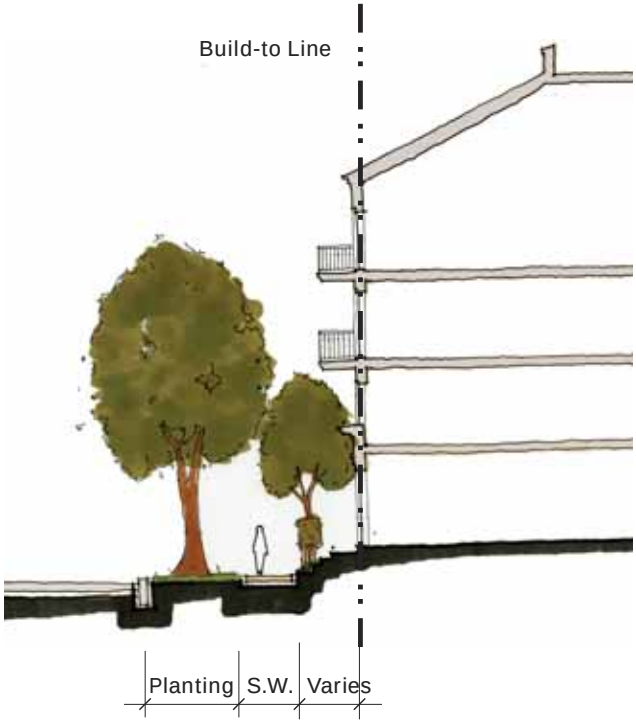
Diagram locating street types for the PNR parcel

TYPE 1-A:

Section along San Pablo Avenue at Area A.

Minimums required:

- Drive lane: none
- Planting: varies
- Sidewalk: 5 feet

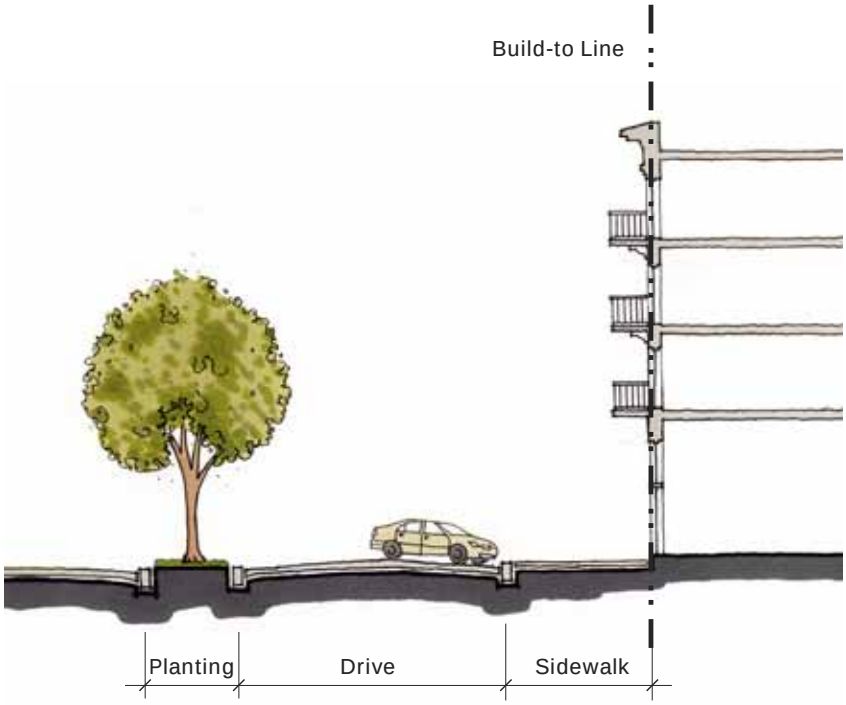


TYPE 1-B:

Section along San Pablo Avenue at Area B1, provides on-street parking along retail frontage, and moves traffic through the front of the site.

Minimums required:

- Drive lane: 11 feet
- Planting: 6 feet
- Sidewalk: 12 feet



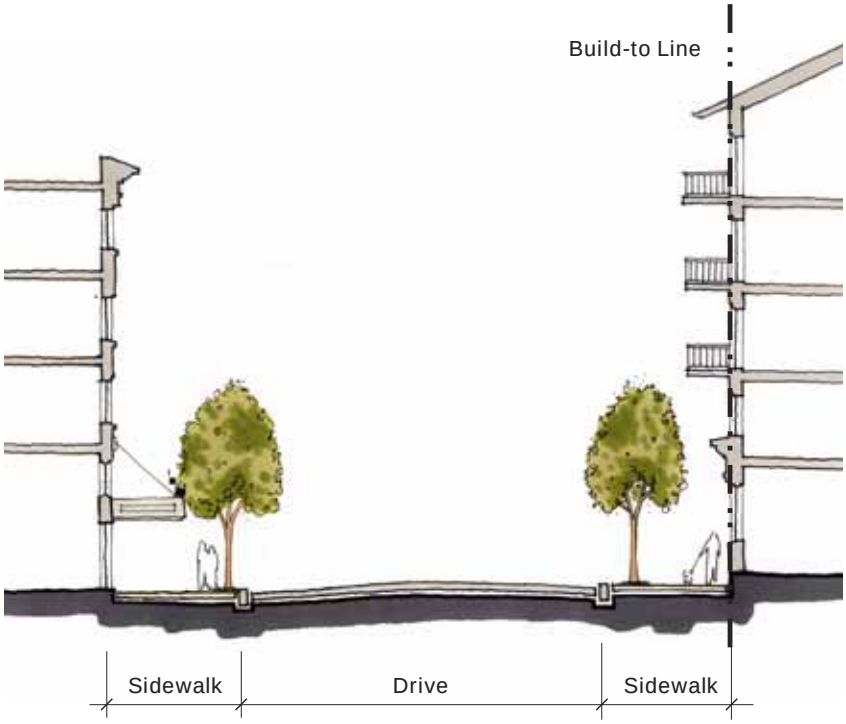
Note: buildings shown are for illustrative purposes only and do not reflect a specific design

TYPE 2:

Provides access east and west through the site from San Pablo Avenue and allows access to parking garages.

Minimums required:

- Drive lane: 22 feet
- Planting: none
- Sidewalk: 4 feet

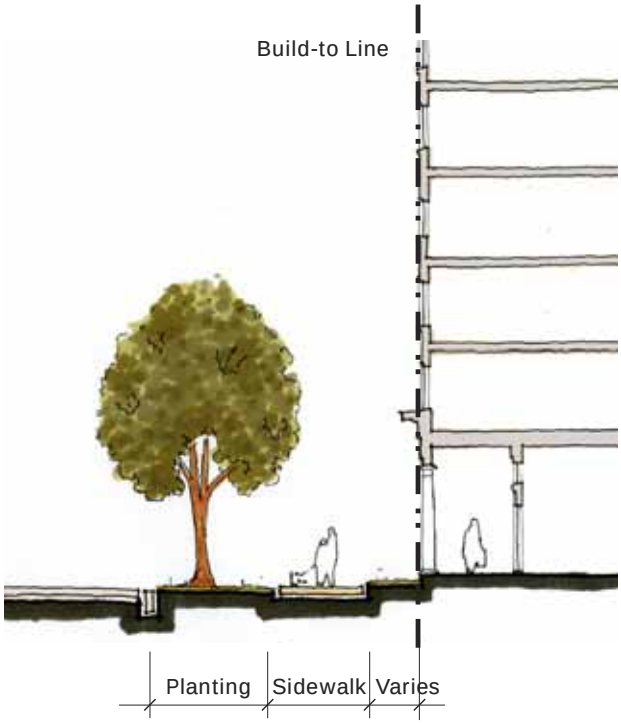


TYPE 3:

Section along Sycamore Avenue at Area B2.

Minimums required:

- Drive lane: none
- Planting: varies
- Sidewalk: 10 feet



SUSTAINABILITY AND GREEN BUILDINGS

The NTC District project is committed to meeting the goals of sustainable development and green building and becoming a model for future development in the City of Hercules. In pursuit of environmental sustainability, the NTC District project has 7 sustainable development goals. These are described below:

1 Reduced Driving Through Land Use and Transportation Decisions: The project will be located in the center of town and with good transit access. The project will also contain a mix of uses to serve the needs of residents of the NTC District and the city and be at a density to support transit use.

2 Climate Change and Energy Conservation: Design the project to use less energy for the buildings development and operations and also create an environment where residents drive less to meet their daily needs, thus reducing vehicle use per capita.

3 Environmental Design/Site Planning: Create a building that works in collaboration with its site and contributes to its ecological functioning. To fulfill this goal, the project will pursue innovative stormwater management techniques and include some native landscaping.

4 Street and Public Space Design: Design streets and public space as aesthetically pleasing places that encourage walking, gathering, and lingering.

5 Indoor Air Quality: Ensure that the buildings achieve a high level of indoor air quality. This goal will be achieved by selecting materials that result in a non-toxic indoor air environment (such as low-VOC paints, non-toxic carpet and low-emitting adhesives). Recycled, Reused and Green Materials: To the greatest extent feasible, the project will use materials that are recycled, reused, or produced with a low-impact method, construction waste will be recycled and the buildings will contain on-site recycling facilities.

6 Water Efficiency: The project will achieve a high level of water efficiency and reduce water use compared to the average use of water by Hercules residents. To achieve this goal, the project will explore the use of the following techniques: xeriscaping in select locations, capturing rainwater and recycling wastewater for irrigation, and water-efficient plumbing in the buildings.

7 Enduring Construction Quality: The project will be constructed with high quality materials and with a design that is enduring. This will encourage future generations to preserve and enhance buildings rather than tearing them down after the depreciation period.

In order to meet the goals of sustainability and green building, the NTC District project will complete one of the following independent certification processes:

- Build all buildings (except parking garages) to LEED for New Construction (LEED-NC) Silver certification or higher certification.
- Achieve LEED for Neighborhood Development (LEED-ND) certification of gold or higher.
- Achieve certification from another similar, recognized rating system that is equivalent to the standards identified above.

LOT LANDSCAPE REQUIREMENTS

The PNR parcel includes three types of open space: active open spaces, private balconies, and a landscape buffer. Those three types of spaces are illustrated in the diagram to the right.

ACTIVE SPACES

Outdoor space is a key feature of this development. The site shall include three active spaces: one within the residential building in Area A; a second space at the center of the site in Area B1, to be known as the Town Center Plaza; and the third, located at the intersection of San Pablo and Sycamore Avenues in Area B2, will anchor the master plan. Each space will be privately owned and operated but all spaces shall allow public access during, at minimum, daylight hours. Thus, these areas shall serve as and contribute to the city's parks and open space network.

The space in Area A shall be treated as a courtyard that provides outdoor space for residents of Area A. This space shall be primary for residents of the site but shall be open to the public during daylight hours. A fence and gate may separate the courtyard from pedestrian rights-of-way. However, the gate shall be open during daylight hours. Appropriate signage shall be provided that announces the public uses of the courtyard. The courtyard shall be a minimum of 10,000 sq.ft., depending on final occupancy. The smallest north-south dimension shall be no less than the average height of the buildings surrounding the courtyard. Refer to the Building Heights and Views pages to find permitted heights .

The Town Center Plaza on Area B1 will be the primary active space for the project and will serve as a public meeting and gathering space for residents of the site and the entire city. It shall be designed to be intimate and comfortable with opportunities for outdoor seating to support the retail areas. In addition, water features and public art shall be allowed in this space. The space may be designed to allow for city-wide public events such as farmers markets, art shows, and live music. Outdoor and temporary vendors are allowed in this area. The town square shall be a minimum of 6,000 sq.ft., with the smallest dimension no less than 75 feet to ensure adequate light enters the space.

The third active space shall be a smaller plaza located at the corner of San Pablo and Sycamore Avenues. This plaza shall be designed as the gateway feature of the site. Public outdoor seating for restaurants and cafes may also be located in this plaza. The plaza shall be a minimum of 5,000 sq.ft., with the smallest dimension no less than 50 feet.

These three spaces combined will be a minimum of 0.5 acres.

PRIVATE BALCONIES

Additional private open space for residential uses may be provided in the form of private balconies.

ADDITIONAL LANDSCAPE OPPORTUNITIES

Any part of the parcel that is not designated as building, interior lane, or sidewalk shall be landscaped to provide additional landscape and open space opportunities. Specifically, the area between I-80 and the parking garages shall be landscaped to soften the image of the site for travelers on the Interstate. In addition, the setbacks previously described provide for a landscaped area adjacent to the building facades.

TOTAL OPEN SPACE

As is stated in the zoning code, the total amount of open space shall be 100 square feet of open space per residential unit. The amount of open space depends on the development program proposed. For example, if there are 400 residential units, then there shall be a minimum of 40,000 square feet of open space on the site. This number shall include the three active spaces previously described, private balconies and landscaped buffers. The exact amount of open space on the site shall be determined by the Final PDP, when an exact number of units is determined.

PEDESTRIAN CIRCULATION

The proposed external pedestrian circulation will run along San Pablo Avenue to the west and Sycamore Avenue to the south. No bicycle lanes external to the site are proposed as there are no existing bicycle lanes to which they can connect. All internal pedestrian and bicycle zones will be described and illustrated in the Final PDP.

Site landscape and open space diagram for the PNR parcel

BUILDING HEIGHTS AND VIEWS

BUILDING AND FLOOR-TO-CEILING HEIGHTS

Building heights shall be regulated by two factors: the Area on which the building is placed and the uses within it. Each area has a maximum height limit and within each building, the proposed program will guide the appropriate floor-to-ceiling height.

Established building height restrictions by Area are listed in the table below. The overall heights noted on the building sections are calculated from the ground to the top of the structure (e.g. roof peak).

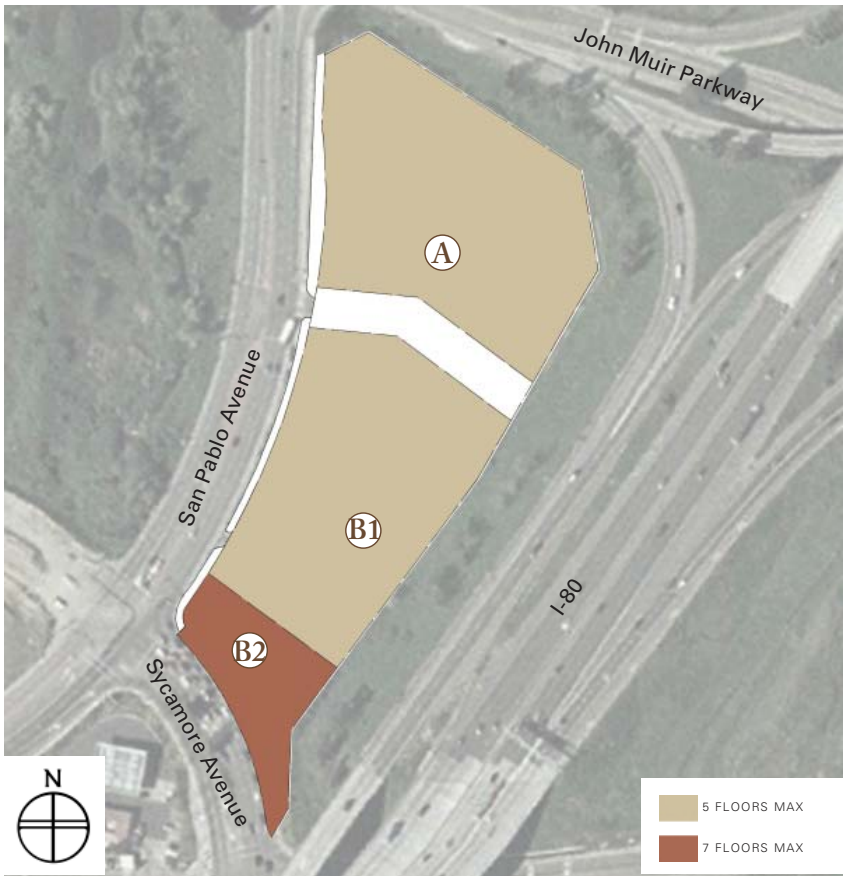
Minimum floor-to-ceiling heights for the ground floor, by use, are also listed in the second table. Additionally, along San Pablo Avenue, the finished floor height for ground-floor residential shall be at least 18 inches above grade. However, finished floor heights of 24 inches minimum are preferred. Elsewhere, finished floor heights may vary with grade or to meet ADA requirements.

The sections to the right illustrate, by area, a variety of program combination options and their corresponding height requirements, including:

- residential only
- residential over retail
- office only

VIEWS

View corridors on site and views from the freeway to the Bay will not be protected.



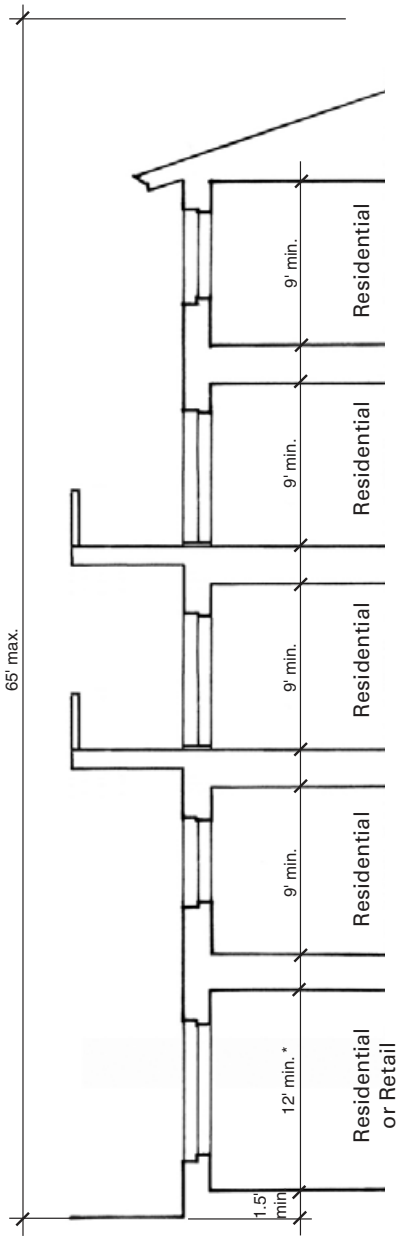
Building height diagram on the PNR parcel

BUILDING HEIGHT RESTRICTIONS BY AREA TABLE

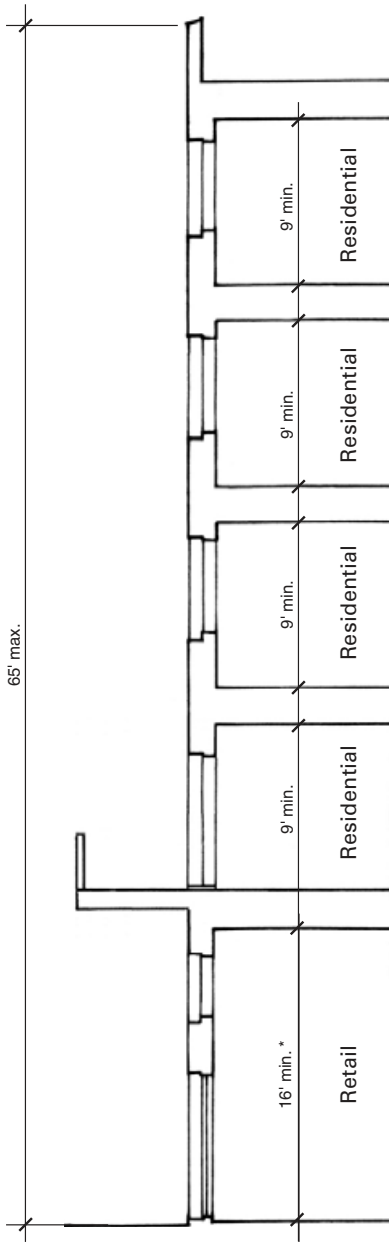
	MAX. BLDG. HEIGHT	TOWER EXTENSIONS
AREA A	65'	25'
AREA B1	65'	25'
AREA B2	85'	25'

MINIMUM FLOOR-TO-CEILING HEIGHTS REQUIRED

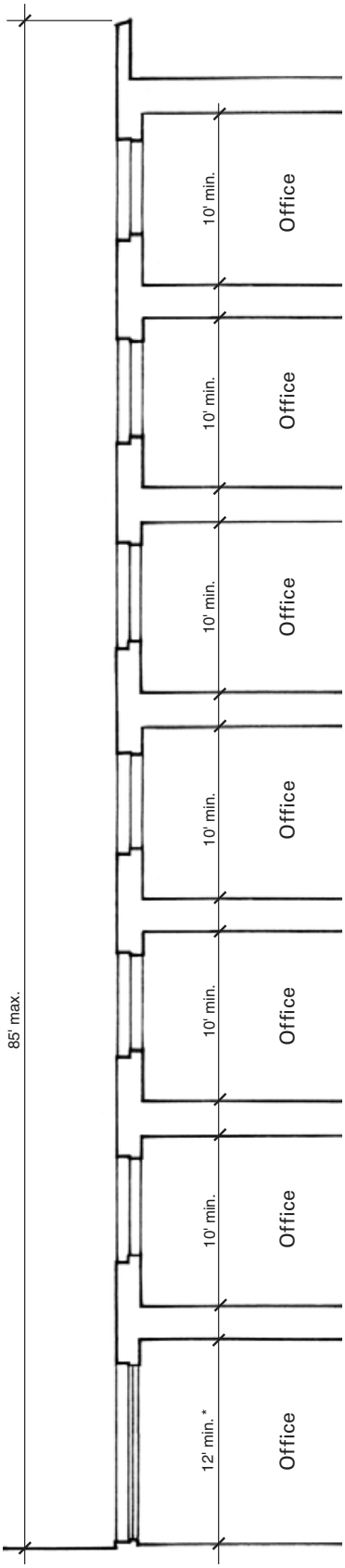
	Ground			Upper Floors		
	A	B1	B2	A	B1	B2
RETAIL	12'.	16’	16’	N/A	N/A	N/A
OFFICE	12'	12'.	12’	10’	10’	10’
RESIDENTIAL	12'	12’	12’	9’	9’	9’



Area A building section: All residential



Area B1 building section: Residential above retail



Area B2 building section: All office

The sections above illustrate several potential use combinations with the required 1st floor height shown.

PLANNING EXAMPLE

Given that the preceding site regulations provide a degree of flexibility in the placement of buildings on the site and the uses within each building, there are multiple development projects that could result from this Initial PDP. The illustration to the right shows one possible configuration of buildings and uses on the site that would be allowed within the Initial PDP. This particular example illustrates a residential building adjacent to several mixed-use buildings centered around a series of small public spaces. Parking is accommodated in two structured garages as well as in on-street parking along a frontage drive on San Pablo Avenue. This is one way to arrange the desired building program, but is by no means the only site plan alternative. The areas of the plan described below respect the guidelines previously recommended in this section of the Initial PDP.

AREA A

Area A will contain residential and live/work uses in up to a five-story, multi-family building. The parking for Area A is accommodated in a multi-level parking garage, Structure A, located at the rear of the parcel against I-80. This garage only services the residential units on this block.

AREA B

Area B is divided into two sub-areas, B1 and B2. Area B1 contains the broadest range of uses. Retail uses will face San Pablo Avenue and line the interior active open spaces of the development. Several floors of residential units will be built above the retail.

The smallest area on the site, Area B2, accommodates up to a seven-story commercial building allowing ground-floor retail with office space above.

Area B1 contains both a multi-level parking structure and surface parking. The surface parking is distributed between a frontage drive along San Pablo Avenue, and along the entry drive between Areas A and B1. Diagonal and head-in parking along those streets is convenient for visitor parking and for retail tenants. Dedicated parking for residential units in Area B is also located in Structure B.

Structure B will be accessible by car from both San Pablo and Sycamore Avenues. The size of both parking structures will be determined by the parking requirements of the uses in each area.

Illustrative plan for the PNR parcel

IMPLEMENTATION SCHEDULE

Approximately 12 months after final discretionary approval is granted, the project will begin the subdivision and engineering process. Next will be the construction of the entire parcel, which is anticipated to be one phase. Construction will start with the parking structures. The expected completion date will be 30 months after the start of construction. This schedule assumes all off-site improvements required to develop the site are completed, such as the replacement parking lots and pedestrian path from PNR parcel to C1 parcel.

The schedule is described in more detail below:

1. Final PDP approval: approval same night as Initial PDP.
2. Begin Construction of off sites: C1 replacement parking lot - (based on City schedule)
3. Construction Documents: completed and permitted by 12 months from Final PDP approval
4. Begin Construction: after C1 replacement parking lot completed and construction permits are in place.
5. Construction Completed: 30 months after construction begins.

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PLAN REVIEW

1 DESIGN REVIEW

1.1 Design Review Required

Except as provided in Section 1.2 below, all structures within the Plan Area are subject to design review in accordance with Chapter 42 of the Zoning Ordinance.

1.2 Buildings Approved in Final PDP

All buildings for which schematic exterior architecture was included within and approved as part of a Final PDP shall be deemed to have passed design review and shall not be subject to the design review provisions of Chapter 42 of the Zoning Ordinance. Temporary uses are subject to standard permits for temporary uses under Section 50.200 of the Zoning Ordinance.

1.3 Design Variations

If Developer desires to vary the approved design of exterior architecture at the schematic level in any manner from a design that previously passed design review under either Section 1.1 or 1.2 above, Developer shall present the proposed design change to the Community Development Director, who shall determine whether the design variation is minor. A minor variation is a variation that the Community Development Director determines, in his or her judgment after consulting with the Town Architect, is consistent with the architectural character, style, quality of materials, and level of detail of the approved design. If the variation is minor, the application shall be processed in accordance with part 1 of Section 42.400 of the Zoning Ordinance for "Applications Subject to Community Development Director Approval." If the variation is not minor, it must be processed for design review in accordance with all relevant provisions of Chapter 42 of the Zoning Ordinance.

2 MODIFICATIONS

2.1 Controlling Provisions for Modifications

Modifications to the Initial PDP or a Final PDP are generally governed by Section 48.900 of the Zoning Ordinance, except that design variations shall be considered under Section 1.3 above and not under Section 48.900 of the Zoning Ordinance.

2.2 Minor Modifications

Chapter 46 of the Zoning Ordinance shall generally apply to modifications to the Initial PDP or a Final PDP that qualify as "minor" under Section 46.300 of that chapter. In addition, any proposed modification to the Initial PDP or a Final PDP of any kind that substantially conforms to the standards, regulations, and guidelines of this Initial PDP and is not in conflict with any provisions of the Zoning Ordinance that apply to the Plan Area, shall be processed as a minor modification under Chapter 46 of the Zoning Ordinance even if such proposed modification would not qualify as minor under Section 46.300. The Community Development Director shall determine whether a proposed variation qualifies as minor for purposes of this Section 2.2.

3 MATTERS OF INTERPRETATION

The rules set forth in Chapter 2 of the Zoning Ordinance for interpreting the Zoning Ordinance shall also apply to the interpretation of the Initial PDP and any Final PDP. The Initial PDP shall supersede the Zoning Ordinance as to matters specifically addressed in the Initial PDP.

4 APPEALS

Appeal procedures for any application described in Sections 1 or 2 above shall be in accordance with the appeal procedures set forth in the Zoning Ordinance.

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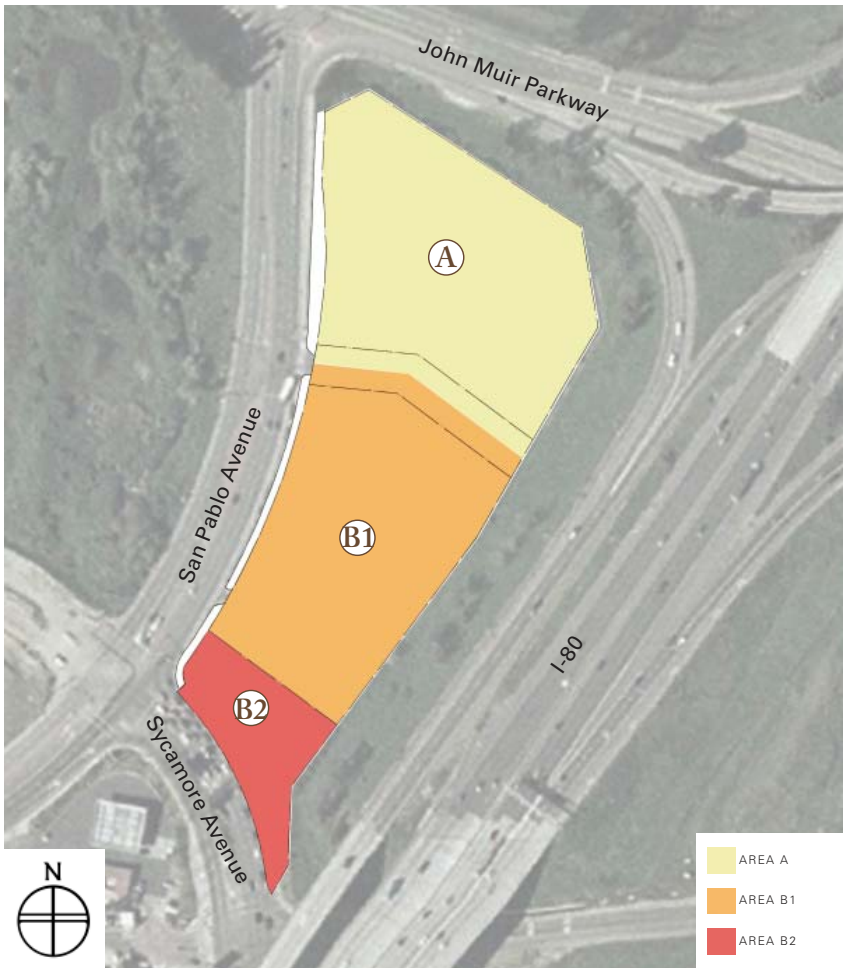
CONCEPTUAL ARCHITECTURE

OVERVIEW

This section of the document illustrates architectural requirements for the Hercules New Town Center PNR Parcel, as well as examples of how the buildings could be designed. On the following pages, illustrative examples of precedents and future design examples are presented along with required standards pertaining to: architectural styles, materials, and building dimensions. The document also includes recommendations and best practices that support sustainability.

The massing alternatives presented in this section for buildings on the site allow for market flexibility for the development team within a framework that meets the vision of the Central Hercules Plan. The massing will be articulated in combinations of the five styles illustrated in this document. These styles are common to the California coastal communities and the Bay Area. The goal is to create an assembly of buildings that reflect the patterns of Town Center environments around the Bay Area – that is, to resemble a collection of buildings assembled over time. This assembly will convey the character of Hercules, where it has been and where it is going in the future.

For all styles, both traditional materials and materials that replicate traditional materials shall be considered for approval. Other styles may be considered for approval, however, it will be the responsibility of the project sponsor to provide precedents of other buildings and to describe how this new style is appropriate for the New Town Center.



The diagram above illustrates the area divisions within the PNR parcel. Architectural styles and massing types will not be restricted by the area location.

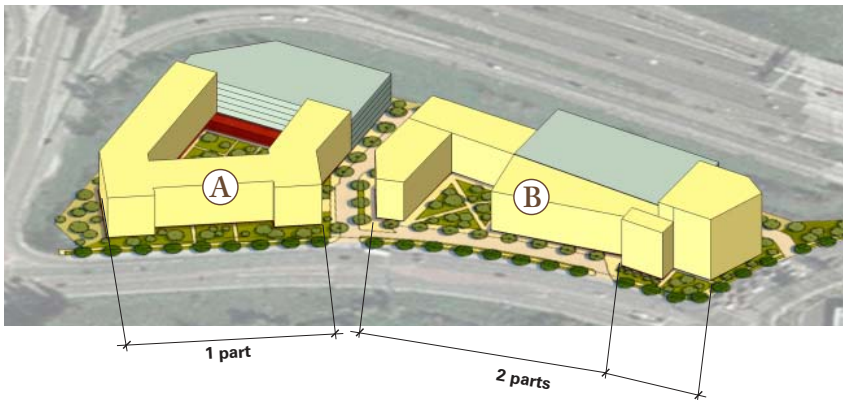
Right: Photos of the five architectural styles including, from top to bottom, Bay Area Eclectic, Bay Area Art Deco, California Spanish Revival, Bay Area Traditional Industrial, and Bay Area Modern Industrial

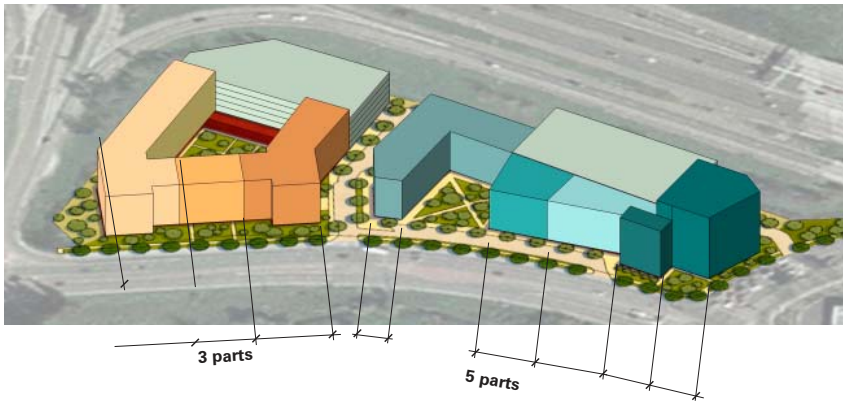
MASSING & COMPOSITION

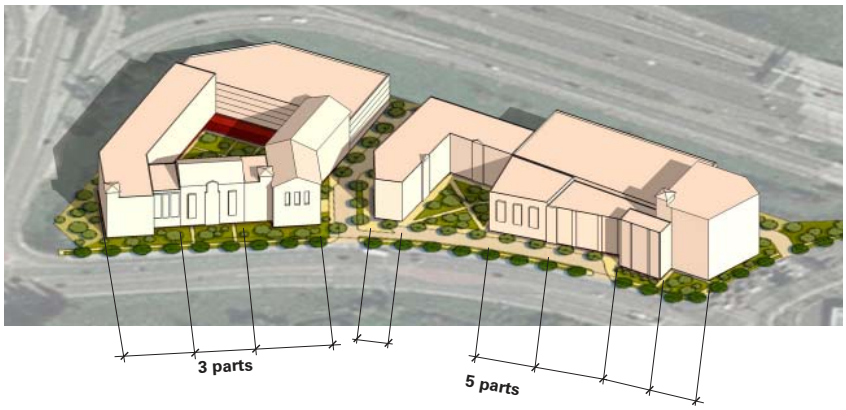
Given the multiple site plan options, this section of the Initial PDP provides guidelines for massing and composition to ensure a town center scale development. Although the development is one large parcel, it will be broken down into smaller elements in order to ensure that the buildings are in scale with the local context. The architecture will exemplify a level of diversity reflective of the architectural styles of the area.

The buildings in areas A, B1, and B2 shall be composed to read as an assembly of multiple buildings. The courtyard building, Area A, and the mixed-use building, Areas B1 and B2, will likely be separate buildings, although they may be connected by the parking structure at the upper levels. The diagrams to the right provide a method for articulating buildings in these three areas.

The project may express a diverse architectural character by breaking down each building into smaller compositional elements which can then be expressed in the various architectural styles defined here. Any of the building areas within the planning area, Areas A, B1, and B2, may be described in one or more of the styles articulated in this section of this document, as long as the general guidelines on the possibility pages are followed. Continuous facades shall be composed to read as individual buildings per the guidelines illustrated on the following page.







Note: Images are for illustrative purposes only. Proposed projects must have further articulation.

BASIC PARCEL ARTICULATION

» The parcel shall be broken down into multiple buildings.

BASIC BUILDING/ FACADE ARTICULATION

» Buildings should be further articulated to read as distinct and separate groupings of elements. This could be done through massing articulation and/or through change of architectural style.

» Parcel A: the building along the San Pablo frontage shall be broken down into a minimum of three parts.

» Parcel B1 & B2: the assembly along the San Pablo frontage, which may include the primary commercial uses, shall be expressed as a minimum of five smaller parts.

DETAILED BUILDING/ FACADE ARTICULATION

To further articulate longer facades the following special elements may be applied:

» parapet walls

» changes in material and color

» special windows and bays

» stairway and chimney elements

» special roof forms

» balconies, canopies and awnings

BAY AREA ECLECTIC

The buildings of Bay Area historic town centers and urban environments contain buildings that exemplify the intersection of a number of styles, hence the use of the word 'eclectic' to characterize this style. The buildings largely represent the work of architects prior to the Great Depression and their architecture reflects its broad inheritance: in any one building, it is common to find design influences from many styles and geographies. Eclectic style buildings are typically very simple, with elaborate ornamentation representing multiple influences, in a common scale and comparable level of detailing. In addition to the many pre-Depression era examples, there are also several examples of new construction in the Bay Area that have been designed in this style.

ESSENTIAL ELEMENTS

- Simple facade fenestration patterns
- At times, elaborate ornamentation
- Flat roofs and parapet walls
- Variety of stylistic influences

Telegraph Avenue, Berkeley

Shattuck Avenue, Berkeley

Embarcadero Avenue, San Francisco

Martin Luther King Drive, Berkeley

Milvia Street, Berkeley

DETAILS

MATERIALS

- Stucco
- Wood
- Fiber cement board
- Copper and metal accents

ROOF

- Flat

DETAILING

- Wood
- Metal railings

WINDOWS

- Windows that replicate wood construction, subject to approval by Town Architect

BAY AREA ART DECO

The Bay Area has several iconic buildings built in the Art Deco period, the two decades spanning the 1920s and 1930s. The designs are, at times, classically inspired both in the plan and elevation proportions and scale. Toward the end of the era, windows were located in corners and had horizontal proportions. However, the detailing is very modern, simple, and streamlined in design to reflect a hopefulness for the future. Although these exemplar buildings are generally classified as Art Deco, they have very specific regional design characteristics. Color, materials, detailing and environmental characteristics at times reflect regional Arts & Crafts designs.

ESSENTIAL ELEMENTS

- Classical scale, proportion and patterns
- Simple, modern detailing
- Flat roofs and parapet walls
- Variety of stylistic influences

Shattuck Avenue, Berkeley

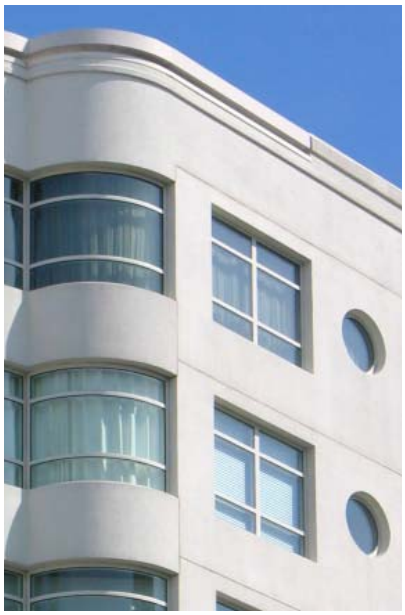
Shattuck Avenue, Berkeley

Shattuck Avenue, Berkeley

Bay Area Art Deco building

Shattuck Avenue, Berkeley

DETAILS



MATERIALS

WALLS

- Reinforced concrete
- Stucco

ROOF

- Flat

DETAILING

- Concrete
- Stucco
- Metal

WINDOWS

- Metal or windows that replicate metal construction, subject to approval by Town Architect

CALIFORNIA SPANISH REVIVAL

California Spanish Revival has been a consistent architectural language in this region for more than a century. The style was first developed by the Spanish and had a significant effect on the development of coastal California. The style is based on a broad variety of influences and combines both local and Mediterranean precedents into a unique architecture for the Bay Area. The California Spanish Revival style developed as a vernacular, regional design based on climatic considerations and features tile roofs over whitewashed stucco walls.

The core elements of the style were combined with modern program requirements to create an inventive language for neighborhoods and an urban environment. The combination of the dramatic setting and the California Spanish Revival style has contributed strongly to the identity of this region.

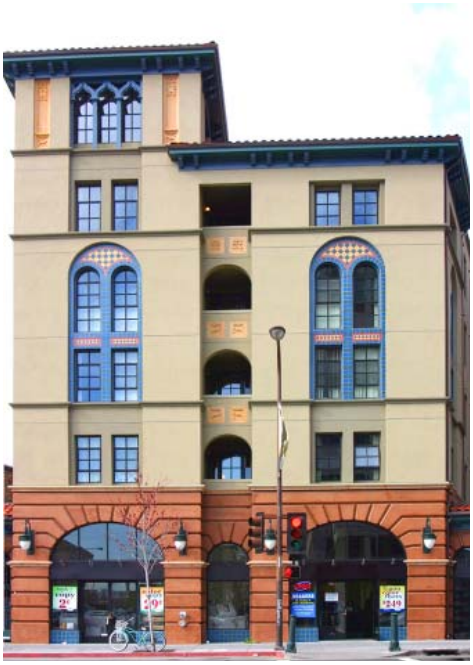
ESSENTIAL ELEMENTS

- Simple masses articulated to create picturesque compositions
- White, light color stucco walls
- Combination of roof forms and shapes
- Vertical proportion for windows and doors
- Bay windows, stairways and chimney elements

Bancroft Street, Berkeley

Santa Barbara

Santa Barbara



University Avenue, Berkeley

DETAILS

MATERIALS

WALLS

» Stucco
» Tile veneer accents

ROOF

» Flat or
» Sloped with slate, concrete or clay tiles to replicate traditional flat and barrel profiles

DETAILING

» Wood or
» Metal railings

WINDOWS

» Windows that replicate wood construction, subject to approval by Town Architect

BAY AREA TRADITIONAL INDUSTRIAL

The Bay Area has a rich industrial past. Much of the architecture from the great eras of industry is gone, but those buildings that remain are cherished. Most of the surviving buildings are located close to San Francisco Bay, with convenient proximity to shipping and rail. The buildings are masonry with large openings of glass for maximum light. While some of the buildings continue to house industrial uses for today's small businesses, many of the existing buildings have been successfully transformed for twenty-first century uses and provide precedents for new construction that incorporates housing, retail and office environments.

ESSENTIAL ELEMENTS

- Simple massing and forms
- Repetitive bays and window patterns
- Flat roofs and parapet walls
- Large openings with operable windows with small-scale panes

Los Angeles lofts

Oakland lofts

Los Angeles lofts

Victoria Gardens regional mall, Rancho Cucamonga

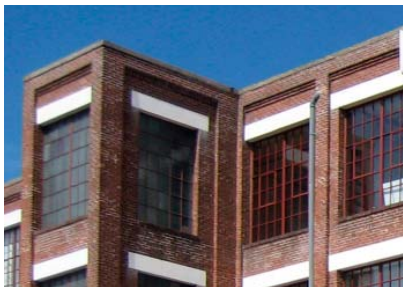
Emeryville

Embarcadero, San Francisco

Emeryville

San Francisco

DETAILS



MATERIALS

WALLS

- Brick
- Reinforced concrete
- Stucco

ROOF

- Flat

DETAILING

- Concrete
- Brick
- Stucco
- Metal

WINDOWS

- Metal or wood windows or
- Windows that replicate wood or metal construction, subject to approval by Town Architect

BAY AREA MODERN INDUSTRIAL

Modern architecture with an industrial sensibility has played an important role in the identity of Bay Area architecture in the last twenty years. Many modern industrial buildings were built after World War II, and the tradition has continued in contemporary designs of residential, retail, and commercial buildings. The buildings have a regional modern aesthetic that responds to climatic considerations.

The buildings do not respond to a set system of proportions or traditions, but have a consistent connection of the interior and exterior through the use of a significant ratio of windows to the walls of the building. Common elements are window shading elements, large areas of glass, and operable windows.

ESSENTIAL ELEMENTS

- Large openings of glass
- Operable windows
- Window shading devices
- Use of metal detailing
- Combination of flat roof repetitive gable and shed roof shapes

Los Angeles factory

Newport Beach Cannery

Healdsburg

Soma, San Francisco

San Francisco

DETAILS

MATERIALS

WALLS

- Reinforced concrete
- Stucco
- Metal

ROOF

- Flat or
- Sloped with metal materials

DETAILING

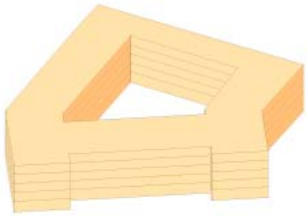
- Stucco
- Metal

WINDOWS

- Metal or
- Windows that replicate metal construction, subject to approval by Town Architect

AREA A | BASE ALTERNATIVE DESIGN POSSIBILITIES

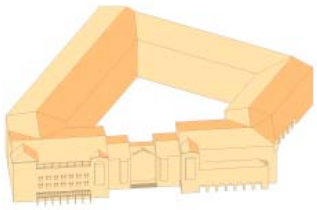
MASSING



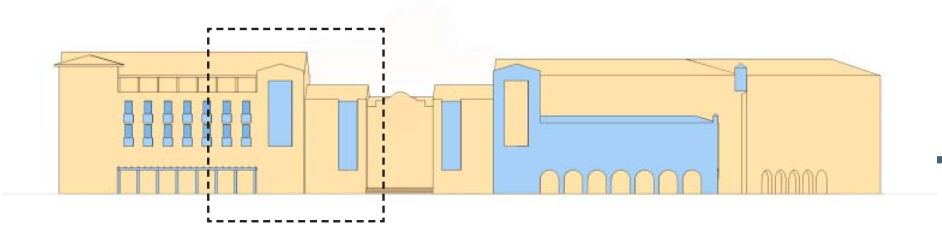
Basic building envelope

In order to benefit from the full variety of the five styles, it is essential that each parcel's massing reflect the unique characteristics of several of the styles. As an example of this method, the courtyard configuration for Area A (illustrated at left) can be sculpted using the California Spanish Revival, the Bay Area Traditional Industrial and/or the Bay Area Art Deco styles, singly or preferably in segments of the building to give it the appearance of a sequence of buildings rather than one continuous mass.

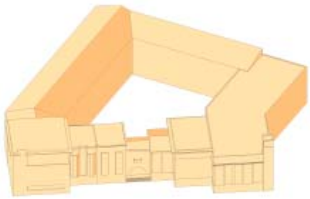
ELEVATION POSSIBILITIES



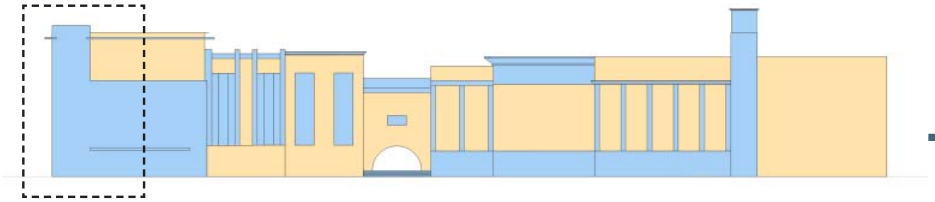
Building massing



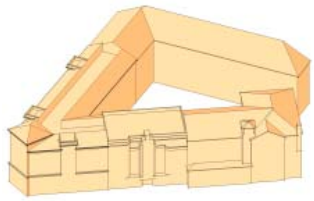
Elevation of one area with a segment in the California Spanish Revival style



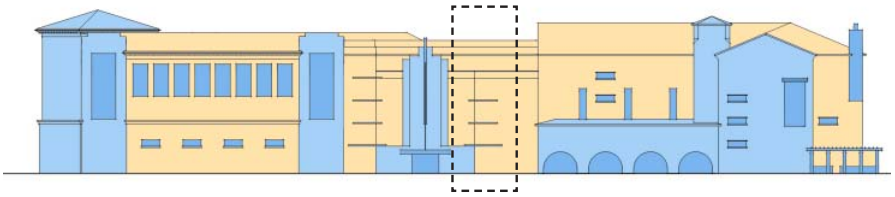
Building massing



The Bay Area Traditional Industrial style shown in a portion of an area elevation



Building massing



A full area elevation emphasizing a section done in the Bay Area Art Deco style

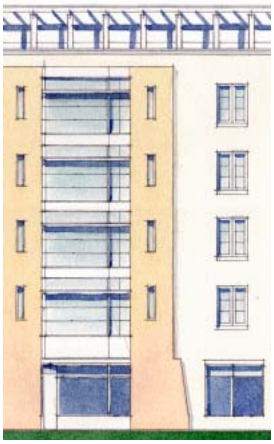
Precedent: Santa Barbara

Elevation detail

Precedent: San Francisco

Elevation detail

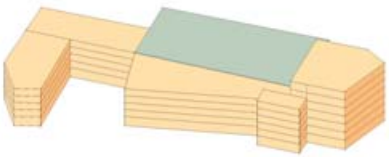
Precedent: Berkeley



Elevation detail

AREAS B1 & B2 | BASE ALTERNATIVE DESIGN POSSIBILITIES

MASSING

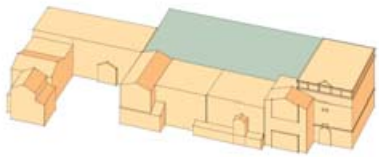


Basic building massing

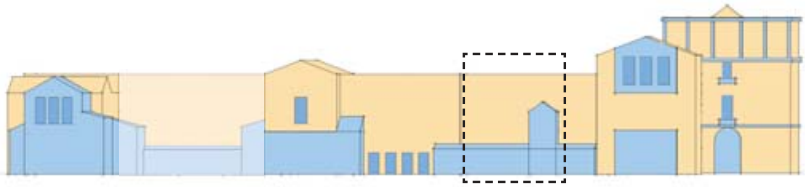
In the New Town Center PNR parcel, styles may be mixed along the length of the facade to both mimic the variety of traditional main streets and also mitigate the length of the buildings. When a single style is chosen for the length of the facade, it is recommended that vertical elements and localized geometries be used to create this same traditional main street effect (as illustrated in the following three variations for Areas B1 and B2).

Each style variation results in a different massing strategy, but each still employs vertical and additive elements that bring scale and rhythm to the street wall. Below, the California Spanish Revival, the Bay Area Eclectic, and the Bay Area Modern Industrial styles are each represented.

ELEVATION POSSIBILITY 1



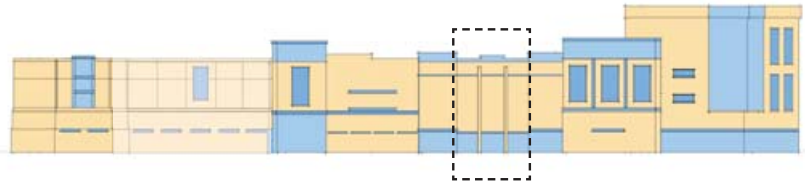
Massing variation



An example of a full area elevation in the California Spanish Revival style

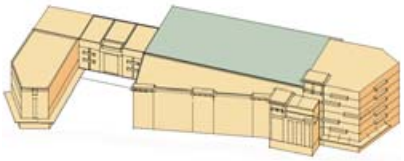
ELEVATION POSSIBILITY 2

Massing variation

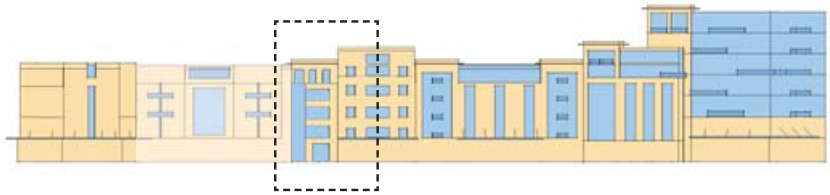


The Bay Area Eclectic style shown in a segment of an area elevation

ELEVATION POSSIBILITY 3



Massing variation



A portion of a full area elevation designed in the Bay Area Modern Industrial style

Precedent: Santa Barbara

Elevation detail

Precedent: Orlando

Elevation detail

Precedent: Emeryville

Elevation detail